



Development Assessment Panel of City of Prospect

(Presiding Member: Mr David Cooke)

The meeting of the Development Assessment Panel will be held in the Civic Centre, 128 Prospect Road, Prospect at **5.30pm Monday 11 January 2016**.

Nathan Cunningham
Director Community, Planning & Communications

Members: Mr David Cooke, Ms Alison Bowman, Mr Sam Green,
Ms Monica Lee, Mr Julian Rutt, Mr Darren Starr,
Mr Simon Weidenhofer

A G E N D A

1. **On Leave**
2. **Apologies**
3. **Confirmation of the Minutes of the Development Assessment Panel held on 14 December 2015.**
4. **Protocol**
 - 4.1 The Panel has adopted the protocol that only those agenda items on the Panel reports reserved by Members on a callover by the Presiding Member will be debated and the recommendations of all other items will be adopted without further discussion.

5. Development Applications for Decision

5.1 253 Churchill Road, Prospect – Four Storey Mixed Use Development comprising 16 dwellings and Ground Level Office, Car Parking and Landscaping (DA 050/248/2014)

Representors: Catherine Purvis, 22 Princes Street Prospect
Tom Colmer, 255 Churchill Road Prospect

Respondent: Michael Loucas to respond on behalf of the applicant

(Pages 1 - 63, Recommendation pages 13 – 17)

6. Other Reports

6.1 Summary of Development Assessment Commission (DAC) Decisions and Proposals Greater than \$3 Million called in by the Coordinator-General

(Pages 64 – 65)

7. Matters Before the Environment, Resources and Development Court

7.1 Summary of Court Appeals

(Page 66)

8. Time, date and place of next meeting

5.30pm Monday 8 February 2016 – Civic Centre, 128 Prospect Road, Prospect

9. Closure

AGENDA ITEM: 5.1

To: Development Assessment Panel (DAP) on 11 January 2016

From: Scott McLuskey, Senior Development Officer Planning

Proposal: Four Storey Mixed Use Development comprising 16 dwellings and Ground Level Office, Car Parking and Landscaping (DA 050/248/2014)

Address: 253 Churchill Road, Prospect (CT 5684/552)

SUMMARY:

Applicant: Loucas Zahos Architects

Owner: Mr D Zarkovic

Planning Authority: Council

Mandatory Referrals: Department of Planning, Transport and Infrastructure (DPTI)

Independent Advice: Lumen Studio

Public Notification: Category 2

Representations: Three

Representors to be heard: Catherine Purvis, 22 Princes Street Prospect
Tom Colmer, 255 Churchill Road Prospect

Respondent: Michael Loucas

Development Plan Version: Consolidated 12 February 2015

Zone and Policy Area: Urban Corridor Zone (Boulevard Policy Area)

Key Considerations: Landscaping, Setbacks, Visual privacy, Car Parking

Recommendation: **Approval**

ATTACHMENTS:

Attachment 1 Development Application Form

Attachments 2-3 Locality plans

Attachments 4-5 Photography of site

Attachments 6-17 Proposal plans

Attachments 18-20 Design Review Comments

Attachments 21-25 Response from Department of Planning, Transport and Infrastructure

Attachments 26-28 Representations

Attachments 29-31 Response to Representations

Attachments 32-45 Traffic Report

Attachments 46 Superseded Proposal Plans

1. EXECUTIVE SUMMARY

- 1.1 The applicant proposes a four storey mixed use building comprising 16 self-contained apartments, a ground level office tenancy and 16 ground floor car parking spaces. The development is proposed on one allotment within the Boulevard Policy Area.
- 1.2 The proposal was referred to the Department of Planning, Transport and Infrastructure as the site fronts an arterial road. The application was also referred to Lumen Studio for Design Review comment. Public notification was undertaken as the proposal is a Category 2 form of development, with three representations received from nearby property owners.
- 1.3 The design review concluded that the building creates substantial visual interest, using modulated faces and forms, changes in materials and well-proportioned openings. Dwelling orientation would generally offer suitable natural light entry and cross-ventilation opportunities. Key considerations relating to visual privacy, building setbacks and landscaping are discussed in greater detail in this report.
- 1.1 The proposal provides suitable occupant amenity and private open space, provides a high level of visual interest and responds well to concerns relating to its scale. Car and bicycle parking would be appropriately catered for on-site, while overlooking and overshadowing impacts would be, with the assistance of imposed conditions, addressed suitably in the context of the zone. The proposal therefore warrants development plan consent.

2. LOCALITY AND SUBJECT LAND

2.1 Locality

- 2.1.1 The locality is predominantly residential in nature, although features several commercial land uses including consulting rooms and offices. A large, vacant parcel of land is adjacent the subject land on the western side of Churchill Road, while development east of the subject land in the residential zone is typically single storey detached dwellings.
- 2.1.2 Churchill Road is a primary arterial road under the control of the Department of Planning, Transport and Infrastructure and is a major transport corridor which links the inner northern metropolitan area with the central Adelaide region. The land is approximately 140m away from the nearest city bound bus stops to the north and south, while a north bound bus stop is approximately 70m away from the subject site.
- 2.1.3 The broader locality, indicating the location of the subject land within the relevant Zone and Policy Area as described in Council's Development Plan, as well as the location of the adjoining property owners who lodged representations, is described in **Attachment 2**.

2.2 Subject Land

- 2.2.1 The subject land is located on the eastern side of Churchill Road, at the intersection of Redin Street. The land comprises one allotment with a total area of 670m², with a frontage of 16.3m to Churchill Road and a depth of approximately 43m.

- 2.2.2 Existing site improvements include a single-storey detached dwelling and two outbuildings in the rear yard. Vehicular access to the site is via two single crossovers, one located to the Churchill Road frontage and one in the south-east corner. There are no significant trees on the subject land or within close proximity on adjoining allotments, though several mature Council street trees are adjacent the site in Redin Street.
- 2.2.3 The site is affected by the Metropolitan Adelaide Road Widening Plan, though the DPTI has advised that all required land at this time has already been acquired. The subject land is illustrated on **Attachment 3**. Photographs of the subject land are also include for the DAP's reference (refer **Attachments 4-5**).

3. PROPOSAL

- 3.1 The applicant proposes the construction of a four storey residential flat building comprising 16 self-contained dwellings and 16 ground floor car parking spaces.
- 3.2 Minimal earthworks would be required to level the site and landscaping of limited height is proposed to the street frontages of the site.
- 3.3 No other works are proposed. The proposal plans are attached (refer **Attachments 6-17**). A traffic report prepared by Phil Weaver of Phil Weaver and Associates is also attached (refer **Attachments 32-45**).

4. REFERRALS

4.1 Internal (Advisory) Referrals

- 4.1.1 The proposal was referred to Mr Julian Rutt of Lumen Studio for informal design review in accordance with Council's Design Review Process for Higher Density Development (refer **Attachments 18-20**).
- 4.1.2 Briefly, the review identified the following:
- The proposal's form and massing has been well treated through the use of modulated faces and forms and well-proportioned openings. Articulation, through material changes and modulation of solid elements, provides significant visual interest and break down the building's scale well. South-facing balconies and openings provide high levels of desirable passive surveillance.
 - While the west-facing (Churchill Road) façade is largely blank, the proposed feature material and ground level treatments overcome the aesthetic impacts of this approach.
 - While apartments generally make use of ideal solar orientation, with passive solar access available to main living areas of all units, an undesirable number of bedrooms within the proposal are reliant on borrowed light. Cross-ventilation opportunities are generally suitable.
 - The large building footprint and vehicle access requirements limit the area available for landscaping substantially. Further, the positioning of landscaping to, effectively, the southern side of the building only limits available planting options.
- 4.1.3 The applicant has not made any amendments to the proposed plans as a result of the above comments, noting broadly in their response that the proposal seeks to balance the competing desires for street level activation, car parking and landscaping.

- 4.1.4 It is also noted that the reliance on borrowed light was highlighted in preliminary discussions by Council staff, with the applicant indicating a desire to preserve an internal layout maximising passive solar access to living areas. It is acknowledged that this configuration is similar to an approved residential flat building at 210 Churchill Road lodged by the same applicant.

4.2 External (Legislated) Referrals

- 4.2.1 The proposal was referred to the Commissioner of Highways as required by Schedule 8 of the Development Regulations 2008. In response (refer **Attachments 21-25**), the Department of Planning, Transport and Infrastructure advised that:
- The use of a single shared access point from Redin Street is supported, with the access point suitably separated from the Churchill Road intersection. Qualified support is also given for the possible alternative access location described by the applicant.
 - Internal manoeuvring areas are considered to be sufficient to achieve compliance with the relevant Australian Standard. It is noted that internal vehicular manoeuvring in relation the possible alternative access location has not been fully assessed.
 - The site is affected by the Metropolitan Adelaide Road Widening Plan, though all land required at this time has already been acquired. As the development is within 6 metres of the possible requirement, consent from the Commissioner of Highways will be required.
 - No objection is raised, in-principle, to the proposed development.
- 4.2.2 If approved, DPTI recommends conditions be imposed (refer **Attachments 23-24**).
- 4.2.3 No other consultation with agencies was required.

5. **PUBLIC NOTIFICATION**

- 5.1 The application is a Category 2 form of development pursuant to Section 38 of the *Development Act 1993*, Schedule 9 of the *Development Regulations 2008* and Urban Corridor Zone Principle of Development Control 22.
- 5.2 A residential flat building is a Category 1 development in the Urban Corridor Zone unless it is located on land adjacent to the Residential Zone or Historic (Conservation) Zone and if it would be three or more storeys, or 11.5 metres or more in height, or if it exceeds the 'Building Envelope - Interface Height Provisions' (UCZ PDC 22). Where it is not a Category 1 development, a residential flat building is a Category 2 development.
- 5.3 It is also noted that an office or a shop (of less than 2,000m² gross leasable area) are Category 1 developments within the Urban Corridor Zone.
- 5.4 The subject land is located adjacent the Residential Zone (refer **Attachment 2**). While the walls of the building themselves do not exceed the interface height provisions, various elements of the building such as balconies, balustrades, screens and the roof of levels 2 and 3 do exceed the interface height provisions.
- 5.5 To this end it is briefly noted that a building, as defined within Section 4 of the *Development Act 1993*, is a building or structure or a portion of a building or structure (including any fixtures or fittings subject to the *Building Code of Australia*). It is thus apparent that the elements described above are relevant portions of the building when considering the interface height provisions.

Given the above, public consultation was undertaken. The public notification period ended with three representations received. Two representors wish to be heard.

5.6 The representors raised the following concerns with the proposal (refer **Attachment 26-28**):

- 5.6.1 The provision of car parking on site was considered to be deficient, with no visitor parking available for apartments or the ground level non-residential tenancy. It was highlighted that limited opportunities for parking adjacent to the site on Churchill Road are available due to the change in road configuration at this location. Further, it was noted that parking opportunities adjacent to the site on Redin Street are limited due to parking restrictions near the Churchill Road intersection and as a result of regular attendance at a nearby place of worship (Gurdwara Sri Sarbat Khalsa Sahib).
- 5.6.2 The scale of the building was considered to be unduly substantial, with reference drawn to the current upper extent of the scale of nearby buildings (Reephram Hotel) and to the relative narrowness of nearby Redin and Princes Streets (noting that Redin Street is identified on Council's Narrow Street List).
- 5.6.3 Visual privacy issues arising from north and east-facing balconies, and the height of balustrading proposed, were considered to inappropriately interfere with the residential amenity enjoyed by neighbouring properties.
- 5.6.4 Issues of environmental pollution arising from the car park and waste storage area were highlighted, with attention drawn to the waste management area and the height and materiality of fencing proposed.

5.7 The applicant responded to the representations and noted the following (refer **Attachments 29-31**):

- 5.7.1 A traffic and parking report is being prepared by Phil Weaver of Phil Weaver and Associates which will address issues of car parking spaces quantity and internal manoeuvring. Amended plans will be provided following receipt of Mr Weaver's advice.
- 5.7.2 The scale and articulation of the proposal has been designed with respect to the policy vision along Churchill Road following the re-zoning of this corridor. In this context, it is noted that existing buildings such as the Reephram Hotel have not informed the scale of the proposal.
- 5.7.3 Amendments have been made to north eastern balconies, though the height of north-facing balcony screening is considered an integral part of the Ecologically Sustainable Development (ESD) design. Further, it is noted that the Development Plan anticipates the provision of lesser screening for buildings of this type within this zone.

5.7.4 Amendments to the proposal plans would result in the waste storage area being enclosed and a 2.4m masonry wall replacing the previously proposed 2.1m colorbond fencing. Maintenance of the waste collection area is anticipated to be addressed by the selected community management body.

6. PLANNING COMMENTARY

- 6.1 The application involves building work and therefore an application to Council is required. The proposal is neither a complying nor a non-complying development with reference to Principle of Development Control 21 of the Urban Corridor Zone and is therefore to be considered on its merits against the relevant provisions of Council's Development Plan.
- 6.2 Pursuant to Section 35(2) of the *Development Act 1993*, a development that is assessed by the Council as being seriously at variance with the Development Plan must not be granted consent. To this end, the Panel must determine whether the proposal is seriously at variance with the Development Plan prior to making a decision on the application.

7. PLANNING ASSESSMENT

7.1 Land Use

- 7.1.1 The Desired Character Statement for the Urban Corridor Zone states that development within the Zone would enable a high quality mixed use environment that contributes to the economic vitality of the City of Prospect by increasing the density of housing, as well as the number and the diversity of businesses and other services offered to residents and the wider community.
- 7.1.2 The above is reiterated by the Objectives of the Urban Corridor Zone which outline that future development should incorporate a mixed of land uses accommodating a range of compatible non-residential and medium and high density residential land uses orientated towards a high frequency public transport corridor (UCZ Objective 1).
- 7.1.3 Within the Boulevard Policy Area, medium and high rise development including mixed use buildings that contain shops or offices at lower floors with residential uses above are desired (UCZ BPA Obj. 1) The proposal is therefore considered to be an appropriate mix of land uses.

7.2 Site area

- 7.2.1 The Boulevard Policy Area anticipates medium and high density housing, primarily in the form of apartment and terrace style dwellings, which would accommodate a range of dwelling sizes to encourage diversity in household types within the precinct. In order to achieve this, the minimum residential site density for residential development within the Boulevard Policy Area is 100 dwellings per hectare net, unless varied by the Concept Plan (UCZ PDC 5).
- 7.2.2 The subject site which has an area of 670m² is not identified within the Concept Plan, therefore the minimum net residential site density would be achieved through the provision of 7 dwellings. The proposal is for 16 dwellings within the mixed use flat building, therefore satisfying the minimum desired residential site density.

7.3 Design and Appearance

- 7.3.1 It is anticipated that development within the Urban Corridor Zone will achieve a high standard of architectural design through careful building articulation and fenestration to all visible sides. The design of building facades should contribute positively to the street by articulating the built form and accentuating the building's functions, emphasising the distinction between the base, middle and top of buildings and providing vertical elements that create a strong vertical rhythm (UCZ Desired Character Statement).
- 7.3.2 Buildings on allotments that have a frontage greater than 10m should be well articulated through variations in forms, materials, opening and colours (UCZ PDC 8). Where a building is sited on or close to a side or rear boundary, the boundary wall should minimise the visual impact of the building as viewed from adjoining properties (Council-wide PDC 132).
- 7.3.3 The Churchill Road façade would have a low solid to void ratio at ground floor, with projected timber cladding above. While there would be no fenestration above ground level to this façade, the feature material and immediate ground level treatments were identified through the design review process as an appropriate design response. The Redin Street façade would feature significant material changes and the modulation of solid elements and window/door openings to create a high level of visual interest.
- 7.3.4 Building materials would consist of pre-cast concrete in natural and dark finishes, a mix of timber cladding finishes, metal balcony balustrades in a copper/rust finish, aluminium louvre screens and a south-facing vertical garden. It is noted that the selection and variation of materials used was commended by Mr Rutt.
- 7.3.5 Overall it is considered that the architectural features of the proposed building, in combination with the diversity of colours and finishes as set out in the elevation drawings, would provide an appropriate level of visual interest and a high quality built form.

7.4 Setbacks

- 7.4.1 Within the Boulevard Policy Area, the minimum setback from the primary road is 3m, and from the secondary road is 2m, unless varied by the Concept Plans within Council's Development Plan. For allotments with a frontage width of 20 metres or less, there is no minimum setback for the first 2 levels of a building from a side boundary when adjoining another allotment and a minimum 2m setback is required for all levels above this height (UCZ PDC 16 and 18).
- 7.4.2 The ground floor is proposed to be built adjacent the northern and eastern boundaries of the site. Level 1 would have balcony balustrades adjacent to the northern site boundary and would be setback approximately 3m from the eastern site boundary. Levels 2 and 3 would generally be setback 2m from the northern boundary, though would have balcony balustrades for three dwellings of each level protruding forward to a boundary setback of 1.3m.
- 7.4.3 Level 2 would be setback 6m from the eastern property boundary while Level 3 would be setback 8.8m from this boundary. These setback distances generally accord with the interface height provisions though the balconies of Apts. 304 and 305, along with associated screening devices and roof overhang, would project into the desirable setback distance. These departures are considered to be relatively minor, as the scale of the building is still suitably softened to the residential zone boundary as a result of the low visual mass of these elements.

- 7.4.4 At ground level, the building would vary in setback to Churchill Road with a portion of the building proposed to be 1m setback from the western property boundary and the remainder proposed to be setback 3m. Levels 1, 2 and 3 would be setback 2m from the Churchill Road boundary. The building would be setback 500mm from the Redin Street boundary.
- 7.4.5 Building setback provisions would be generally achieved to the more sensitive northern and eastern boundaries, with balcony protrusions being of limited mass. While it is considered that the scale of the building is well broken down through other design techniques, it is acknowledged that the less than desirable setbacks proposed limit landscaping opportunities. Overall the siting of the building on the allotment is considered appropriate, though further consideration of landscaping occurs later in this report.

7.5 Private open space provision

- 7.5.1 Private open space should be provided for each dwelling and should be located so that it is accessible directly from internal living areas. Private open space should have sufficient area and shape to be functional (Council Wide PDC 148).
- 7.5.2 Private open space areas located above ground level should have a minimum width of 2 metres, while ground level open space should have a minimum width of 3 metres (Council-wide PDC 153). Above ground level, two bedroom dwellings should have a minimum of 11m², while ground level dwellings should have a minimum of 24m² of private open space (Council-wide PDC 152).
- 7.5.3 13 of the proposed dwellings would be provided with a north facing balcony, while the remaining 3 dwellings would have an east facing balcony. The balconies would be directly accessed by sliding door from the living areas. The minimum balcony dimension would generally be 2m, though Apt. 304 would have a minimum 1.9m dimension. Apts. 101 and 106 would also have a south facing courtyard.
- 7.5.4 Excluding courtyards and areas less than 1.9m in minimum dimension, the minimum balcony area would be 11.2m², while the maximum balcony area would be 18m². It is also noted that visual privacy screening discussed later in this report would not unreasonably impact the functionality of the spaces.
- 7.5.5 Therefore the private open space that would be provided for the dwellings is considered appropriate.

7.6 Traffic and Vehicular Movements

- 7.6.1 It is anticipated that new developments minimise the number of access points onto arterial roads, by providing vehicle access from side streets, rear access ways, via rights of way or common vehicle parking areas (UCZ PDC 11).
- 7.6.2 Vehicular access to the site would be via a two-way crossover to Redin Street that would be 6m wide. The current single crossover to Churchill Road would be reinstated to kerb and gutter.
- 7.6.3 A roller door is proposed to secure the car park which would be setback 2m from the Redin Street boundary and 6.5m from the kerb of the road. The crossover would be setback 40m from the Churchill Road intersection minimising conflict between vehicles accessing the site and those travelling through the intersection.

- 7.6.4 Advice has been provided by a traffic expert, Phil Weaver, who has concluded that all vehicles will be able to enter and exit each parking space and the site itself in accordance with the relevant Australia Standard.
- 7.6.5 The proposed access point would result in the removal of a mature Council street tree. The removal and replacement contributions to be paid, as well as the location(s) and species of replacement trees, would be determined by Council's Infrastructure and Assets department in accordance with the typical process.
- 7.6.6 The applicant has provided an alternative possible access point and ground level configuration for consideration (refer **Attachment 45**), though it is noted that this access arrangement does not appear to resolve conflict with the street tree due to the necessity that conflict with an existing stobie pole also be avoided. While Council staff are supportive of the applicant's intent in providing this alternative access possibility, it is recommended that the access as proposed best balances competing street infrastructure demands.
- 7.6.7 Accordingly, the proposed access point to the site is considered to provide safe and convenient traffic movement.

7.7 Energy Conservation Measures

- 7.7.1 It is desired that all dwellings provide adequate thermal comfort for occupants through passive design features such as orientation of windows, living areas and private open space, and cross-ventilation (Council-wide PDC 79).
- 7.7.2 Dwelling windows would be predominantly north and south facing, with no west facing windows present to any dwelling. Living and private open space areas would be north or east facing, with south facing walkways proposed to provide access to each dwelling. Living room windows would be suitably shaded from northern sun by overhangs formed by each level. Ceiling heights are sufficient to allow the installation of ceiling fans to maximise cross ventilation opportunities.
- 7.7.3 Accordingly, the building design would incorporate features to provide adequate thermal comfort to occupants which should not impact on adjoining properties.

7.8 Noise Attenuation

- 7.8.1 It is anticipated that noise and air quality impacts should be mitigated through appropriate building design and orientation (UCZ Objective 1). Residential development on sites abutting roads with traffic volumes exceeding 3000 vehicles per day should be sited and designed to reduce the impact of traffic noise on occupants. Further, residential buildings should feature adequate separation between the habitable room windows and balconies of other buildings (Council-wide PDCs 111 and 161).
- 7.8.2 In addition to the above, the subject land is identified with Map Pr/1 (Overlay 5) for the purpose of noise and air emissions. It is outlined by PDC 1 of the Noise and Air Emissions Overlay that sensitive development located adjacent to high noise and/or air pollution sources should be additionally protected from these additional potential impacts.

7.8.3 It is also desirable that attached dwellings are designed to minimise the transmission of sound between dwellings, particularly between living areas and bedrooms (Council-wide PDC 93). It is noted that for this proposal the layout of dwellings is such that bedrooms would be separated from the living or kitchen areas of adjoining dwellings, which is a desirable outcome.

7.8.4 The construction of the building would need to be undertaken in accordance with the Ministers Specification SA78B – Construction requirements for the control of external sound. Compliance with the Minister's Specification would be required as part of the Building Code of Australia (BCA). Given the layout of the dwellings and the lack of west facing door and window openings, it is anticipated that compliance with the Minister's Specification would suitably achieve compliance with relevant provisions of the Development Plan.

7.9 Affordable housing

7.9.1 Development within the Urban Corridor Zone which proposes 20 or more dwellings should have a minimum of 15% affordable housing (Affordable Housing Overlay PDC 1). The proposal is for 16 dwellings and as such the affordable housing provision does not apply.

7.10 Car and Bicycle Parking

7.10.1 Within the Urban Corridor Zone, it is anticipated that the provision of car and bicycle parking would be in accordance with Tables Pr/5 and Pr/6 of Council's Development Plan.

7.10.2 Table Pr/6 suggests that one bicycle park should be provided for every four dwellings, and one bicycle park should be provided per visitor for every ten dwellings. Therefore 6 bicycle parks should be provided within the development. No bicycle parking is presently described by the proposal plans, though two scooter parking spaces are shown adjacent to the car park roller door.

7.10.3 Table Pr/5 suggests that one car parking space is desired for each 1-2 bedroom dwelling and an additional 0.25 space is desired per dwelling for visitor parking. Consequently, the anticipated car parking rate for the 16 dwellings would be 20 car parking spaces, comprised of 16 for occupants and 4 for visitor parking.

7.10.4 Additionally, Table Pr/5 suggests that 3-5 car parking spaces per 100m² of gross leasable floor area should be provided on-site for non-residential land uses. Therefore one additional car parking space should be provided.

7.10.5 The proposal would accommodate a total of 16 car parks, providing a single car park for each dwelling. It is anticipated that further bicycle parking can be provided with minor alterations to the ground floor configuration and it is therefore not inappropriate that this be resolved by way of an imposed condition.

7.10.6 In his report (refer **Attachments 32-45**), Mr Weaver considers that it is appropriate to apply provisions of the Development Plan that allow lesser car parking rates than those described above. Mr Weaver concludes that due to the site's proximity to the Churchill Road bus network and the availability of nearby visitor parking outside of peak weekday periods that the provision of 16 parking spaces would appropriately cater to anticipated on site demand.

7.10.7 It is therefore considered that the proposed development would provide a sufficient number of car parks for dwelling occupants.

7.11 Overshadowing

- 7.11.1 Generally, the design and location of buildings should enable direct winter sunlight into adjacent dwellings and private open space areas while minimising the overshadowing of windows of main internal living areas, upper-level private balconies that provide the primary open space area for a dwelling and solar collectors (Council-wide PDC 138).
- 7.11.2 The subject site, along with properties directly north, south and west of the subject site, are identified to be developed at a greater intensity than that of the existing built form. As Redin Street separates the subject site from its southern neighbour, overshadowing impacts are minimised.
- 7.11.3 It is noted that some late afternoon shadow will be cast over the adjoining residence at 82 Redin Street. As the garage and driveway of this dwelling abuts the subject site, it is anticipated that overshadowing will be of relatively low impact to the occupier. Given this, the overshadowing impact would be relatively minor, but in any event consistent with that expected for new development in the Urban Corridor Zone.

7.12 Visual Privacy

- 7.12.1 It is anticipated that a variety of measures should be used to minimise direct overlooking into adjacent internal living and private open space areas. Such measures should be integrated into the overall building design and should have minimal negative effect on the amenity enjoyed by the occupants of neighbouring dwellings (Council-wide PDC 139).
- 7.12.2 It is noted that while it is desirable that overlooking be prevented in most developments (refer Council-wide PDC 90), for buildings greater than three storeys height within the Urban Corridor Zone it is desirable that overlooking be minimised (refer Council-wide PDC 139 and UCZ DCS).
- 7.12.3 The applicant proposes a two-level approach to privacy screening on northern balconies. Floor to ceiling horizontal louvres are proposed that would screen approximately half of each north facing balcony, while the remaining half would be screened by a copper/rust finished metal balustrade of 1.1m height. The same 1.1m height metal balustrade is proposed to east facing balconies, while south facing courtyards would be screened by a 1.8m height metal balustrade. Vertical timber screening would be used to south facing walkways which would address passive surveillance and privacy outcomes in a balanced manner.
- 7.12.4 With advice from the Government Architect and Design Review Panel members, the DAP has previously expressed that 1.5m balustrade and window screening balances well the competing issues of occupant amenity and visual privacy. It is therefore recommended that if the application is supported, a condition be imposed to the effect that the north and east facing metal screening devices are of a 1.5m effective height.

7.13 Landscaping

- 7.13.1 It is anticipated that landscaping would be provided in the form of low-lying shrubs and grass plantings, together with trees that have relatively clean trunks and high canopies to provide visual softening of the built form and reflect the scale of landscaping in the public realm (BA PA Desired Character Statement).

- 7.13.2 The location of the building on the site would enable limited landscaping to be provided to street frontages. It is noted that mature Council street trees in the Redin Street verge would be complementary to the pedestrian scale plantings proposed on site, though it is also noted that one of these trees would be removed (and replaced) due to the establishment of the crossover.
- 7.13.3 It is noted that DPTI have expressed a desire that Council impose a condition requiring all landscaping along the Churchill Road and Redin Street site frontages to be low growing to preserve sightlines around the intersection and to the internal driveway. It is thus acknowledged that the taller plantings typically desired within the policy area along street frontages would be limited in relation to this site irrespective of the building footprint.
- 7.13.4 While species selections for plantings are not presently nominated, it is anticipated that species selections could occur appropriately at a later date by way of a reserved matter for plantings of this scale.

7.14 Stormwater Management

- 7.14.1 The provisions of Council's Development Plan suggest that site drainage should be designed to safely direct surplus flows to a public street without causing harm to adjoining properties (Council-wide PDC 97) and that all proposed developments should be designed to retain as much stormwater as possible, minimising the overflow to the kerb and water table (Council-wide PDC 98).
- 7.14.2 The applicant has not yet provided finished floor level details or a stormwater management plan, though it is noted that the substantial footprint of the building will result in the majority of stormwater being captured by the roof of the building. It is thus unlikely that earthworks will be required to suitably address stormwater management.
- 7.14.3 Given that the proposal will result in a notable increase in stormwater run-off from the subject land, it is appropriate that Council receive and assess such a plan. To this end, it is recommended that the consideration of the stormwater design be reserved for further assessment and approval by Council.

7.15 Waste Management

- 7.15.1 Council's Development Plan outlines that new development incorporates opportunities for minimising waste and enable waste management options that provide adequate storage while screening these areas from public view (Council-wide PDC 147).
- 7.15.2 Additionally, new developments should provide a dedicated area for the on-site storage, collection and sorting of recyclable materials and waste that is easily and safely accessible to the collection point, well screened, and designed to reduce odours and other external impacts (Council-wide PDC 170).
- 7.15.3 Pursuant to the *Zero Waste South Australian Better Practice Guide – Waste Management in Residential or Mixed Use Developments* guidelines, anticipated waste demand is calculated with reference to the 32 bedrooms within the development.
- 7.15.4 General waste demand is anticipated at 35 litres of waste per room a week, providing a weekly total of 1,120 litres of waste. Recycling waste is anticipated at 30 litres per bedroom, providing a total of 960 litres weekly demand, and 20 litres of green organics per bedroom would result in an anticipated demand capacity of 640 litres each week.

- 7.15.5 The ground floor plan displays the storage of four 660L Mobile Garbage Bins (MGBs) for general and recyclable waste and three standard 240L bins for green organic waste. Collection is proposed to occur via Churchill Road, with access to the service area via sliding door. It is anticipated that the contractor would remove the bin from the compound and return it following collection, though the community corporation would retain responsibility for ensuring that the waste storage area is appropriately maintained.
- 7.15.6 The proposed waste management approach is considered to be generally appropriate, subject to conditions reinforcing the maintenance of the waste storage area.

8. CONCLUSION AND RECOMMENDATION

- 8.1 The proposal seeks to establish a high density residential land use on the subject land. The building would be four storeys in height as anticipated within the Boulevard Policy Area.
- 8.2 The proposal would achieve the desired density, provide adequate private open space, storage facilities, waste collection, car parking and passive surveillance in accordance with the development plan provisions. The building would generally achieve desirable setbacks, and uses window openings and lower level landscaping to create a pedestrian scale building interface. The internal dwelling layout would provide functional living spaces for occupants, while the external appearance would be well articulated and incorporate design features and material variations that break down the scale of the building well and provide significant visual interest.
- 8.3 The application is therefore considered to be relatively consistent with the relevant provisions of the Prospect (City) Development Plan and warrants the granting of development plan consent, subject to appropriate conditions.

It is recommended:

That with reference to the relevant provisions of the Prospect (City) Development Plan, the zoning of the land within which the proposed development is situated and the locality within which the land is situated, the Panel resolves that development application 050/344/2015 is not seriously at variance with the Development Plan and as such a decision shall be made on the merits of the application; and

That pursuant to the *Development Act 1993*, as amended, Development Plan Consent be approved to DA 050/344/2015 from Loucas Zahos for a Four Storey Mixed Use Development comprising 16 dwellings and Ground Level Office, Car Parking and Landscaping at 253 Churchill Road, Prospect (CT 5684/552), subject to the following reserved matters, conditions and notes:

Reserved Matters:

1. A detailed stormwater management plan shall be provided that, to the satisfaction of Council, provides evidence that all dwellings are suitably protected from 1 in 100 year ARI storm events and that post-development outflow rates from the site will match pre-development rates in 1 in 20 ARI storm events. The location and capacity of any on-site detention tanks shall be clearly described.
2. A detailed landscaping design including species selections and suitable management techniques.

Conditions:

1. The development shall take place in accordance with plans and details stamped by Council relating to Development Application Number 050/344/2015, except as modified by any conditions detailed herein. All works detailed in the approved plans and required by conditions are to be completed prior to the occupation of the approved development.
2. All driveways, parking and manoeuvring areas must be formed, surfaced with concrete, bitumen or paving and maintained to the reasonable satisfaction of Council. Driveways, car parking spaces, manoeuvring areas and landscaping areas shall not be used for the storage or display of materials or goods including waste products and refuse. The obsolete crossover and/or any portion of crossover that is not required for the subject development shall be reinstated to Council standard kerb and gutter at the applicant's cost prior to occupation of the completed development.
3. The paving located at the front of the site adjacent Churchill Road, shall be re-laid to distinguish between the crossover and on-street parking areas. All work shall be completed to the reasonable satisfaction of Council and at the applicant's cost.
4. Prior to the grant of development approval, detailed plans of the north and east facing balcony privacy screens shall be provided to the satisfaction of Council and that shall result in the copper/rust finished metal screens being 1.5m in height above the finished floor level of the related balconies.
5. Air-conditioning units and solar hot water heaters shall be provided with screening devices designed to complement the colours, materials and finishes of the building approved herein, and shall be sited to adequately screen the units from view from neighbouring properties and public land (roadways) to the reasonable satisfaction of Council.
6. The Community Corporation shall ensure that the waste storage area is cleaned and maintained to the satisfaction of Council. General, recyclable and green organic wastes shall be co-mingled, with the Community Corporation maintaining responsibility for ensuring that bins are transported between the collection point and the storage area in a timely fashion to the satisfaction of Council.
7. A minimum of 4 x 660 litre mobile garbage bins shall be provided for general and recyclable waste and collected at least once per week. A minimum of 3 x 240 litre mobile garbage bins shall be provided for the collection of green organic waste and collected at least once per week. Collection of the waste shall occur outside of peak traffic periods.
8. To maximise the efficiency of waste recycling:
 - a) Provision shall be made for the separation of recyclable materials for collection and recycling, including paper, cardboard, glass and plastic containers, tins, and any other plastic that 'holds its shape';
 - b) Separate provision shall be made for the collection of food waste (food organics) and food-contaminated cardboard, paper or paper products, which are to be collected for composting; and
 - c) Paper attached to plastic, wax paper or chemically-treated/gloss cardboard will not be included with the materials collected for composting.
9. Any difference in finished ground levels between the subject site and adjoining sites at the boundary shall be retained by an appropriate wall or plinth of masonry, concrete or similar construction. Retaining walls must be designed to accepted engineering standards and will not be of timber construction if retaining a difference in ground levels exceeding 200 mm.

10. The landscaping shall be planted prior to occupancy of the development, and maintained at all times to the reasonable satisfaction of Council and to ensure appropriate lines of sight for vehicles and pedestrians. The Community Corporation shall cultivate, tend and nurture the landscaping, and shall replace any landscaping that becomes diseased or dies. An automated drip irrigation or similar watering system shall be established and maintained to ensure that sufficient water is available to satisfy the needs of the landscaping species selected.
11. Footpaths adjacent to the site are to be kept in a safe condition for pedestrians at all times during construction works. All driveways and footpaths traversed by vehicles using the site are to be maintained in a reasonable condition for the duration of the works, and are to be reinstated to the satisfaction of Council on completion of the works.

All works on Council land shall be conducted to Council's specification, with all works to be bunted off safely and pedestrian safety to be maintained throughout the construction period. Plantings will also need to be undertaken in line with council specifications in terms of sight distance interference and safety to the community (thorns/poisonous plantings). Plans displaying all relevant details of the Road/Kerbing/Footpath Works shall be submitted to the Assets and Infrastructure Officer for approval prior to the commencement of any such works.

The following conditions have been imposed by the Department of Planning, Transport and Infrastructure in accordance with Section 37(7) of the Development Act 1993:

1. The access point to Redin Street shall be designed in accordance with Loucas Zahos Architects Ground, Site & Level 1 Plan, Drawing A104, Revision P4, dated 06/11/15.
2. The access shall be appropriately flared to the road to facilitate unimpeded ingress/egress.
3. Appropriate signage and line marking shall be installed to reinforce the desired traffic flow at the Redin Street access point.
4. The shared internal vehicle manoeuvring areas shall be clear of all obstructions including meters, letterboxes and bicycle/scooter parking.
5. All landscaping located along the side boundaries forward of the building and along the Churchill Road boundary shall be low growing to preserve sightlines across the corner.
6. Signage associated with the development that is visible from Churchill Road shall not contain any element of LED or LCD display that is viewable from the adjacent/nearby roads.
7. Signage upon the site that is visible from Churchill Road shall not contain any element that flashes, scrolls, moves or changes.
8. Signage associated with the development shall be finished in a material of low reflectivity to minimise the risk of sun/headlamp glare that may affect motorists' perception of the road.
9. All stormwater generated by the proposal shall be appropriately collected and disposed of without entering or jeopardising the safety of the adjacent arterial road network.

Advisory Notes:

- (1) Pursuant to Section 86(1)(a) of the Development Act, 1993, you have the right of appeal to the Environment, Resources and Development Court against either 1) a refusal of consent or 2) any condition(s) which have been imposed on a consent. Any such appeal must be lodged with the Court within two (2) months from the day on which you receive this notification or such longer period as may be allowed by the Court.

The Environment, Resources and Development Court is located in the Sir Samuel Way Building, Victoria Square, Adelaide SA 5000 (Postal Address: GPO Box 2465, Adelaide SA 5001).

- (2) The development plan consent granted herein is effective for a period of twelve (12) months from the date of the decision. Unless Council extends this period, building rules consent is required within this time or the consent will lapse.

Any request for an extension of the operative period of the consent must be submitted to Council in writing, accompanied by the applicable fee.

- (3) Further application pursuant to the Local Government Act shall be made to the Infrastructure Assets and Environment Department for the proposed crossover prior to construction activities occurring.

Road/Kerbing/Footpath Works will need to be inspected by an Assets and Infrastructure Officer to determine they have met all relevant requirements. All work including line marking will be the responsibility of the applicant as will the reinstatement of any damaged Infrastructure / Services related to these works. All works will be carried out at the cost to the applicant.

- (4) Prior to the commencement of construction of the development herein approved, it is strongly recommended that you employ the services of a licensed Land Surveyor to carry out an identification survey of the subject land and to peg the true boundaries, to ensure that building work will be either on the true boundaries or the specified distance from the true boundaries of the subject land, as the case may be.

Failure to correctly site the development on the land in accordance with the plans approved herein would constitute a breach of the *Development Act 1993*. Any amendments required to the approved plans as a result of the survey are to be submitted to Council for approval prior to works commencing.

- (5) You are encouraged to consult with adjoining property owners before commencing any work, to assist in minimising nuisance or inconvenience caused during construction.

- (6) You are required to give formal notification to, and consult with, the adjoining property owner if you are removing, replacing or altering an existing fence or building a freestanding wall along the common boundary that would, for all purposes, be a dividing fence (Section 5 of the *Fences Act 1975*).

- (7) During construction of the development approved herein, measures will be implemented to ensure that the construction works do not result in an unreasonable impact on occupiers of adjacent properties or pollution of existing infrastructure through drag-out or stormwater runoff. Measures shall include as necessary:

- A hard surface and controlled washing zone at the entry/exit points to the site, designed to reduce the potential for mud and material dragged out by construction vehicles; and
- Containment of stormwater run-off within the site, which if being discharged into the stormwater system will be filtered to the satisfaction of Council; and
- Reduction of the potential for dust and other airborne particles by the use of water sprinklers and/or other means of containment; and

- The establishment of an appropriate storage compound for waste materials and litter. No building waste material shall be stored outside of the storage compound or similar industrial bin; and
 - All mechanical equipment shall be used in a manner to minimise the potential for noise pollution and ensure compliance with the requirements of the Environment Protection (Noise) Policy.
- (8) To ensure compliance with applicable standards as described in the Environment Protection (Noise) Policy established under the Environment Protection Act, construction activities should only take place between the hours of 7:00am and 7:00pm, Monday to Saturday inclusive, and not on Sundays or public holidays.
- (9) The construction of the building shall be undertaken in accordance with the Ministers Specification SA78B – Construction requirements for the control of external sound. Compliance with the Minister’s Specification would be required as part of the Building Code of Australia (BCA).
- (10) Occupants/owners of the dwellings should be advised that the crossover/driveway is likely to be blocked by buses stopping at the adjacent bus stop and that the adjacent bus stop cannot be relocated.
- (11) All vehicles shall enter and exit the site in a forward direction.
- (12) The Metropolitan Adelaide Road Widening Plan (MARWP) shows a possible requirement for a strip of land up to 4.5 metres in width from the Churchill Road frontage of this site and a 4.5 metres x 4.5 metres corner cut-off at the junction of Churchill Road with Redin Street. It is noted that CT 5684/552 shows that a 3.04 metre x 3.04 metre corner cut-off has already been provided. No further land is required at this time.

Notwithstanding this, the consent of the Commissioner of Highways under the MARWP Act is required to all building works on or within 6.0 metres of the corner cut-off requirement. Accordingly, the attached consent form and three copies of the approved site plans must be provided to DPTI for consent purposes.



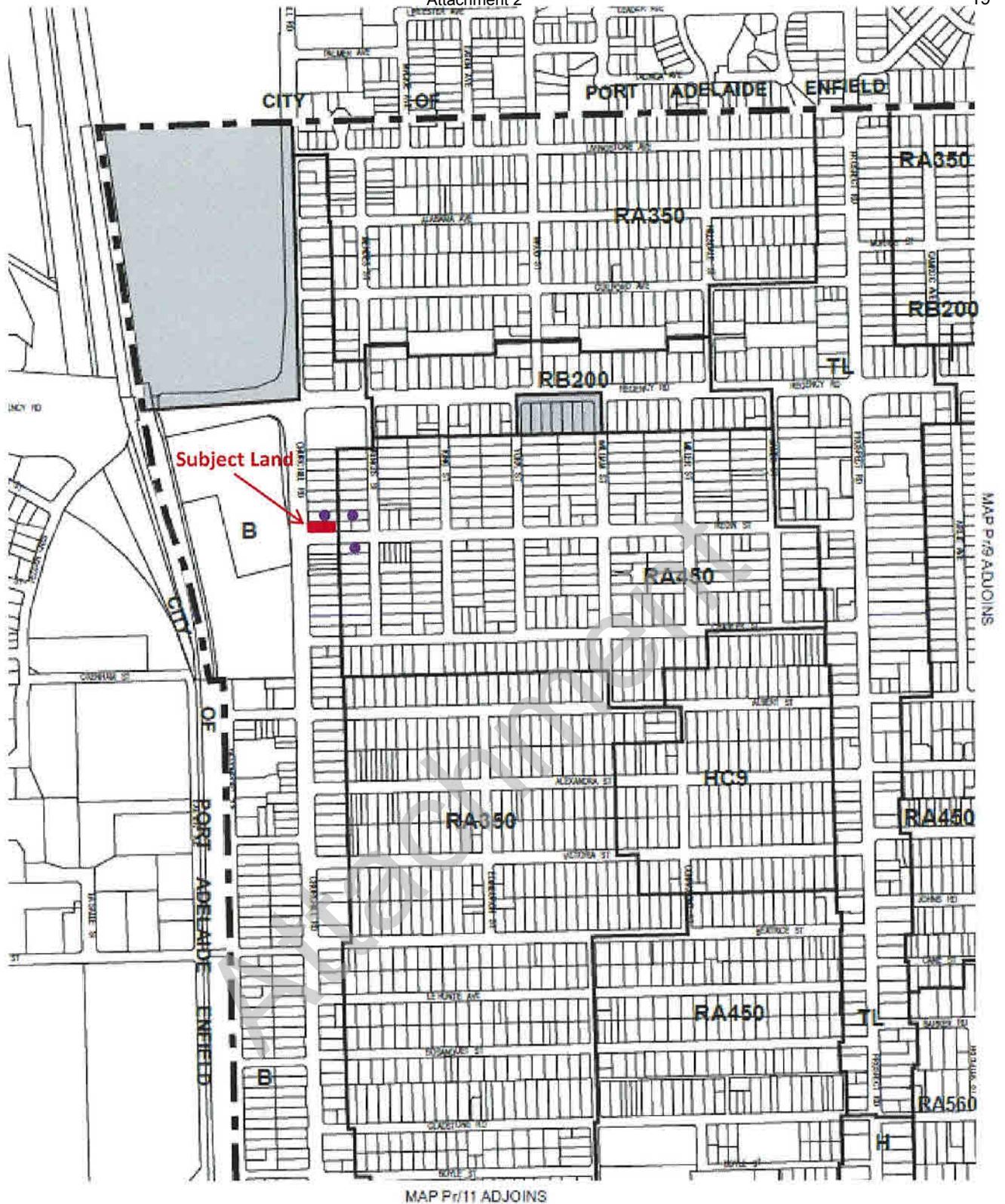
**CITY OF PROSPECT
Development Services**

128 Prospect Road, Prospect SA 5082
Telephone (08) 8269 5355 Facsimile (08) 8269 5834

Development Application Form

Application no.: 050 / 2015 / 1344 (Office Use Only)

1. Application Type (select one)			
<input checked="" type="checkbox"/> Development Plan Consent only	<input type="checkbox"/> Building Rules Consent only	<input type="checkbox"/> Full Development Approval	
2. Location of Proposed Development			
Unit #:	House #: 253	Street: Churchill Road	
Suburb: Prospect		Postcode: 5082	CT Volume & Folio:
3. Details of Parties			
Applicant			
Name: Loucas Zahos Architects		Address:	
Contact No.:		Email:	
Owner <input type="checkbox"/> as applicant			
Name: Jenny and Dan Zarkovic		Address:	
Contact No.:		Email:	
Builder <input type="checkbox"/> owner builder <input type="checkbox"/> as applicant <input checked="" type="checkbox"/> to be advised <input type="checkbox"/> other			
Name:		Address:	
Contact No.:		Email:	
Builders Licence Number:			
4. Proposal Details			
Description of proposal: Proposed 3 Level Apartment Building and Ground Floor Carpark + Entry			
Existing use of property: Commercial Tenancy			31 AUG 2015
Estimated cost of development: \$ 3,200,000			
5. Declarations			
• Building Rules Classification sought: 2		Present Class: 1586	
• Proposed number of employees (for Class 5, 6, 7, 8 or 9):			
• Proposed number of persons for whom accommodation is provided (Class 9a only):			
• Proposed number of occupants on the premises (Class 9b only):			
• Does Schedule 21 or Schedule 22 of the <i>Development Regulations 2008</i> apply (activities of environmental or major environmental significance)? <input type="checkbox"/> yes <input checked="" type="checkbox"/> no			
• Has the Construction Industry Training Board (CITB) levy been paid? <input type="checkbox"/> yes <input checked="" type="checkbox"/> no			
I acknowledge that Council may make copies of this application and documentation in accordance with the <i>Development Regulations 2008</i> and <i>Development Act 1993</i> . Details forming part of the application may be included in a Development Assessment Panel agenda published on Council's website.			
If published, I request that Council obscures my telephone number and email address. <input checked="" type="checkbox"/> yes <input type="checkbox"/> no			
I have sought permission from the architect/engineer to allow reproduction of the application documents for provision to third parties. <input type="checkbox"/> yes <input type="checkbox"/> no			
Name: MICHAEL LOUCAS		Signature:	
<input checked="" type="checkbox"/> Applicant <input type="checkbox"/> Owner <input type="checkbox"/> Builder		Date: 20/07/15	



Subject Land

● Representors

Scale 1:8000



- RA560 Residential Policy Area A850
- RA450 Residential Policy Area A450
- RA350 Residential Policy Area A350
- RB200 Residential Policy Area B200
- TL Transit Living Policy Area
- B Boulevard Policy Area
- H High Street Policy Area
- HC9 Historic Conservation Area 9 Policy Area

- Policy Area Boundary
- Development Plan Boundary
- Area not covered by Policy

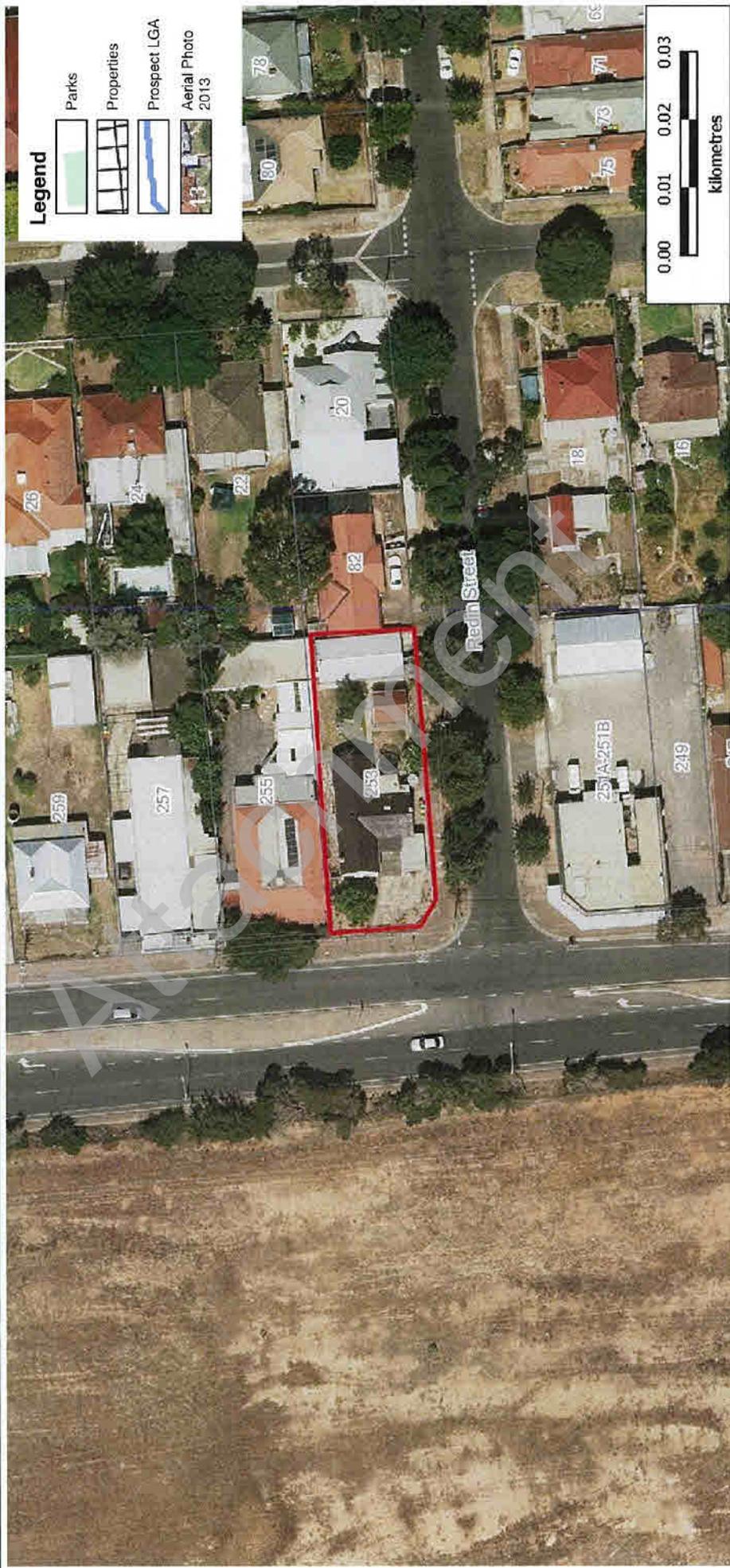


PROSPECT COUNCIL POLICY AREAS MAP Pr/8



Civic Centre
 128 Prospect Road
 Prospect SA 5082 AUSTRALIA
 Telephone: 08 8269 5355
 Email: admin@prospect.sa.gov.au

Subject Land



Legend

- Parks
- Properties
- Prospect LGA
- Aerial Photo 2013

0.00 0.01 0.02 0.03
 kilometres

Notes

253 Churchhill Road, Prospect

Disclaimer

This map is a representation of the information currently held by the City of Prospect. While every effort has been made to ensure the accuracy of the product, Council accepts no responsibility for any errors or omissions. Any feedback on omissions or errors would be appreciated.



Eastern boundary of subject land showing existing access and adjoining dwelling, facing north



Existing street trees and parking controls adjacent Redin Street boundary of subject land, facing north east

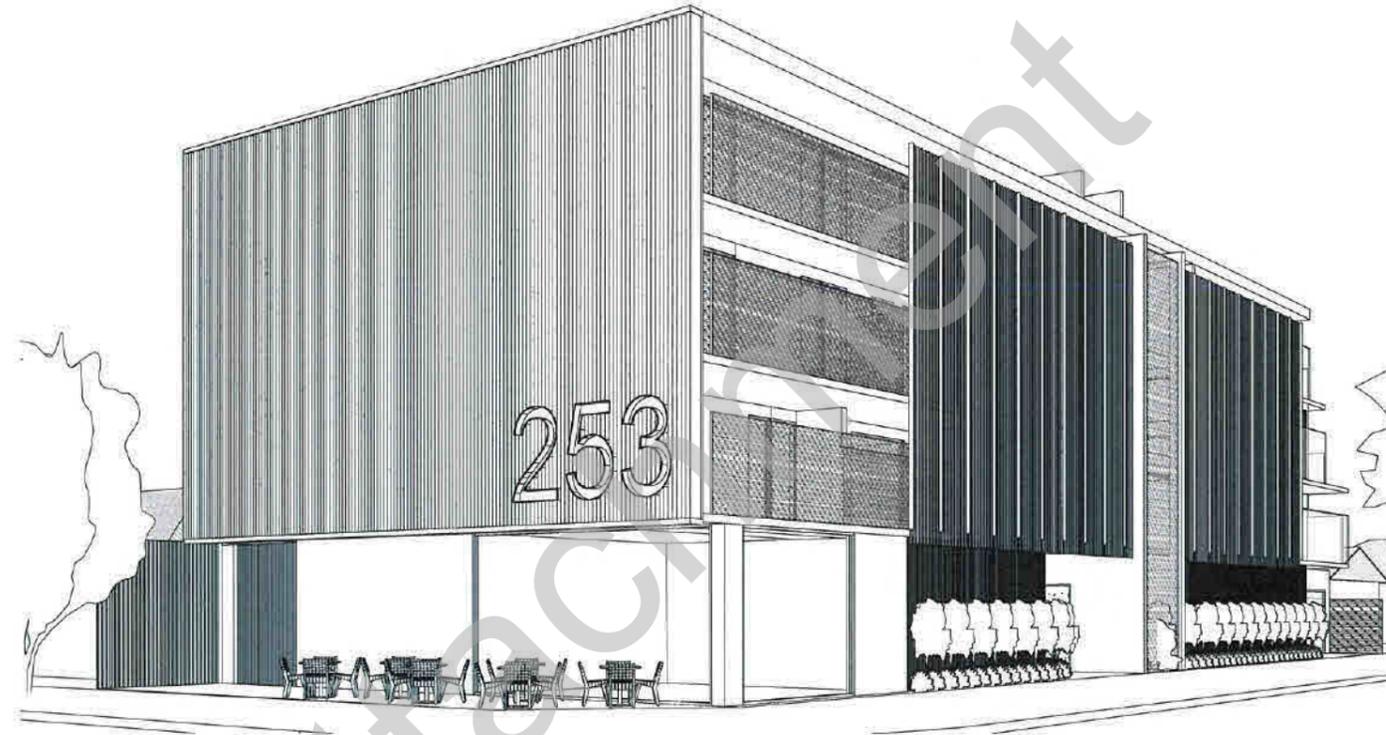


Subject land and Redin Street streetscape, facing north east



Subject land and adjoining dwelling at 255 Churchill Road, facing east

DEVELOPMENT SUMMARY	
SITE AREA	656.6m ²
DRIVEWAY EASEMENT	m ²
RETAIL	41.7m ²
CARPARK + ANCILLIARY	206.5m ²
RESI ENTRY, EXIT, LOBBY, STAIR & LIFT	264m ²
TOTAL NUMBER OF APARTMENTS	16
APARTMENT AREAS (NET TOTAL AREA - INCLUDES BALCONIES)	2102.7m²
APARTMENT TYPES	
APARTMENT TYPE 1 (2 BED)	15
APARTMENT TYPE 2 (1 BED)	1
BEDROOMS	31
APARTMENTS	16
CARPARKS	14
BICYCLE PARKS	5
APARTMENT STORAGE CABINETS (REQUIRED)	10m³ PER APART.
NUMBER OF FLOORS	
GROUND & CARPARK	1
APARTMENTS	3
TOTAL	4
GROSS FLOOR AREA / FLOOR (SQM)	
GROUND	611.7m ²
LEVEL 1 - APARTMENTS	673m ²
LEVEL 2 - APARTMENTS	470.7m ²
LEVEL 3 - APARTMENTS	447.3m ²
TOTAL	2102.7m²



DA DRAWING LIST			
Drawing No	Sheet Name	Date	Issue
A100	Title Page	26/08/15	P2
A101	Site Views	21/07/15	F1
A102	L2 Images	21/07/15	F1
A103	Inspirational Images	21/07/15	F1
A104	Ground, Site & Level 1 Plan	26/08/15	P2
A105	Level 2 & 3 Plan	26/08/15	P2
A106	Site & Location Plan	26/08/15	P2
A201	Elevations	26/08/15	P2
A202	Perspectives	26/08/15	P2
A203	Isometric Context Images	21/07/15	F1
A204	Perspective Context Images	26/08/15	P2
A301	Sections	26/08/15	P2



VIEW LOOKING SOUTH-EAST



VIEW LOOKING NORTH



VIEW LOOKING NORTH WEST



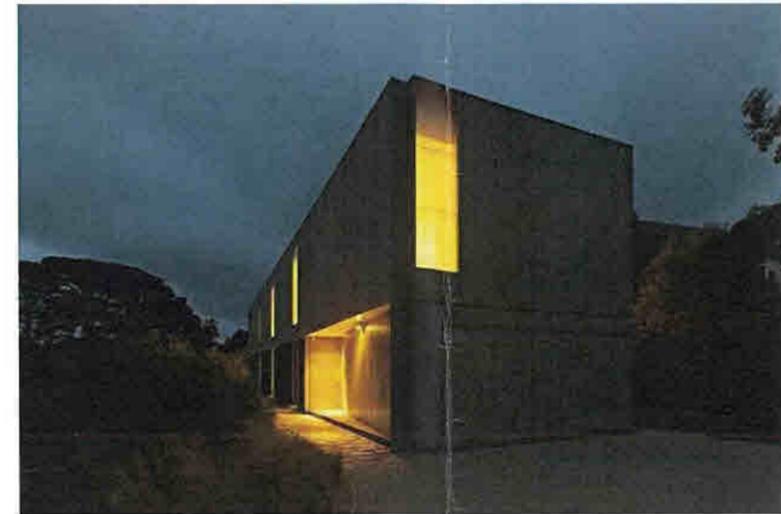
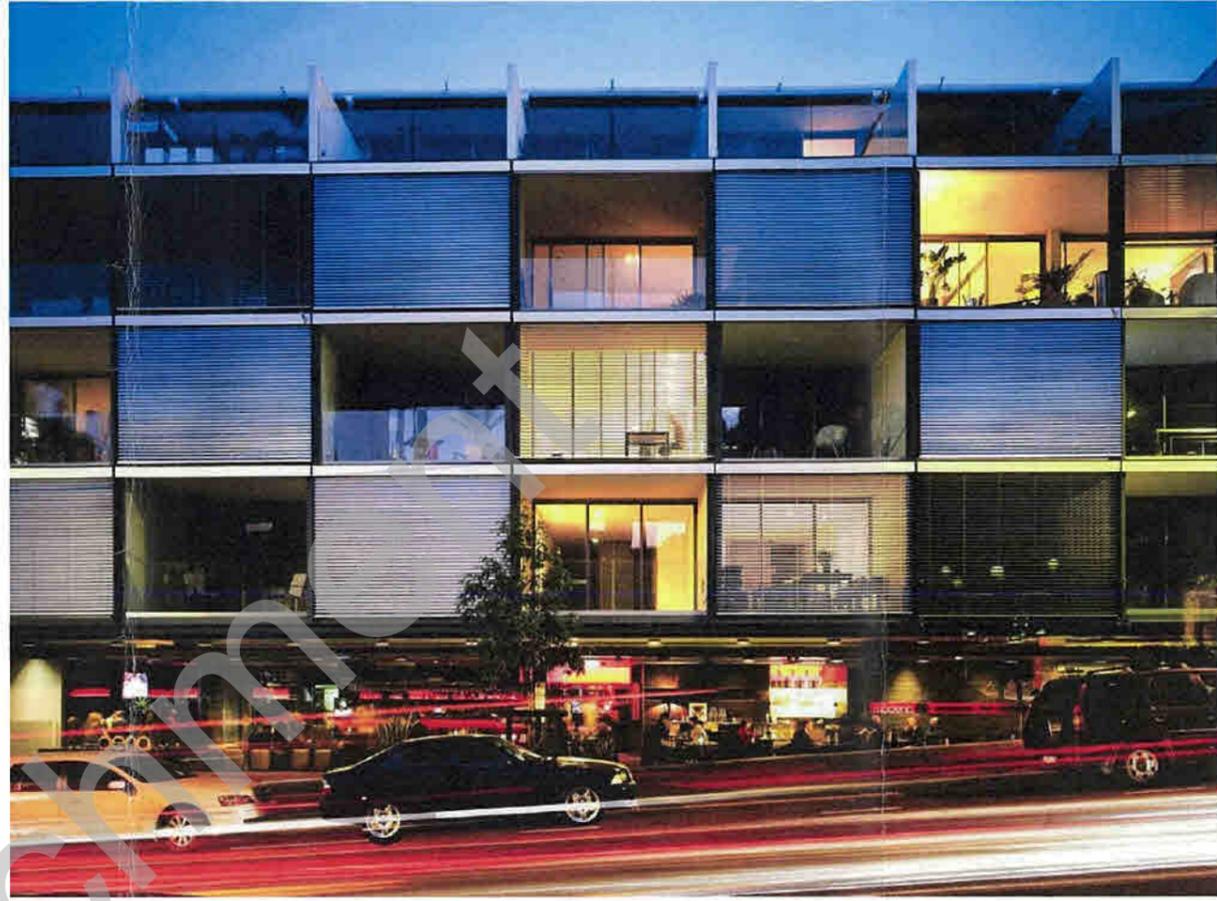
VIEW LOOKING SOUTH



- Street number cut into timber batten facade
- Polished copper lining
- LED ambient lighting to illuminate street numbers

1
A102 253 Street Number Illumination
Scale





GROUND FLOOR SCHEDULE

SITE AREA - 677m²
TOTAL CARPARKS 16 OFF

GROUND FLOOR

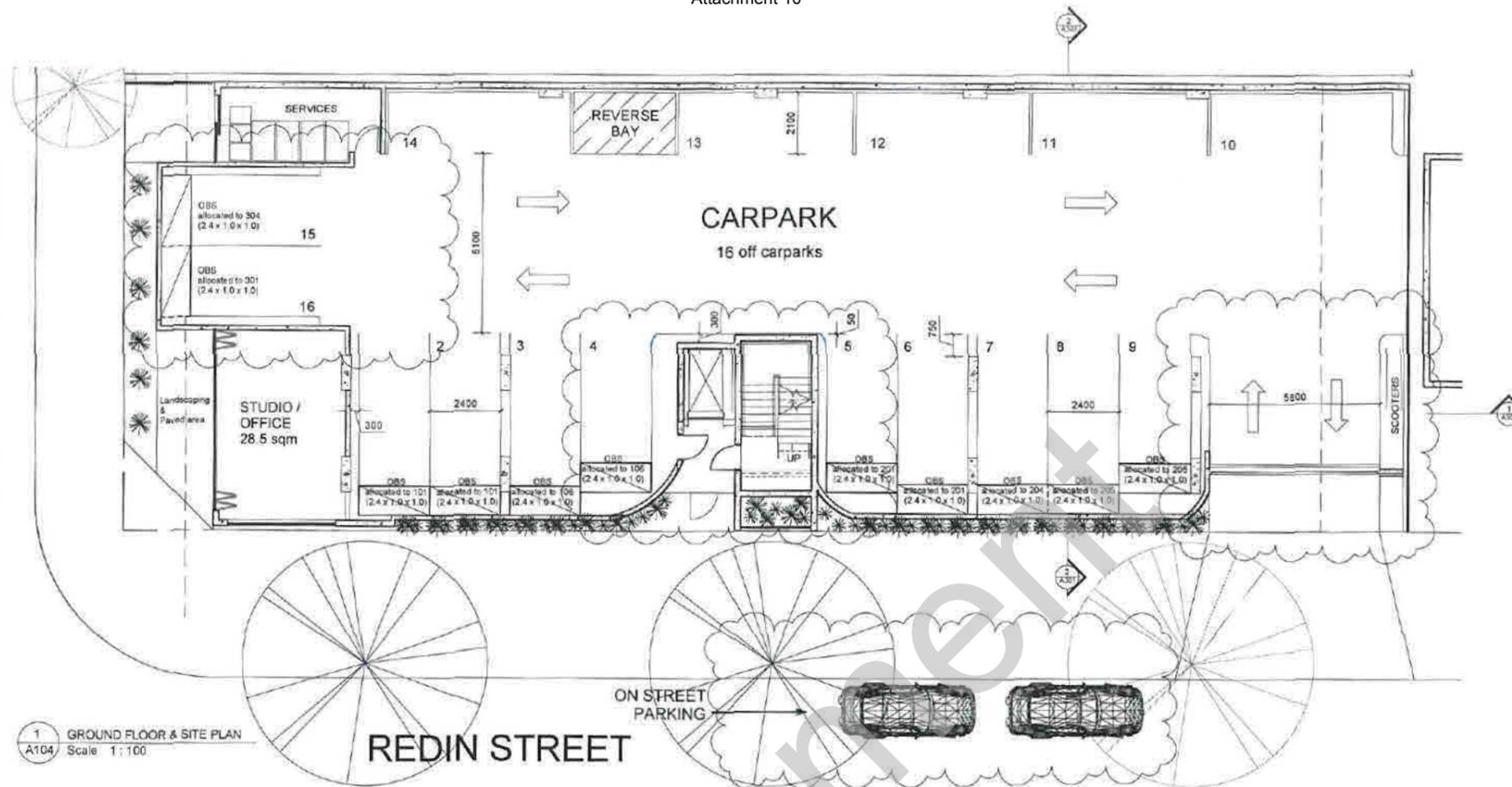
AREA SCHEDULE

CARPARK AND RESIDENTIAL ENTRY

LOBBY, LIFT + STAIR 30.6m²
SERVICES 19.9m²
CARPARK + ANCILLIARY 525.5m²
OBS - OVER BONNET STORAGE 26.40m³

GROSS FLOOR AREA (GROUND FLOOR) 617.7m²
TOTAL GROSS APT. AREA 2102.7m²

CHURCHILL ROAD



1 GROUND FLOOR & SITE PLAN
A104 Scale 1:100

AMENDED PLAN

APARTMENT SCHEDULE

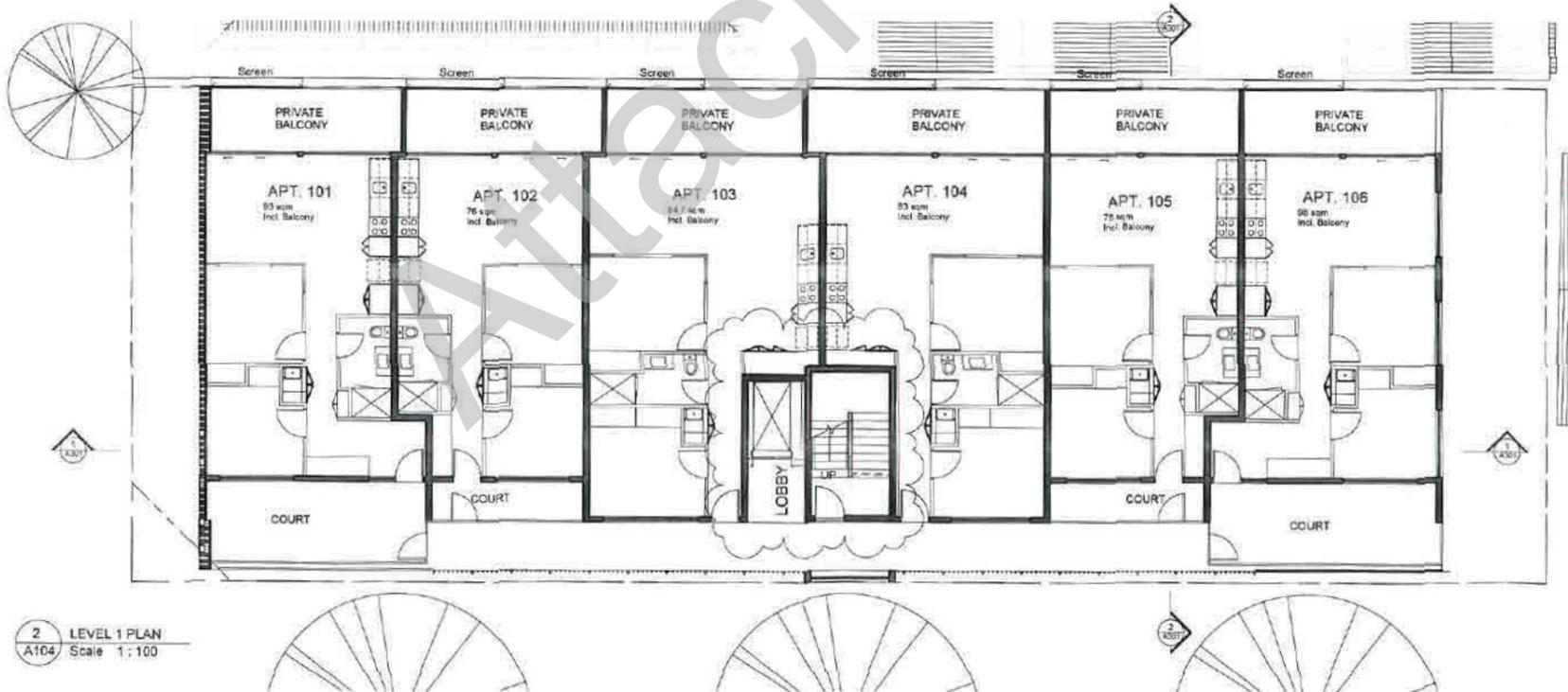
LEVEL 1 FLOOR: 6 UNITS
BUILDING TOTAL 16 OFF

APARTMENT LEVEL 1

AREA SCHEDULE

APARTMENTS (incl. balc) 512.4m²
LOBBY, LIFT & STAIR 21.3m²
WALKWAYS 37.5m²

GROSS FLOOR AREA (LEVEL 1) 571.2m²
TOTAL GROSS APT. AREA 2102.7m²



2 LEVEL 1 PLAN
A104 Scale 1:100

APARTMENT SCHEDULE

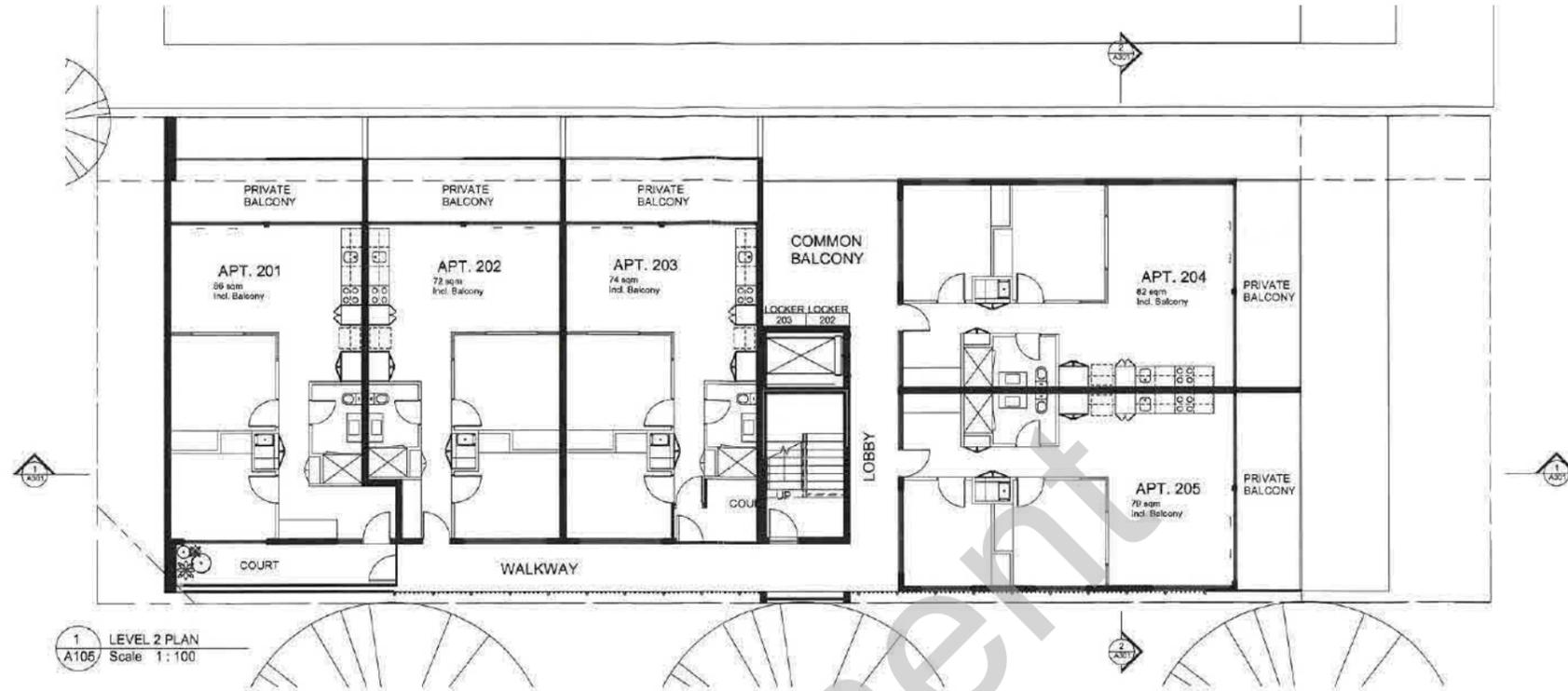
LEVEL 2 FLOOR 5 UNITS
BUILDING TOTAL 15 OFF

APARTMENT LEVEL 2

AREA SCHEDULE

APARTMENTS (incl. balc) 396.3m²
LOBBY, LIFT & STAIR 29.4m²
WALKWAYS 23.4m²
COMMON BALCONY 19.5m²

GROSS FLOOR AREA (LEVEL 1)	470.7m ²
TOTAL GROSS APT. AREA	2102.7m ²



APARTMENT SCHEDULE

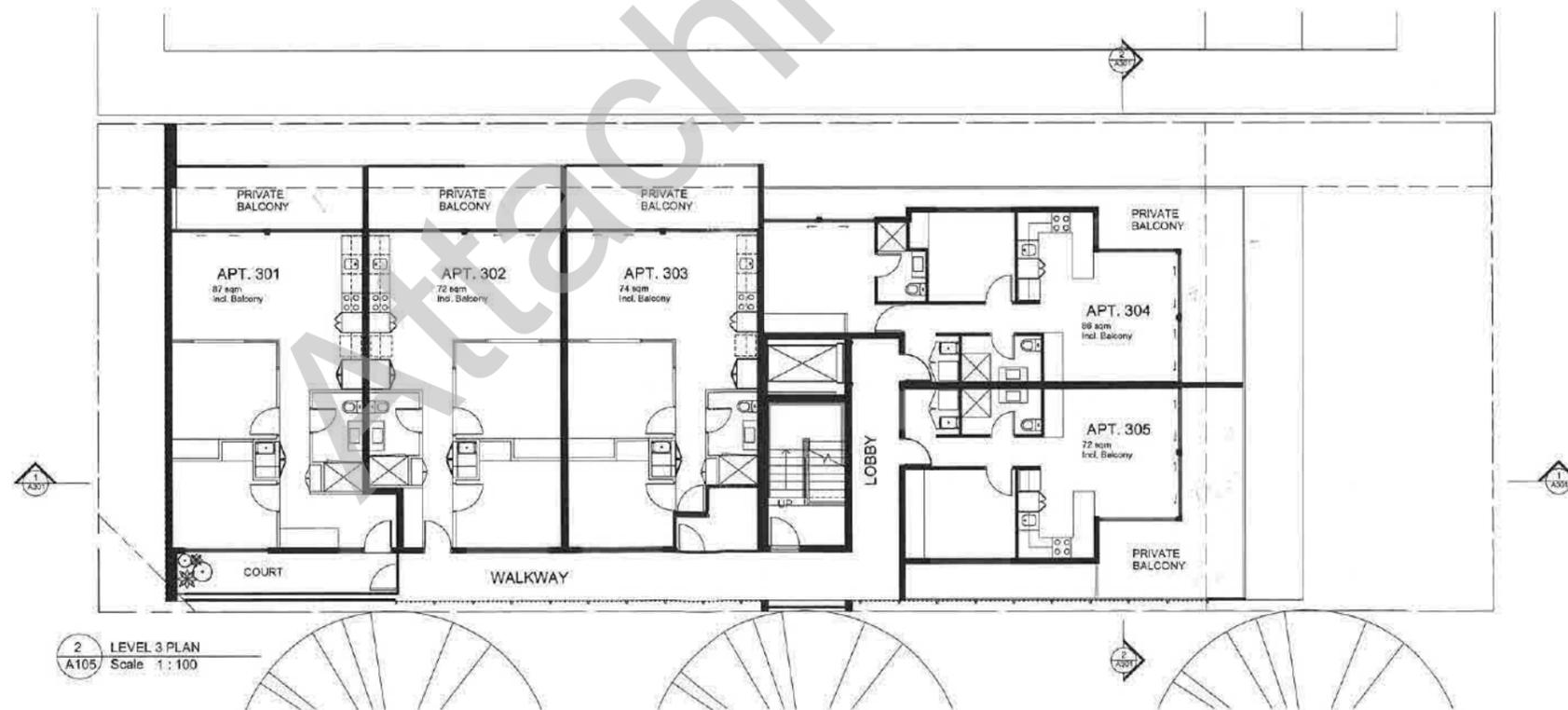
LEVEL 3 FLOOR 4 UNITS
BUILDING TOTAL 15 OFF

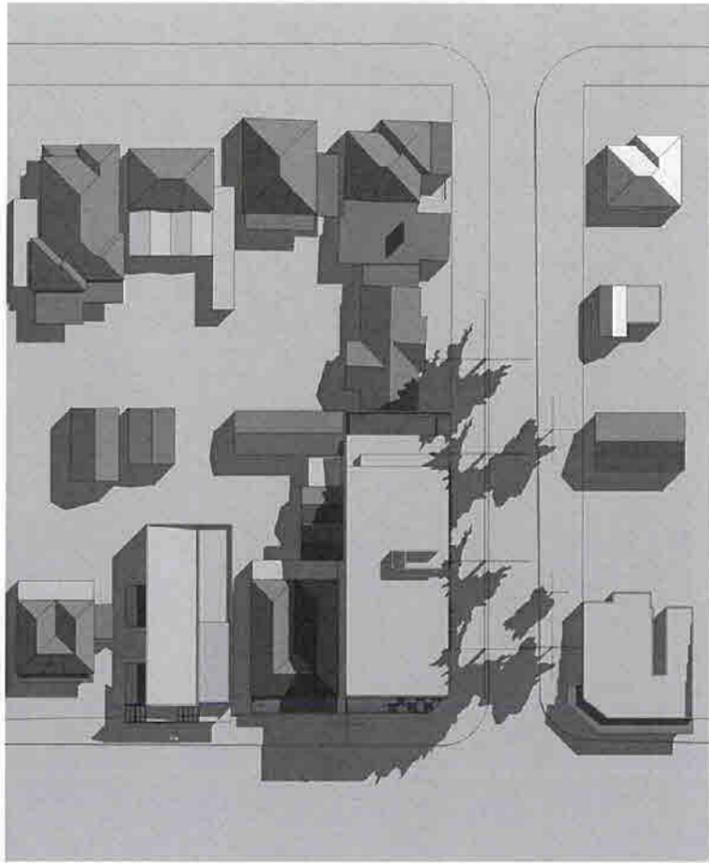
APARTMENT LEVEL 3

AREA SCHEDULE

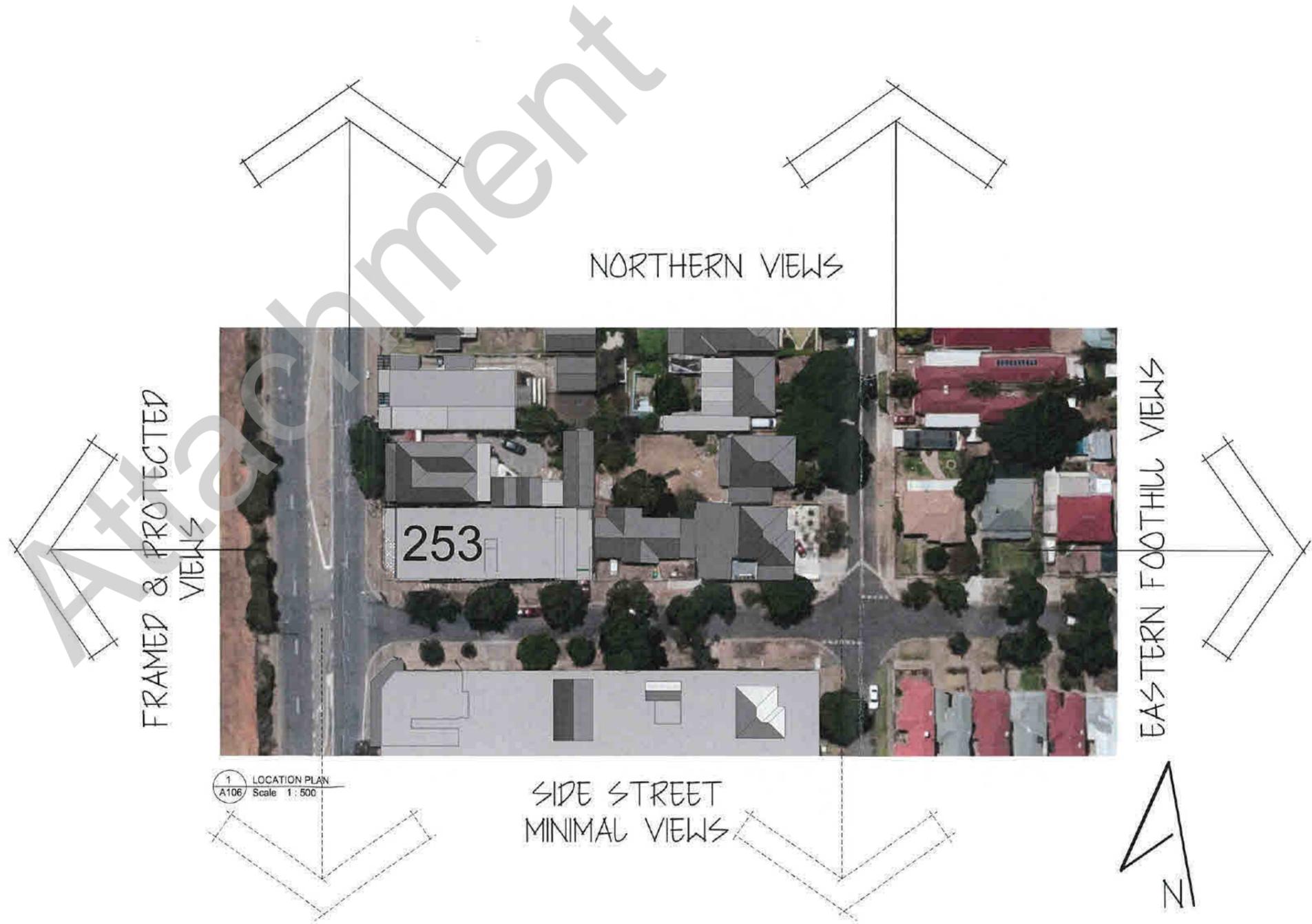
APARTMENTS (incl. balc) 373.1m²
LOBBY, LIFT & STAIR 29.3m²
WALKWAYS 24.8m²
COMMON BALCONY 19.4m²

GROSS FLOOR AREA (LEVEL 1)	447.3m ²
TOTAL GROSS APT. AREA	2102.7m ²



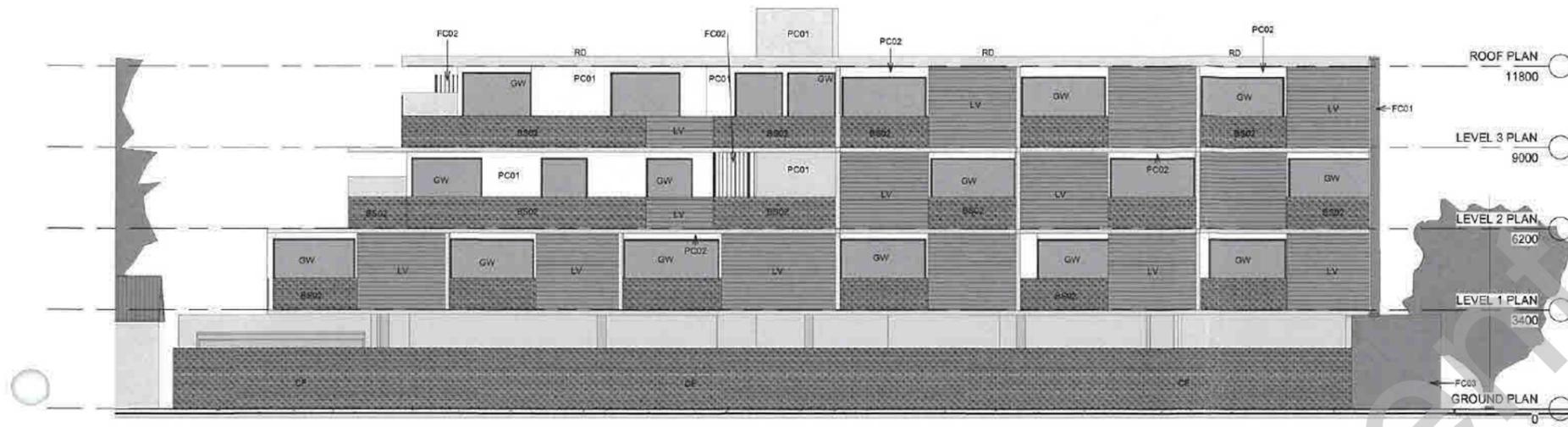


2 Site & Location
A106 Scale

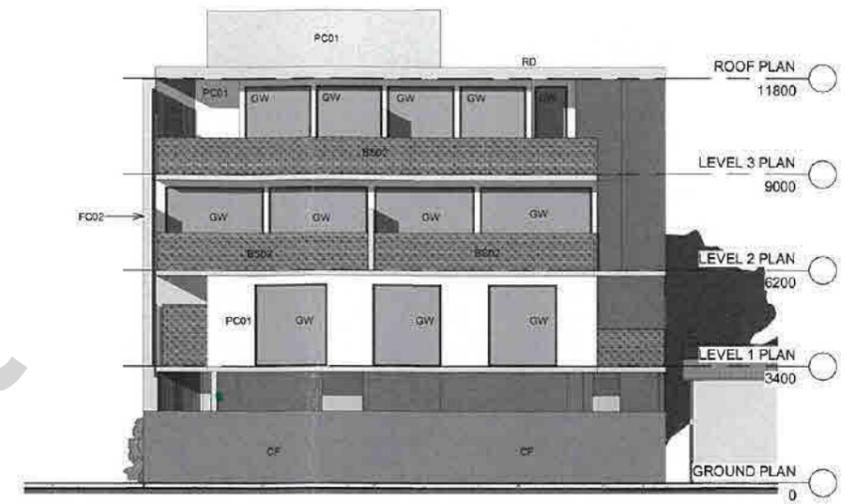


1 LOCATION PLAN
A106 Scale 1:500

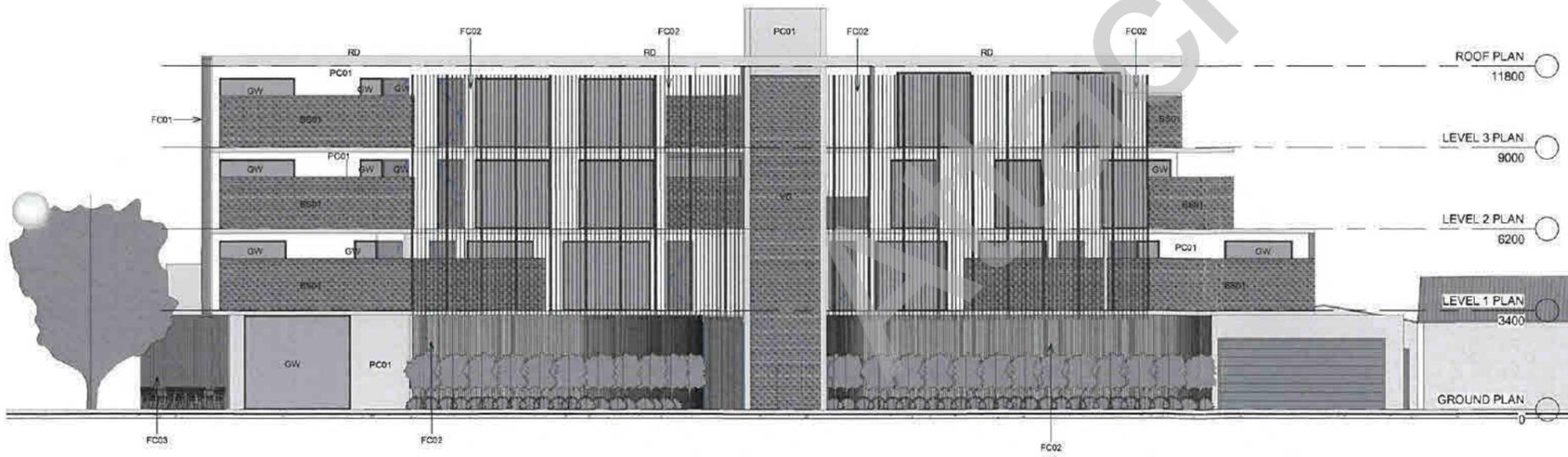
P1	21.07.15	DEVELOPMENT APP.	LL
P2			



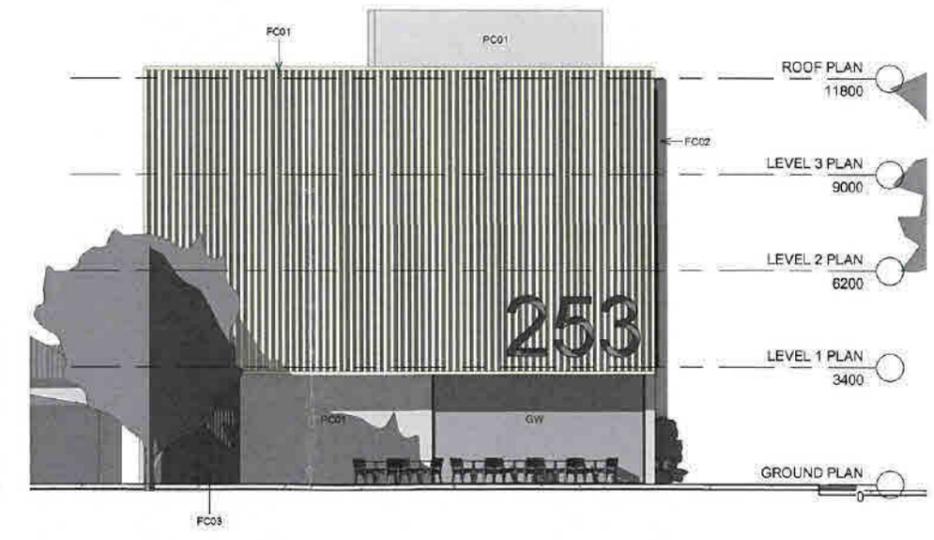
2 NORTH ELEVATION_DA
A201 Scale 1:100



1 EAST ELEVATION_DA
A201 Scale 1:100



3 SOUTH ELEVATION_DA
A201 Scale 1:100



4 WEST ELEVATION_DA
A201 Scale 1:100

MATERIALS LEGEND

- CF 2100mm high colorbond good neighbour fence
- GW Aluminium framed glass window assembly
- LV Weatherproof aluminium louvers
- BS01 1500mm metal balcony screen wall (Copper or rust finish)
- BS02 1100mm metal balcony screen wall (Copper or rust finish)
- CBS Over-bonnet storage
- PC01 Precast concrete wall - Natural finish
- PC02 Precast concrete wall - Dark/black oxide
- RD Colorbond metal roof deck
- FC01 Timber facade screen cladding
- FC02 Screenwood batten screen
- FC03 Merbau timber
- VG Vertical garden system



1 North East View
A202 Scale



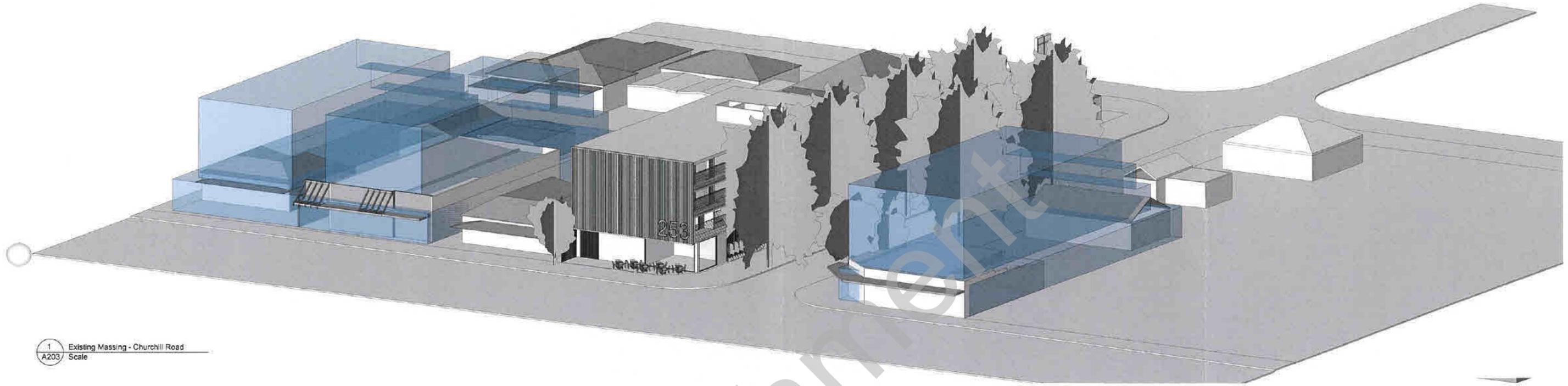
2 Northern View
A202 Scale



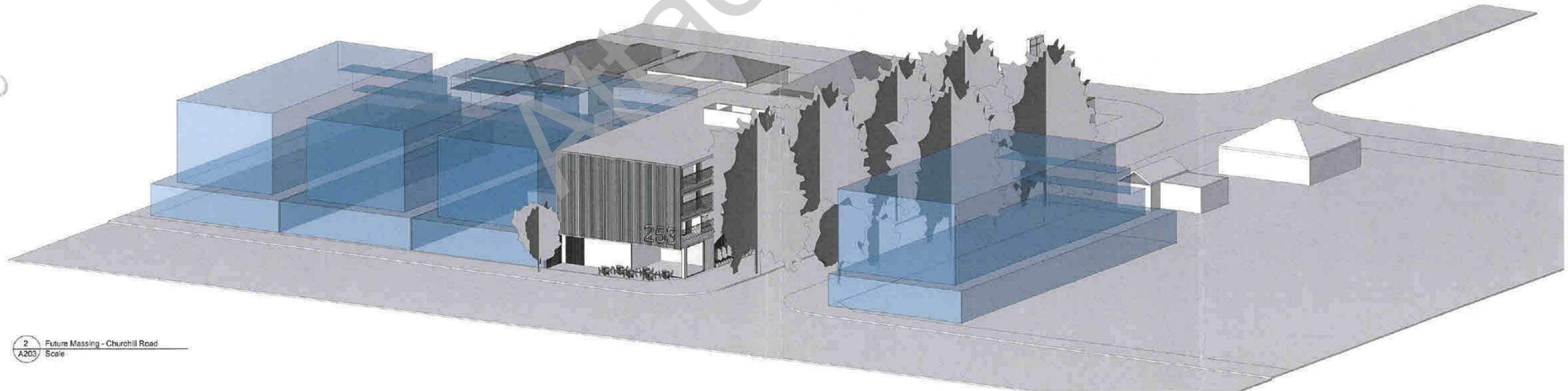
3 South East View
A202 Scale



4 North West View
A202 Scale



1 Existing Massing - Churchill Road
 A203 Scale



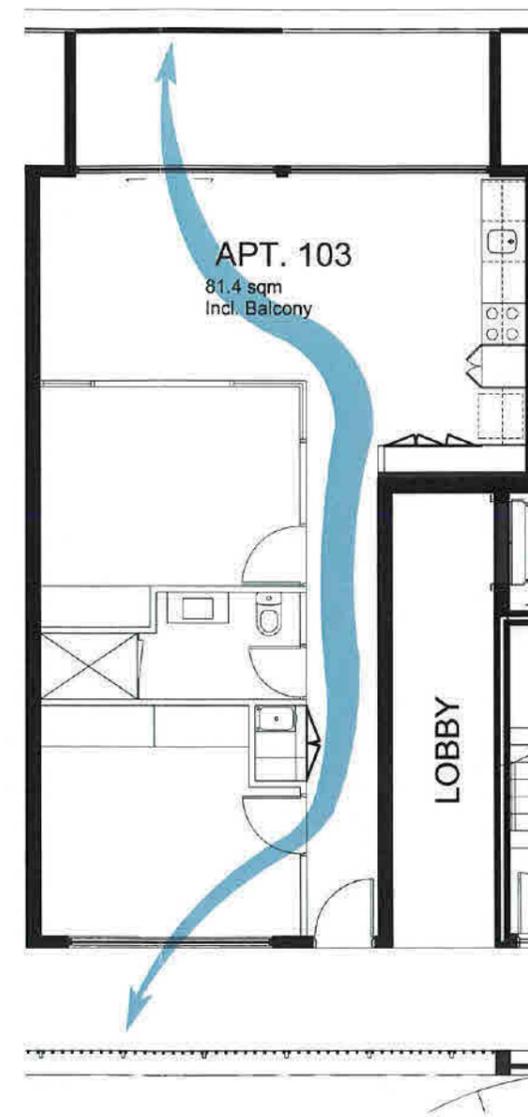
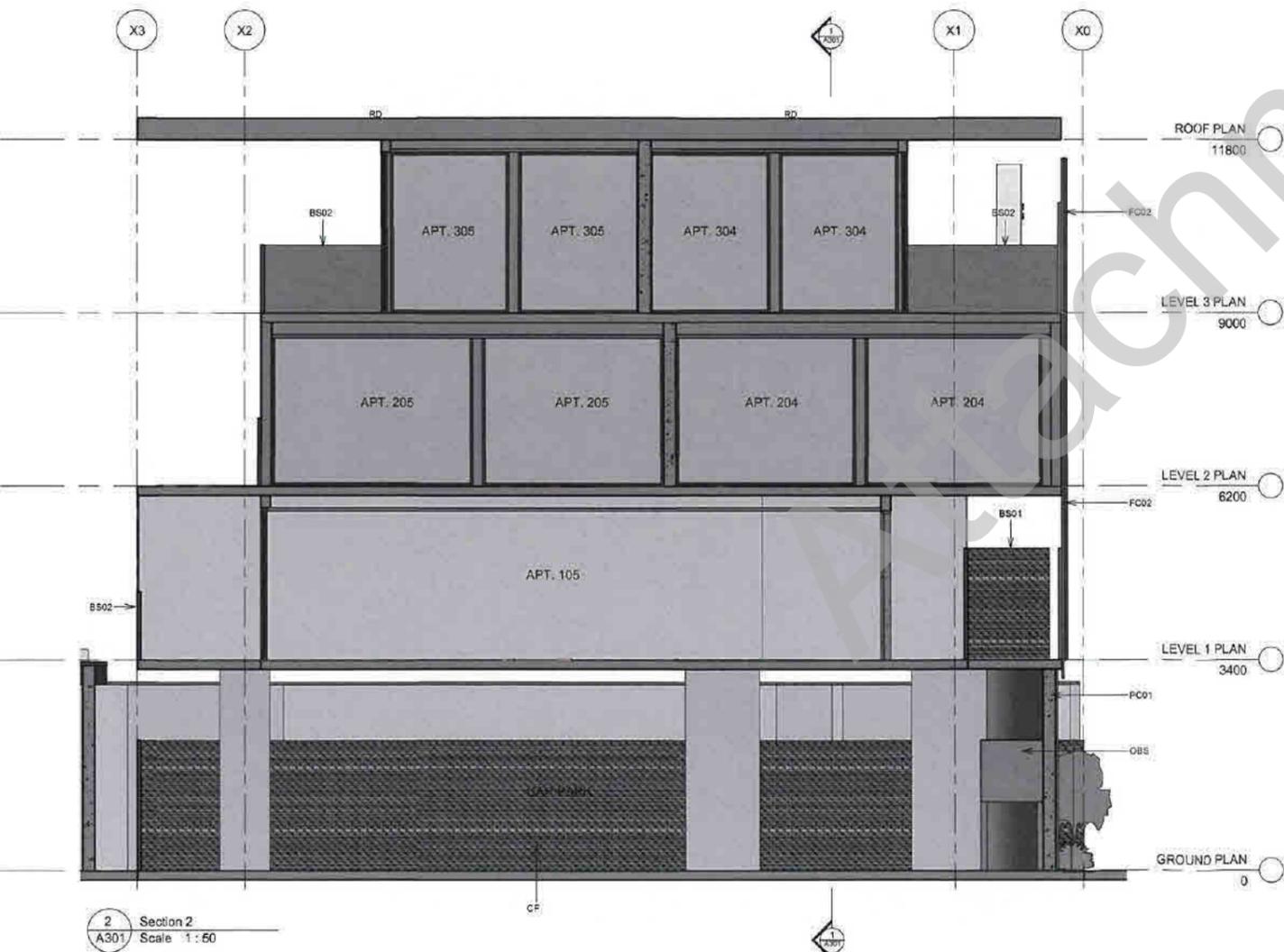
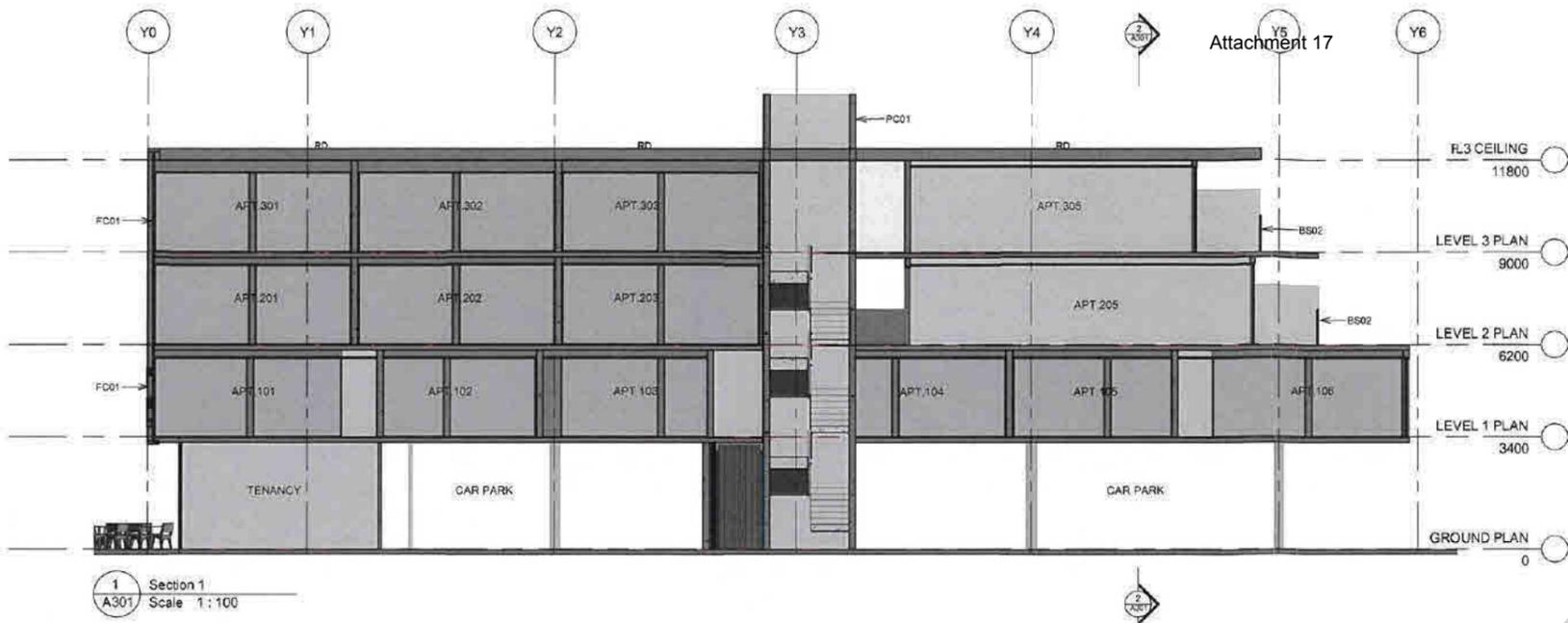
2 Future Massing - Churchill Road
 A203 Scale



2 NorthEast Scale A204



1 SouthEast Scale A204



3 TYPICAL FLOOR PLAN
A301 Scale 1:50

MATERIALS LEGEND

- CF 2100mm high colorbond good neighbour fences
- GW Aluminium framed glass window assembly
- LV Weather proof aluminium louvers
- BS01 1800mm metal balcony screen wall (Copper or rust finish)
- BS02 1100mm metal balcony screen wall (Copper or rust finish)
- OBS Over bonnet storage
- PC01 Precast concrete wall - Natural finish
- PC02 Precast concrete wall - Dark/black oxide
- RD Colorbond metal roof deck
- FC01 Timber facade screen cladding
- FC02 Screenwood baller screen
- FC03 Merbau timber
- VG Vertical garden system

LUMENSTUDIO

Julian Rutt
B.Arch (hons), AIA
jrutt@adam.com.au
0422 305 099

15 October 2015

By email: susan.giles@prospect.sa.gov.au

City of Prospect
Development Services
128 Prospect Road
Prospect SA 5082

Attention: Susan Giles
PRIVATE AND CONFIDENTIAL

Dear Susan

DESIGN REVIEW: 253 CHURCHILL RD, PROSPECT

Regarding the information provided on the Louka Zahous drawings provided for the proposed multi-residential dwelling at 253 Churchill Road, Prospect, and in the framework of providing design review advice, I offer the following comments;

At four storeys the proposal meets the desire to increase density along major thoroughfares, though as noted is (currently) at odds with local precedent though more than meets the minimum desired density of 1 dwelling per 100m² with 16 dwellings in 677 m² of site.

The building's scale is well broken down and provides significant visual interest to the streetscape and is commendable for this.

The format of units makes partial use of ideal solar orientation for access to northern light and passive solar design to main living areas in all units and there is some ability to make use of cross breezes, though there are bedrooms relying on borrowed light. Consideration of sustainable design ideals has not been highlighted though there would be some ability to incorporate some strategies to minimise water and energy consumption on site.

Landscaping has been only partly detailed. Increased passive surveillance to the street from this proposal is desirable. Aesthetically, the building form goes well beyond the minimum or purely functional response to create a positive precedent for future developments - subject to acceptable resolution of the issues noted above and in the detailed notes.

Yours sincerely



Julian Rutt
Architect, AIA

LUMENSTUDIO

Julian Rutt

B.Arch (hons), AIA

jrutt@adam.com.au

0422 305 099

15 October 2015

DESIGN REVIEW: 253 CHURCHILL RD, PROSPECT**Context***Good design responds to and contributes to its context. Context can be defined as the key natural and built features of an area.*

Little information provided on surrounding/local context though a review of Google Streetview indicates predominantly detached, single storey buildings, a mix of commercial and residential along Churchill Road with the Islington rail yards opposite. As expected the proposal is at odds with the local precedent noting that it is however aimed at the desired future character for increased densities along major 'Urban Corridors'.

Scale*Good design provides an appropriate scale in terms of the building height relative to the width of the street and height of the surrounding buildings.*

The proposal does not comply with the minimum 3m setback to the North and provides no set back along the southern boundary though this is to the side (Redin) street though appears to meet the requirements of the 45 degree plane from 3m above the rear boundary. Despite the set backs, the height and scale of the proposal is generally appropriate given the desired increase in density for the site, the proposed 4 storeys meet the desired provision of maximum of four storeys.

Built form*Good design achieves an appropriate built form for a site and the buildings purpose, in terms of building alignments, proportions, building type and the manipulation of building elements.*

The proposal is reasonably well articulated to the most prominent facades through significant material changes and modulation of solid elements, and window/door openings, in good measure to the North and South facades. Although the most prominent facade (west) facing Churchill Road is largely blank, the proposed feature material and immediate treatment at ground level and adjoining facades negates this as problematic.

Density*Good design has a density appropriate for a site and its context, in terms of dwelling yields (or number of units or residents).*

The proposal meets the desired density of 1 unit per 100m² for the new urban corridor provisions though provision of 16 units on a 677m² site.

Resource, Energy & Water efficiency*Good design makes efficient use of natural resources, energy and waste throughout its full life cycle, including construction.*

Little information provided regarding sustainability of materials, structure, energy use or generation, water harvest or reuse, waste or similar. Dwelling orientation allows some access to northern aspect for main living areas to all dwellings and double open sided design will allow some potential for use of cross breezes, though perhaps less so to apartments 204 & 205 on level 2 and 304 & 305 on level 3.

Landscape

Good design recognises that together landscape and buildings operate as an integrated and sustainable system, resulting in greater aesthetic quality and amenity for both occupants and the adjoining public domain.

The maximised footprint and vehicle access requirements do not provide much area to allocate for landscaping and very little has been provided beyond a small strip along Redin Street which would effectively be in shade year round and have limited planting options.

Amenity

Good design provides amenity through the physical, spatial and environmental quality of a development.

It is not clear if overlooking issues to the North have been fully addressed – the provided drawings did not have legible notes. The internal layouts are less than ideal requiring several bedrooms to rely on borrowed light.

Safety and security

Good design optimises safety and security, both internal to the development and for the public domain.

Passive surveillance to the street significantly improved by the proposal through additional balconies overlooking the street. Secure gate and entry to site prevents unauthorised (and hidden) site access. Deeply recessed entry problems reduced through curved entry and reasonable sight lines.

Social Dimensions

Good design responds to the social context and needs of the local community in terms of lifestyles, affordability and access to the social facilities.

Reasonable variation between dwelling layouts though mostly two bedroom with one single bedroom unit, though short of the 20 dwelling number where varied types and sizes are required. Increased density and reduced size of dwellings are in themselves a variation to the existing dominant housing precedent.

Aesthetics

Quality aesthetics require the appropriate composition of building elements, textures, materials and colours and reflect the use, internal design and structure of the development.

The proposal's form and massing has been well treated through the use of modulated faces and forms, changes in materials, well proportioned openings in solid to void proportions and has minimised large expanses of blank walls.

Though there are some issues that need to be more adequately addressed, the proposal sets a positive precedent for future developments.

In reply please quote 2015/00161/01, Process ID: 362032
 Enquiries to Reece Loughron
 Telephone (08) 8226 8386
 Facsimile (08) 8226 8330
 E-mail dpti.luc@sa.gov.au



Government of South Australia

Department of Planning,
 Transport and Infrastructure

26/11/2015

**SAFETY AND SERVICE –
 Traffic Operations**

GPO Box 1633
 Adelaide SA 5001

Telephone: 61 8 8226 8222
 Facsimile: 61 8 8226 8330

ABN 92 386 288 135

Mr Scott McLuskey
 City of Prospect
 PO Box 171
 PROSPECT SA 5082

Dear Scott,

SCHEDULE 8 - REFERRAL RESPONSE

Development No.	050/344/15 – Amended Plan
Applicant	Loucas Zahos Architects
Location	Lot 59 in DP 991 (CT 5684/552), 253 Churchill Road (cnr Redin Street), Prospect
Proposal	Construction of a four storey mixed use development comprising 16 apartments and ground level café, car parking and landscaping

I refer to the above development application forwarded to the Safety and Service Division of the Department of Planning, Transport and Infrastructure (DPTI) in accordance with Section 37 of the *Development Act 1993*. The proposed development involves development adjacent a main road as described above.

The following response is provided in accordance with Section 37(4)(b) of the *Development Act 1993* and Schedule 8 of the *Development Regulations 2008*.

THE PROPOSAL

The development involves the demolition of all existing structures on the site and construction of a four storey mixed use development comprising 16 apartments and a ground level café (41.7m²). All vehicular access will be gained via Redin Street.

CONSIDERATION

The subject site abuts Churchill Road and Redin Street. Churchill Road is identified as a Peak Hour Route and a Public Transport Corridor under *DPTI's A Functional Hierarchy for South Australia's Land Transport Network* and is gazetted for 26.0 metre B-Double vehicles. At this location Churchill Road has an AADT of 25,600 vehicles per day (8% commercial vehicles) and a posted speed limit of 60 km/h. Redin Street has a posted speed limit of 40km/h.

Road Safety

DPTI has received the amended plan (refer Loucas Zahos Architects, Drawing A104, Revision P4, dated 06/11/15) and associated Traffic and Parking Assessment undertaken by Phil Weaver & Associates (dated 6/11/15).

/DOCS AND FILES# 9843757

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DPTI notes that the amended site plan provides access to Redin Street approximately 36 metres from the Churchill Road property boundary. DPTI considers this access to have negligible impacts to the free flow of traffic on Churchill Road. All existing access points to Churchill Road should be closed and reinstated with kerb and gutter.

It is noted that on-street parking is banned along the northern side of the Redin Street boundary along with No Parking restrictions along the southern side for Redin Street between 8.00am and 6.00pm Monday – Friday and 8.00am to 12 noon Saturday. DPTI supports the current parking restriction in order to ensure simultaneous two-way movements can be accommodated at the Churchill Road / Redin Street junction during the peak traffic periods.

DPTI also recognises that vehicles turning right into Redin Street will be able to utilise the existing right turn lane and therefore it is considered unlikely that additional vehicles turning right into Redin Street to access the site will impact the Churchill Road traffic flows.

Car Parking & Manoeuvring

It is noted that the site generates a requirement of 20 on-site car parking spaces, however only 16 parks are being provided. This shortfall may result in some demand for on-street parking. Consequently, Council should be satisfied that the shortfall of spaces will not adversely impact on the surrounding roads.

DPTI has reviewed the Traffic and Parking Report and considers the proposed design has provided sufficient internal manoeuvring area for B85 vehicles to enter, manoeuvre and exit onto Redin Street in a forward direction. It is noted that car park 10 will be dedicated to a small vehicle only.

The report also identified an alternative access location as shown in Figure 8. Whilst no objection would be raised to this design, DPTI recommends that all on-street parking located on the northern side of Redin Street, west of the access, is banned along with the provision of turning profiles indicating that parks 7-9 and 10 can still be conveniently accessed. In addition a clear 1.0m separation should be provided from the adjacent stobie pole.

Council should ensure that all common access and manoeuvring areas should be clearly identified as right of way or common property at the land division stage.

Road Widening

The Metropolitan Adelaide Road Widening Plan shows a possible requirement for a strip of land up to 4.5 metres in width from the Churchill Road frontage of this site along with a 4.5 metre x 4.5 metre corner cut off at the Churchill Road / Redin Street corner for future upgrading of the Churchill Road / Regency Road intersection. It is noted that a 3.04 metre x 3.04 metre corner cut off has been provided from Lot 59 (CT 5684/552). DPTI has reviewed this corner cut-off and is satisfied that no further land is required for the corner cut off at this time.

Whilst it is considered that no further land will be required from the subject site for road purposes, the consent of the Commissioner of Highways under the Metropolitan Adelaide Road Widening Plan Act is required to all building works on or within 6.0 metres of the possible requirement. As the development is within the above areas, consent will be required in this instance.

CONCLUSION

In-principle, no objection is raised to the proposed development subject to the following conditions.

ADVICE

The planning authority is advised to attach the following conditions to any approval:

1. The access point to Redin Street shall be designed in accordance with Loucas Zahos Architects Ground, Site & Level 1 Plan, Drawing A104, Revision P4, dated 06/11/15.
2. The access shall be appropriately flared to the road to facilitate unimpeded ingress/egress.
3. Appropriate signage and line marking shall be installed to reinforce the desired traffic flow at the Redin Street access point.
4. All vehicles shall enter and exit the site in a forward direction.
5. The shared internal vehicle manoeuvring areas shall be clear of all obstructions including meters, letterboxes and bicycle/scooter parking.
6. The redundant Churchill Road crossover shall be closed and reinstated to Council's satisfaction at the applicant's cost prior to habitation of the dwellings.
7. All landscaping located along the side boundaries forward of the building and along the Churchill Road boundary shall be low growing to preserve sightlines across the corner.
8. All car parking shall be designed in accordance with AS/NZS 2890.1:2004 and AS/NZS 2890.6:2009
9. Signage associated with the development that is visible from Churchill Road shall not contain any element of LED or LCD display that is viewable from the adjacent/nearby roads.
10. Signage upon the site that is visible from Churchill Road shall not contain any element that flashes, scrolls, moves or changes.
11. Signage associated with the development shall be finished in a material of low reflectivity to minimise the risk of sun/headlamp glare that may affect motorists' perception of the road.
12. All stormwater generated by the proposal shall be appropriately collected and disposed of without entering or jeopardising the safety of the adjacent arterial road network.

The following notes provide important information for the benefit of the applicant and are required to be included in any approval:

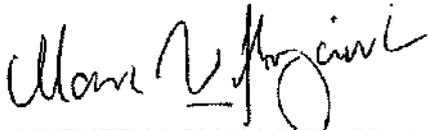
- i. The Metropolitan Adelaide Road Widening Plan (MARWP) shows a possible requirement for a strip of land up to 4.5 metres in width from the Churchill Road frontage of this site and a 4.5 metres x 4.5 metres corner cut-off at the junction of Churchill Road with Redin Street. It is noted that CT 5684/552 shows that a 3.04

4

metre x 3.04 metre corner cut-off has already been provided. No further land is required at this time.

Notwithstanding this, the consent of the Commissioner of Highways under the MARWP Act is required to all building works on or within 6.0 metres of the corner cut-off requirement. Accordingly, the attached consent form and three copies of the approved site plans must be provided to DPTI for consent purposes.

Yours sincerely,



A/GENERAL MANAGER, OPERATIONAL SERVICES

For **COMMISSIONER OF HIGHWAYS**

Encl: MARWP consent form

A copy of the decision notification form should be forwarded to dpti.developmentapplications@sa.gov.au

METROPOLITAN ADELAIDE ROAD WIDENING PLAN ACT, 1972



Government of South Australia
Department of Planning,
Transport and Infrastructure

**APPLICATION FOR CONSENT OF THE COMMISSIONER OF HIGHWAYS
FOR BUILDING WORK**

Owner's Name _____

Applicant's Name _____
(If not owner)

Relationship to Owner (Lessee, Prospective Purchaser, Etc.) _____

Postal Address _____

Postcode _____

Location of Proposed Building Works (Full Details Required):

L.T.O. Plan No.: _____ Allotment No.: _____ Section: _____

Hundred: _____ House No.: _____

Street Name _____ Suburb: _____

Certificate of Title _____ Folio _____

Volume _____

Nature of Proposed Building Work _____

Proposed Location of Building Work on Land:

Distance from existing front road boundary to nearest point of building _____

Distance from existing side boundary (if on corner) to nearest point of building _____

Total estimated cost of proposed building work \$ _____

Estimated cost of proposed building work on land to which the Act applies \$ _____

Signature(s): _____ (Applicant) _____ (Owner or Authorised Agent) _____ Date _____

Notes:

1. *Three (3) copies of the approved site plan describing fully the nature and extent of the proposal must be forwarded with this application form together with one copy of the notification of the relevant authority's decision to approve the development under the Development Act 1993.*
2. *The Act requires that the consent of the Commissioner of Highways must be obtained for any building work on land to which the Act applies.*
3. *The Act applies to all land shown on the Metropolitan Adelaide Road Widening Plan as possibly required for road widening and all land within six metres of the boundary of that land.*

PLEASE RETURN TO:

COMMISSIONER OF HIGHWAYS
DEPARTMENT OF PLANNING, TRANSPORT AND INFRASTRUCTURE
GPO BOX 1533
ADELAIDE SA 5001

For further information regarding the Metropolitan Adelaide Road Widening Plan Act,
please contact DPTI on (08) 8343 2303 or (08) 8343 2306.

STATEMENT OF REPRESENTATION
Pursuant to Section 38 of the Development Act 1993
Ref. 050/344/2015

City of Prospect
30 SEP 2015
Received

TO: City of Prospect
128 Prospect Road
PROSPECT SA 5082

NAME OF REPRESENTOR(S): BERYL BOULTON

RESIDENTIAL/BUSINESS ADDRESS: 18 FRANCES ST PROSPECT

POSTAL ADDRESS:

MY REPRESENTATION IS IN REGARD TO THE PROPOSED DEVELOPMENT AT:

253 Churchill Road PROSPECT

SCANNED
01 OCT 2015
CITY OF PROSPECT

THIS REPRESENTATION IS (please tick one of the following):

- In favour of the application
- Against the application
- Neither for nor against the application

MY COMMENTS ARE AS FOLLOWS (if space is insufficient, attach additional pages):

THIS DEVELOPMENT WILL NOT BE IN KEEPING WITH THE AREA .
THE REEPHAM HOTEL NEARBY IS ONLY 2 STOREY BUILDING .
CAR PARKING WILL NOT BE SUFFICIENT FOR A GROUND LEVEL CAFE .
ROAD CONGESTION HAPPENS NOW IN THIS AREA WITH INCREASED
TRAFFIC ON CHURCHILL ROAD, PROSPECT .
AT 253 CHURCHILL RD, CHURCHILL ROAD GOES FROM 2 LANES
TO 1 LANE FLOW
THIS DEVELOPMENT SITE WILL LOOK TOO HIGH FOR THE AREA .
REDIN STREET USED TO HAVE NO PARKING NEAR THERE ,
SO THAT EXPLAINS CAR PARKING WILL BE A PROBLEM .

Please indicate below whether or not you wish to be heard by the Development Assessment Panel in support of your submission:

- I do not wish to be heard
- I wish to be heard personally
- I will be represented by

SIGNED: B BOULTON DATED: 29/9/15

For a representation to be valid, it must:

- Be submitted before the end of the notification period;
- Include your name and address;
- Set out the reasons for your representation;
- Indicate whether or not you wish to be heard by Council's Development Assessment Panel; and
- If being made by 2 or more persons, nominate a person who will be taken to be making the representation.

Scott McLuskey

From: Tom colmer <tomcolmer5@gmail.com>
Sent: Monday, 28 September 2015 2:33 PM
Subject: Application 050/344/2015

Good morning

I would like to respond to the above Notice of Application for Category Two development. My details are Tom Colmer – 255 Churchill Road, Prospect SA 5082.

In response I am neither for nor against the application, however I would like to raise concerns in the above areas:

1. The balconies overlooking my house (facing North). I can accept that balconies should face north for the light benefits however in this instance I think that the height of the balcony fences is too low and the balconies should be designed to respect the privacy of my adjacent property. I would also like the council to confirm that if the house to the north of mine is developed in the future then their balconies should not be on the south (i.e. facing my property as well).
2. The number of car parks being 14 is too low for this many apartments. Based on the guidelines for the area the apartments should have one car park each and .25 car parks for visitor parks. This would be 20 car parks. There should also be consideration to include car parking for the café workers and potentially their customers.
3. The fence of 2.1 metre colour-bond good neighbour fence seems inadequate for this sized development.
4. The rubbish/storage adjacent to my property needs to be described in more detail. It should be properly contained so that it does not cause any problems in smell or noise to my house.

I am happy to appear before the Development Assessment Panel if required.

Regards
Tom Colmer

STATEMENT OF REPRESENTATION
Pursuant to Section 38 of the Development Act 1993
Ref. 050/344/2015

TO: City of Prospect
128 Prospect Road
PROSPECT SA 5082

NAME OF REPRESENTOR(S): Catherine Purvis + Michael Holzinger
RESIDENTIAL/BUSINESS ADDRESS: owners of 22 Princes Street Prospect 5082
POSTAL ADDRESS: 56 Charles Street Norwood 5067

MY REPRESENTATION IS IN REGARD TO THE PROPOSED DEVELOPMENT AT:

253 Churchill Road PROSPECT

THIS REPRESENTATION IS (please tick one of the following):

- In favour of the application
 Against the application
 Neither for nor against the application



MY COMMENTS ARE AS FOLLOWS (if space is insufficient, attach additional pages):

- Balconies on northern and eastern faces have a clear view into our property. We propose that the balconies are removed or relocated to the southern side. This is our most major complaint about the plans as they stand.
- Without an electrical plan we cannot determine the impact of potentially obtrusive lighting.
- More parking needs to be provided to account for visitors to the cafe.
- Our view is that a structure of four storeys is too large for the site. The main impact of the development is on Redin Street and surrounds given that the property does not have a large frontage on Churchill Road. Surrounding streets (particularly Princes Street) are narrow and Redin Street is already congested due to urban infill and regular attendance at the Sikh temple (Gurdwara Sahib) at 16-18 King Street. This traffic frequently extends all the way west on Redin Street to Churchill Road.

Please indicate below whether or not you wish to be heard by the Development Assessment Panel in support of your submission:

- I do not wish to be heard
 I wish to be heard personally
 I will be represented by

SIGNED: Cal V.R.

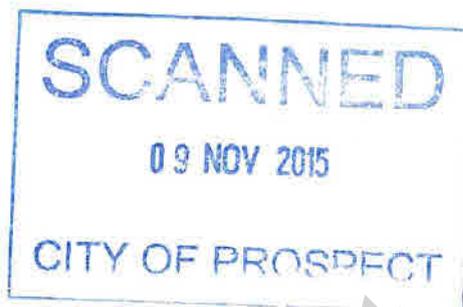
DATED: 30/09/2015

For a representation to be valid, it must:

- Be submitted before the end of the notification period;
- Include your name and address;
- Set out the reasons for your representation;
- Indicate whether or not you wish to be heard by Council's Development Assessment Panel; and
- If being made by 2 or more persons, nominate a person who will be taken to be making the representation.



Scott McLuskey
City of Prospect
PO Box 171
Prospect, SA 5082



30th October 2015

Attention: **Scott McLuskey**

253 CHURCHILL ROAD

Subject: **FOUR STOREY MIXED USE DEVELOPMENT COMPRISING 16 APARTMENTS AND GROUND LEVEL CAFÉ, CAR PARKING AND LANDSCAPING (DA 050/344/2015)**

Dear Mr McLuskey,

We refer to your letter of the 16th of October 2015 (Received 16th October) and write on behalf of our clients Mr D and Mrs J Zarkovic.

In reply to the two representations and one submission received by Council after the advertising we advise as follows:

REPRESENTATIONS

- 1) *MS BERYL BOULTON
18 PRINCES STREET, PROSPECT*

In response to Ms Boulton's representation we comment as follows:

- a) The proposal is totally in keeping with the Councils master plan of re-zoning for Churchill Road including heights, massing and articulation. Bears no relevance to the Reepham Hotel.
- b) The ground level 'café' will only generate minimal traffic which can park on the Redin Street frontage of the property. The café is simply proposed as a street activation device and to add some amenity for residents and is not pivotal to the development. We do believe a food outlet can assist with building a human scale and character along Churchill Road.
- c) All apartments will have parking for their own use and that of their visitors when taking into consideration the provision of cars and scooters.
- d) The height, massing and articulation as mentioned above is in keeping with Council (and the State Government's) vision for the area.



Our client has also commissioned Phil Weaver Traffic Consultants to provide professional comment and surveys and these will be provided to Council in due course.

2) *C. PURVIS AND M. HOLZINGER*
66 CHARLES STREET, NORWOOD
(OWNERS OF 22 PRINCES STREET, PROSPECT)

With respect to this property and the owner's comments we advise as follows:

- a) The north eastern balconies have been appropriately modified or screened on the latest submitted drawings to ensure overlooking into the yard is eliminated. Please refer the updated documents.
- b) Lighting will to Australian Standards and be focused only on the building or its balconies and not be angled out to any other property.
- c) We refer you to our response above Item 1(a) and 1(d) and 1(e)

3) *TOM COLMER*
255 CHURCHILL ROAD, PROSPECT

With this email reply we respond to Mr Colmes concerns as follows:

- a) The north facing balconies are an integral part of the ESD design of this project and also anticipated in the Council Guidelines for properties developed along Churchill Road. Fortunately the extensive structures to the south and east of Mr Colmer's residence make it very difficult if not impossible to view the car parking and driveway arrangements at the back (east) of his house.
 With the site to the north of number 255 (which Mr Colmer asks council to comment on) our client has no control over.
- b) We refer you to item 1(a), 1(d) and 1(e) in relation to parking and traffic.
- c) As far as the fence is concerned we have referred this to our client and they propose the 2.1m Colorbond good neighbor fence be now changed to a 2.4m high blockwork fence. This should ameliorate this matter.
- d) The rubbish and storage proposal has been outlined in the Waste Management Plan and is located west of the house alignment and now behind a masonry wall.
- e) There is also an enclosure to ensure potential vandalism is limited.
 Noise will be limited and the future Body Corporate management will ensure the area remains clean and regularly empties and cleared.

4) *TRAFFIC AND PARKING*

We currently have Phil Weaver and Associates looking at the traffic and parking requirements for this project and will provide his comments and recommendations shortly, but in brief the responses to matters raised are expected to be:

- f) Narrow parking aisle (approx. 5.6m) – the Aisle has been amended to 6.0m
- g) No turnaround area being provided – a Reversing bay has now been provided
- h) Reversing movements from parks 13 and 14 over 33+m in length - Reversing bay will rectify this .
- i) All parks seem narrow - 2.4m wide for parallel and 2.1 wide for tandem comply with AS 2890.1
- j) (reverse entry) Parallel parks (9-12) make it very difficult to exit in a forward direction – the reversing bay will rectify this .
- k) 16 units (plus café) but only 14 car parks being provided - 15 parking spaces are now



provided and it is envisioned that café patrons would be predominantly residents or would park on Redin Street .

Phil Weaver and Associates supporting information and turning profiles will be sent early next week .

We trust the above comments respond and deal with the comments raised but also request that we be able to attend and reply to any further comments, concerns and/or questions by the DAP or residents on the night of the application.

Yours Faithfully
Loucas & Zahos Pty Ltd

Michael Loucas
Director

Sent to: Scott McLuskey, City of Prospect
Cc: D+J Zarkovic
Phil Weaver and Associates

Attachment



Consultant Traffic Engineers
ABN 67 093 665 680

204 Young Street
Unley SA 5061

P: 08 8271 5999
F: 08 8271 5666
E: mail@philweaver.com.au

File: 146-15

6 November 2015

Mr Louis Petridis
Loucas Zahos Architects
Level 1
270 Flinders Street
Adelaide SA 5000

Dear Mr Petridis,

**PROPOSED RESIDENTIAL DEVELOPMENT - 253 CHURCHILL ROAD, PROSPECT -
TRAFFIC AND PARKING ASSESSMENT**

I refer to our recent discussions relating to the proposed construction of 16 residential dwellings and associated car parking on the above site.

As requested I have undertaken the following review of the traffic and parking related aspects of the subject development.

Existing Situation

The subject site is located on the north eastern corner of the intersection of Churchill Road with Redin Street, Prospect.

The subject site currently accommodates a residential dwelling and a garage.

The subject site has frontages of approximately 43m to Redin Street and approximately 15m to Churchill Road, inclusive of a 3m by 3m corner cut-off.

The subject site has two existing access points, consisting of a single access point off both Churchill Road and Redin Street. The access point on Churchill Road is approximately 3m wide and is located approximately 5m from the northern boundary of the site. The access point on Redin Street is approximately 3.5m in width and is located adjacent to the eastern boundary of the site.

Churchill Road, adjacent to the subject site provides two traffic lanes and a bicycle lane in each direction separated by a central median. The bicycle lanes operate between 7.30am and 9.30am Monday to Friday on the eastern side of this road and between 4.30pm and 6.00pm Monday to Friday on the western side of this road. Parking is otherwise unrestricted on both sides of Churchill Road in the vicinity of the subject site. There is an opening within the central median of Churchill Road adjacent to Redin Street, within a short right turn lane provided within the median on the southern approach to this median. The right turn storage lane would permit at least two cars to store within this lane when drivers are waiting to turn into Redin Street without cars extending into the adjacent through lane.

Bus stops are located immediately to the north of Charles Street (approximately 150m to the south of the subject site) on both sides of Churchill Road.

Redin Street, adjacent to the subject site has a kerb to kerb width of approximately 7.5m with a verge width adjacent to the site of 5m.

Parking along the northern side of Redin Street, between the intersection with Churchill Road and a point approximately mid-way along the southern boundary of the site, is prohibited by a No Stopping Anytime restriction. East of this point, parking along the northern side of Redin Street is unrestricted.

A No Parking restriction extends along the southern side of Redin Street, opposite the subject site. This restriction applies between 8.00 a.m. and 6.00 p.m. Monday to Friday and between 8.00 a.m. and 12 noon Saturday. Outside of these periods parking in this area is unrestricted.

The speed limit on Churchill Road, adjacent to the subject site, is 60 km/h. The speed limit along Redin Street, east of Churchill Road, is 40 km/h.

Details of traffic volumes on Churchill Road have been obtained from the Department of Planning, Transport and Infrastructure, (DPTI). From a traffic count undertaken on at the intersection of Churchill Road with the Bunnings Hardware Store access point on Tuesday 2nd June 2015 it is identified that the two-way Annual Average Daily Traffic (AADT) volume to the immediate north of the subject site is approximately 35,000 vpd on Churchill Road.

Proposed Development

The proposed development is identified on a series of plans including the Ground Floor and Site Plan (Drawing No. A104 P4).

I note from the plans that the development will include:-

- Demolition of the existing buildings on the site,
- Construction of a multi-level building to accommodate:-
 - An undercroft car park, providing 16 spaces (11 ninety degree spaces and 5 parallel spaces). The car park will provide a reversing bay for cars to turn around within the western end of the aisle. Turning path drawings (Figures 1 to 7) have been prepared in order to demonstrate that a B85 design vehicle will be able to access various spaces and also turn around on site,

- A first floor level, providing six two-bedroom apartments,
 - second and third floor levels each providing five two-bedroom apartments,
 - A studio / office with an area of 28.5m² to be located in the north western corner of the site. It is understood that this area will most likely be tenanted by a resident within the building. Consequently there should be no additional car parking demand generated by this floor space,
- An approximately 6m wide access point off Redin Street, approximately 1m from the eastern boundary of the site. The existing fence along the eastern boundary of the site, where it meets the Redin Street boundary is a solid fence. This would potentially need to be replaced with an open style fence in order to provide an adequate pedestrian - vehicular sight splay to the east of the proposed access point.

The design of the 90° car parking spaces will provide: -

- car parking spaces of 2.4m in width,
- car parking spaces of 5.4m in length, and
- an aisle width of 6.1m.

The five parallel parking spaces have each been designed with a minimum width of 2.1 m and a length of at least 6 m and would therefore meet the spatial requirements for car parking by residents. However, access into space number 10 will be constrained and it is therefore recommended that this parking space be allocated for use by a small car thereby enabling occupants including the driver to access the car with the vehicle facing towards the west.

On the above basis, I consider that the design of the on-site car parking area would conform to the requirements of the relevant off-street car parking standard (AS/NZS 2890.1:2004).

Parking Assessment

Table Pr/5 – Off Street Vehicle Parking Requirements for the Urban Corridor Zone within the Prospect City Development Plan identifies car parking provisions as follows:-

- Residential development in the form of residential flat buildings or multi-storey buildings - 1 space per studio, one or two bedroom dwelling
plus
0.25 spaces per dwelling for visitor parking

On the above basis the residential component of the proposed development will require 20 spaces. The proposed residential component of the subject development will provide 16 spaces, which would result in a shortfall of 4 spaces.

The Development Plan specifies that a lesser car parking rate may be applied where:-

- Sites are located within 200 metres walking distance of a convenient and frequent service fixed public transport stop, and
- Generous on-street parking and/or public parking areas are available and in convenient proximity, other than where such parking may become limited or removed by future loss of access, restrictions, road modifications or widening.

It is most likely that the studio / office area will be tenanted by a resident with the building and consequently there should be no additional car parking requirement generated by this floor space.

As identified above, bus stops are located on both sides of Churchill Road, approximately 150m to the south of the subject site. Hence there is a significant potential for both residents and tenants to use public transport which would reduce the use of private vehicles visiting the site.

As also identified above, parking in the vicinity of the subject site is generally unrestricted on both sides of Churchill Road at weekends and during the following weekday periods:-

- Before 7.30am,
- Between 9.30am and 4.30pm, and
- After 6.00pm.

On the above basis, I consider that the short periods when car parking is not permitted on Churchill Road would generally not coincide with the peak visitor demand of the residential dwellings.

In summary, I consider that the proposed development will provide the equivalent of one car parking space for each residential dwelling and would therefore meet the residential requirement for such a development as identified by Council's Development Plan. It is further anticipated that the peak visitor parking demands associated with the subject development will be accommodated on street within close proximity of the subject development.

Traffic Assessment

The "Guide to Traffic Generating Developments" report produced by the former Roads and Traffic Authority of NSW identifies a peak hour traffic generation rate of 0.4 to 0.5 trips per unit for a medium density residential development.

On the above basis, I consider that the residential component of the subject development should generate of the order of 8 trips in the am and pm peak hour periods.

In the am peak hour period there would typically be 6 exit movements and 2 entry movements, whereas in the pm peak hour period there would typically be 3 exit movements and 5 entry movements associated with the residential component of the proposed development.

On the above basis, I suggest that the maximum volume of traffic likely to be generated by the subject development would be of the order of 8 trips in any one hour period.

Given the location of the subject development on the eastern side of Churchill Road, I anticipate that there would be at most 4 additional right turn movements into the subject development in any one hour period. Consequently, I consider that this would have minimal impact on the operation of the intersection of Redin Street with Churchill Road.

The ability of cars to access the car parking spaces has been reviewed with AutoTrack turning path software. Relevant turning path drawings are attached to this report. The attached turning path drawings include:-

- Figure 1: the ability of the be B85 design vehicle to be reversed from space 15 and exit the site in a forward direction,
- Figure 2: the ability of the be B85 design vehicle to be reversed from space 16 and exit the site in a forward direction,
- Figure 3: the ability of the B85 design vehicle to be reversed from space 1 and exit the site in a forward direction,
- Figure 4: the ability of the B85 design vehicle to be reversed from space 3 and exit the site in a forward direction,
- Figure 5: the ability of the B85 design vehicle to access space 14 and to both enter and exit the site in a forward direction,
- Figure 6: the ability of a small car to access space 10 and for the driver or passengers to exit the car, and
- Figure 7: the ability of the B85 design vehicle to use the dedicated reversing area to turn on site.

The proposed location of the access point at the eastern end of the site may conflict with an existing street tree and would therefore be subject to more detailed review. However, in the event that the access point would conflict with the street tree there would be an opportunity to relocate the access point slightly further to the west as identified in Figure 8. This figure is also attached to the report.

Possible relocation of the vehicle access point would still result in more than sufficient separation of the access point from Churchill Road and would permit a similar number of on street car parking spaces to be retained along the northern side of Redin Street.

Summary and Conclusions

As identified above the proposed development will provide a total of 16 parking spaces on site.

At most there would typically be of the order of 8 vehicles accessing the car park in any one hour period including both entry and exit movements.

On the basis of the above analysis there should be an increase of at most 4 vehicles waiting to enter Redin Street from Churchill Road in any one hour period. Consequently there would be minimal delay to traffic on Churchill Road.

I therefore consider that the volumes of traffic to be generated by the subject development will not be significant and will not adversely impact on the adjoining road network.

In summary, I consider that the proposed development will:-

- Provide a total of 16 car parking spaces. While such a car parking provision is less than that required by Council for the residential component of the subject development, there are bus stops located on Churchill Road within close proximity to the site. It is also identified that parking is generally unrestricted on both sides of Churchill Road with the exception of the morning and afternoon peak hour periods on weekdays. However, in these periods there would be relatively few visitors to the residential component and a lower use of the café than in lunchtime or evening periods,
- Not result in adverse traffic impacts on the adjacent road network, and
- Provide a design standard which is appropriate and essentially meets the requirement of the relevant Australian / New Zealand Standard for off-street car parking areas.

Yours sincerely



Phil Weaver
Phil Weaver and Associates Pty Ltd

Enc

Attachment

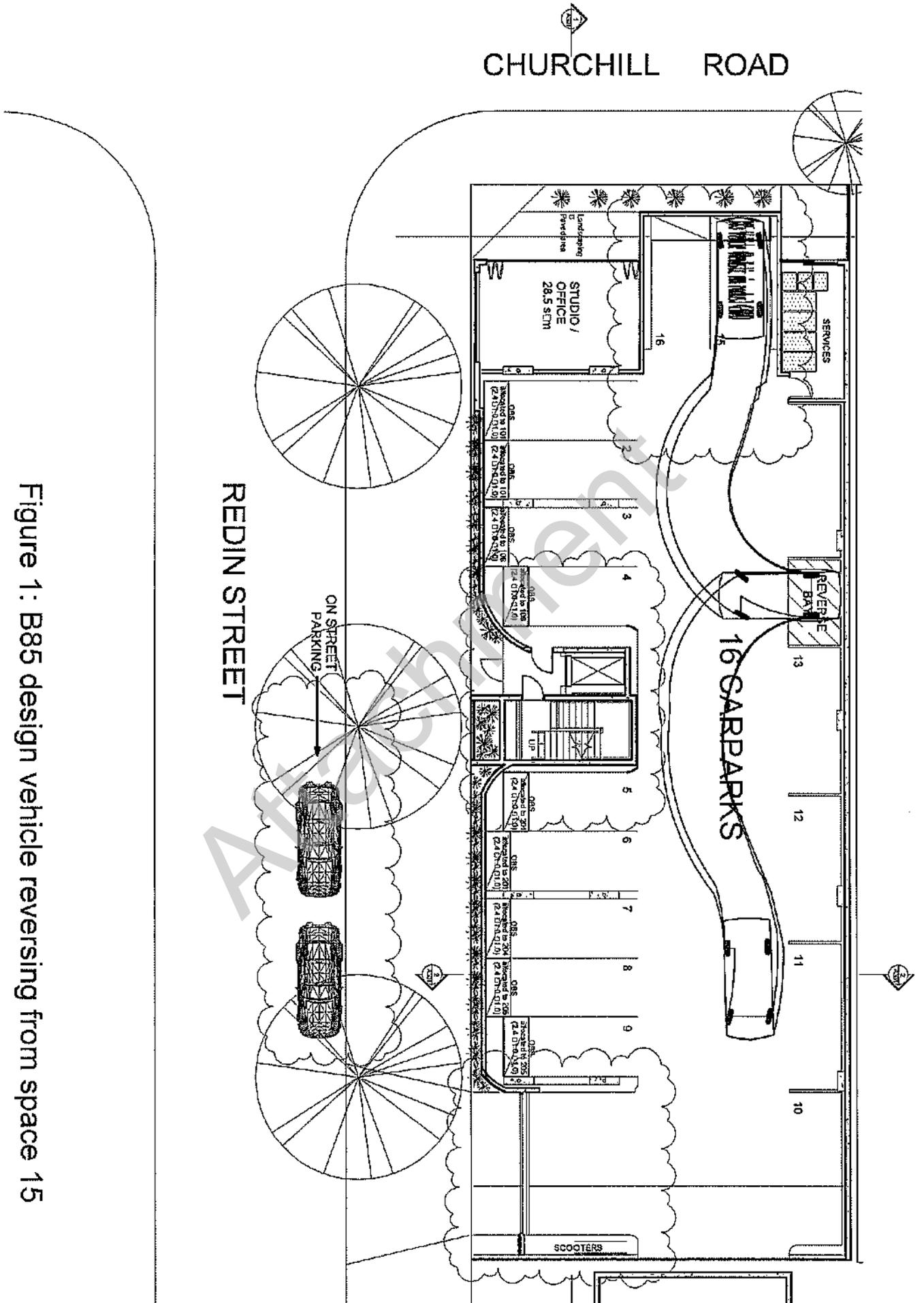


Figure 1: B85 design vehicle reversing from space 15

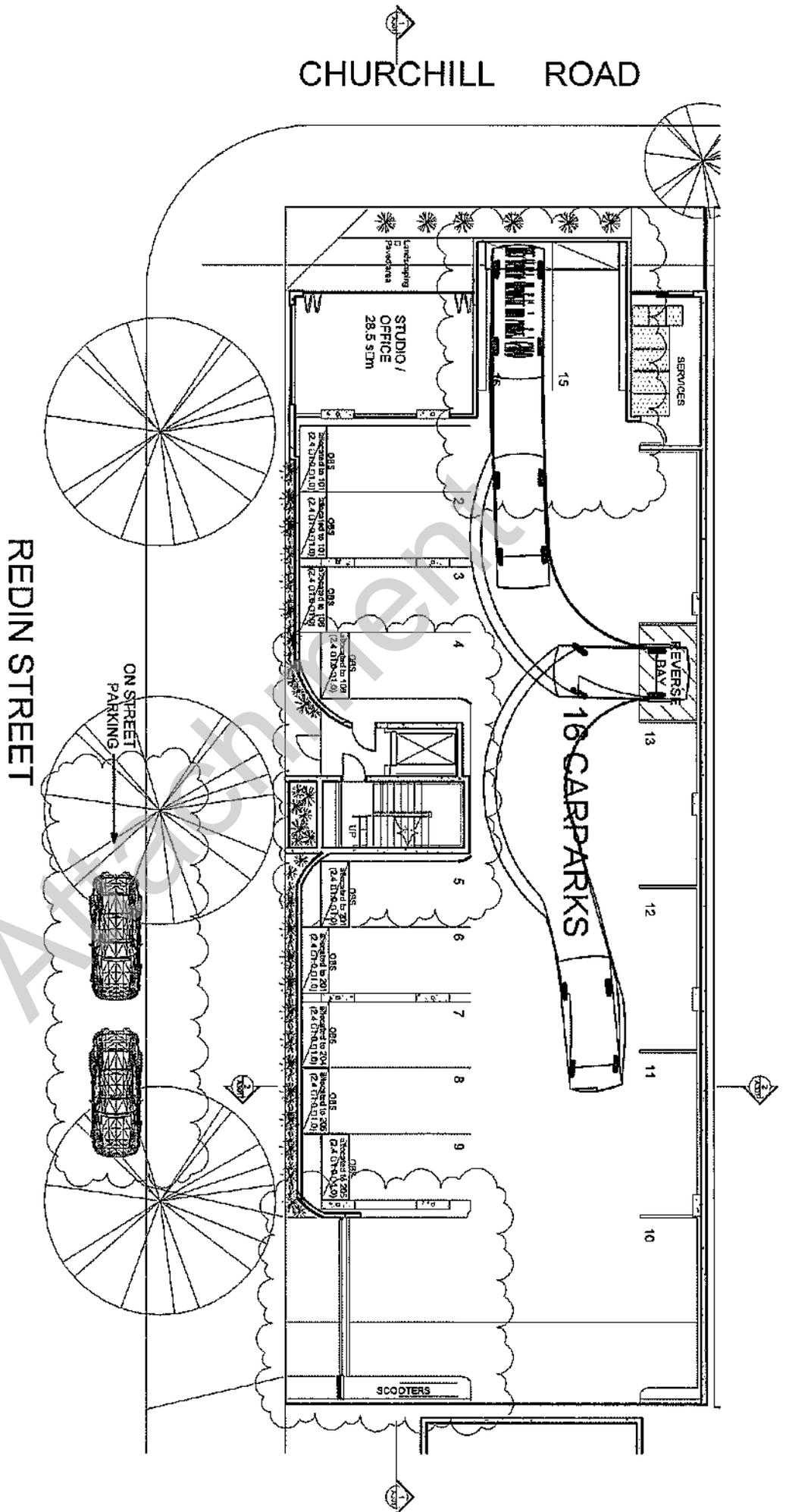


Figure 2: B85 design vehicle reversing from space 16

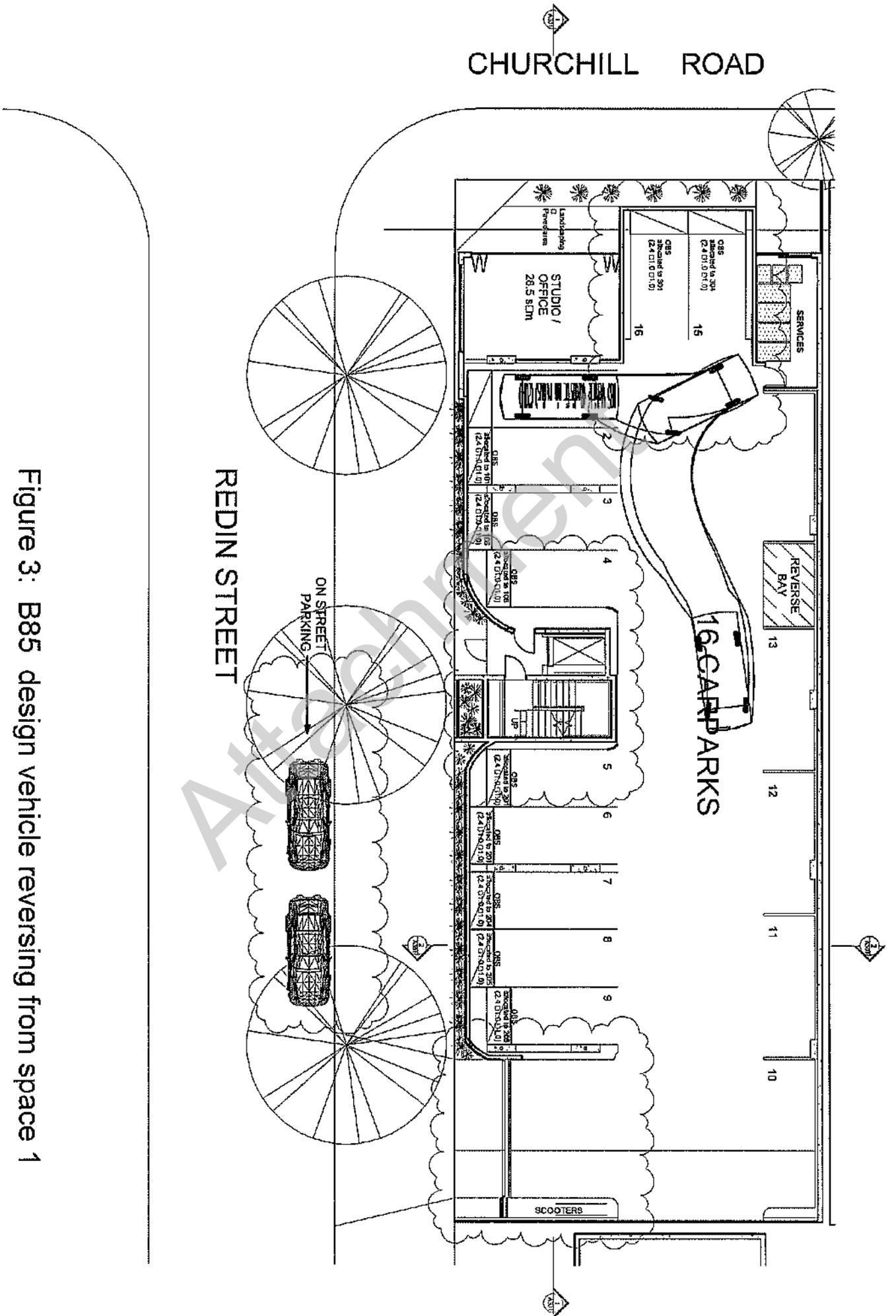


Figure 3: B85 design vehicle reversing from space 1

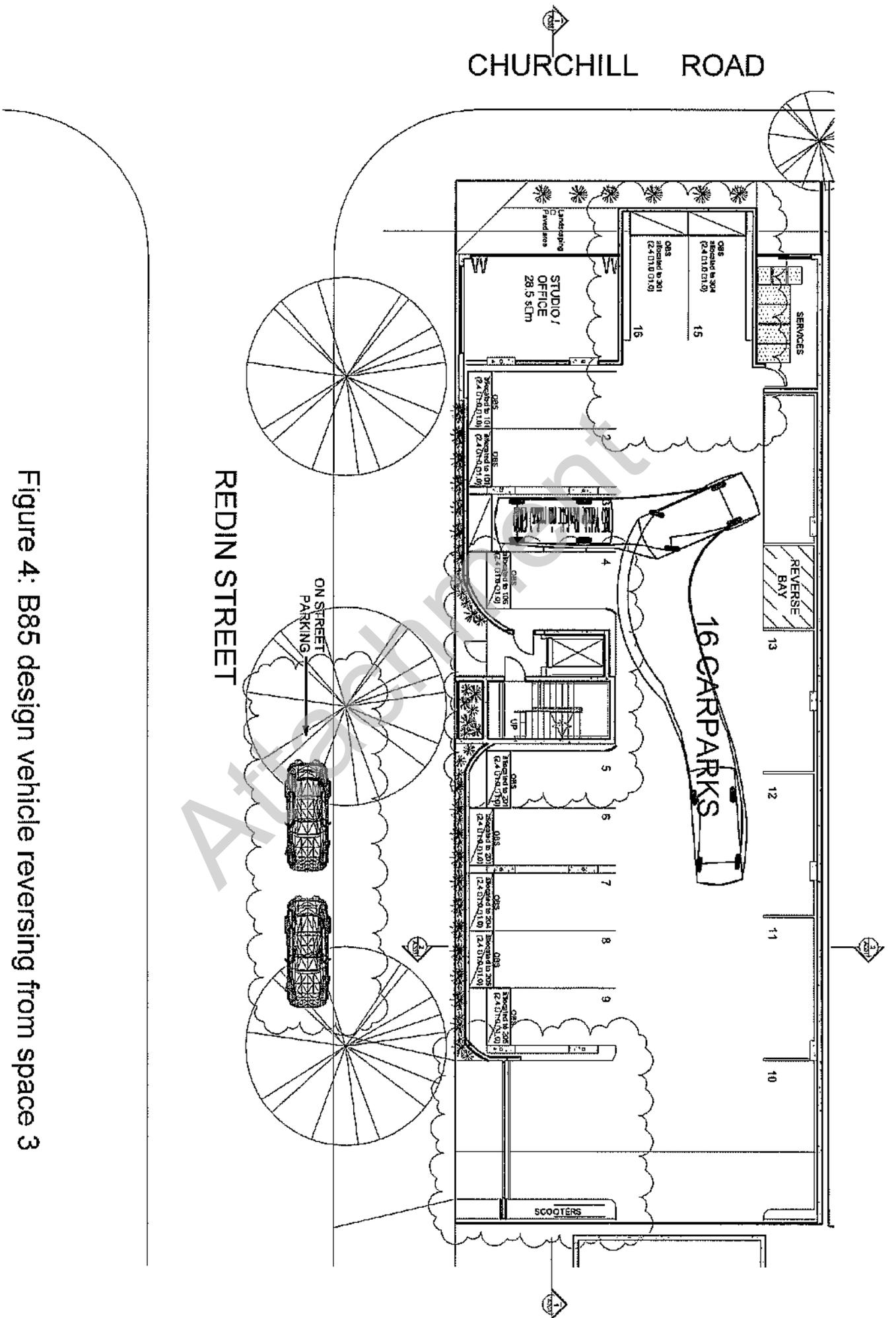


Figure 4: B85 design vehicle reversing from space 3

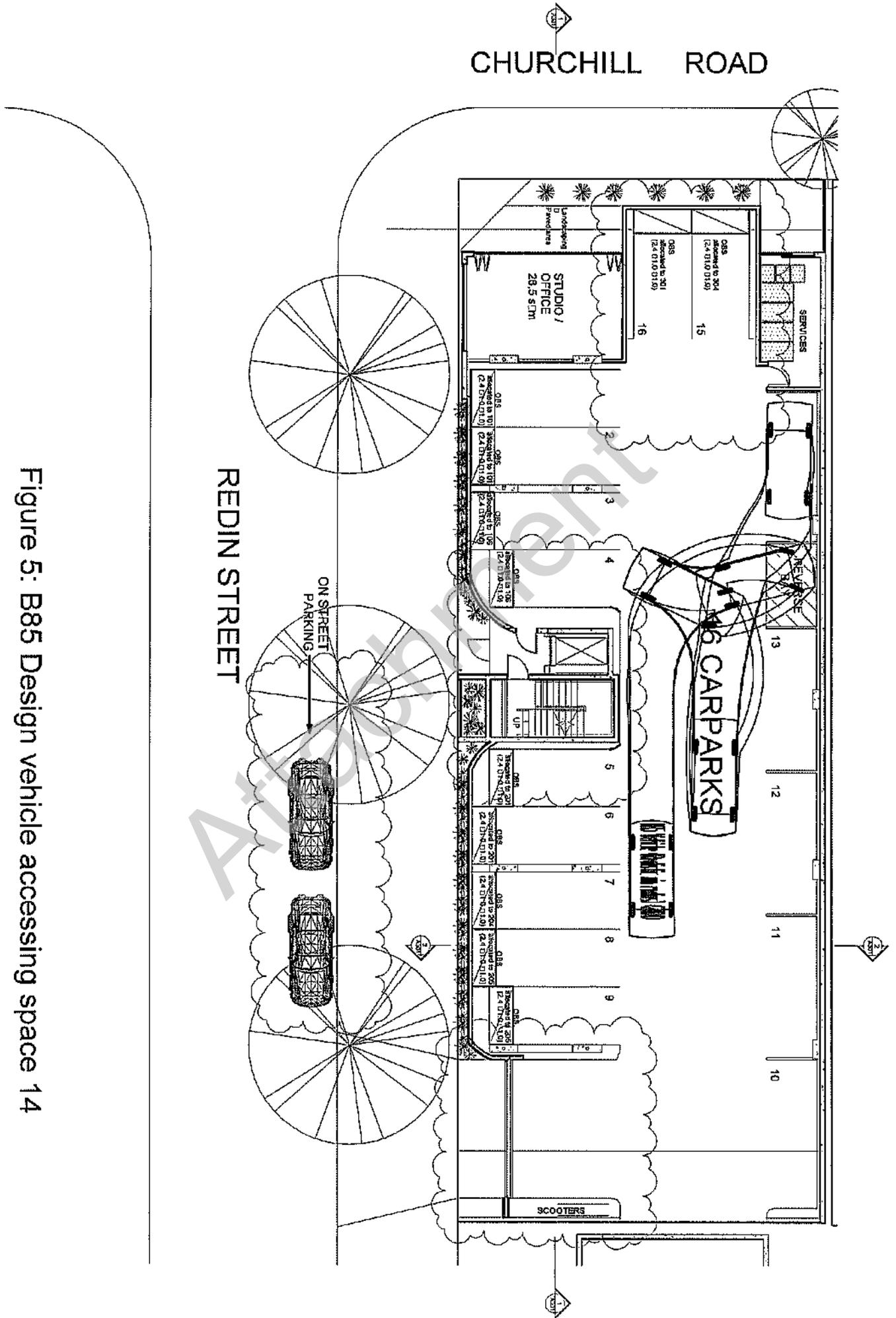


Figure 5: B85 Design vehicle accessing space 14

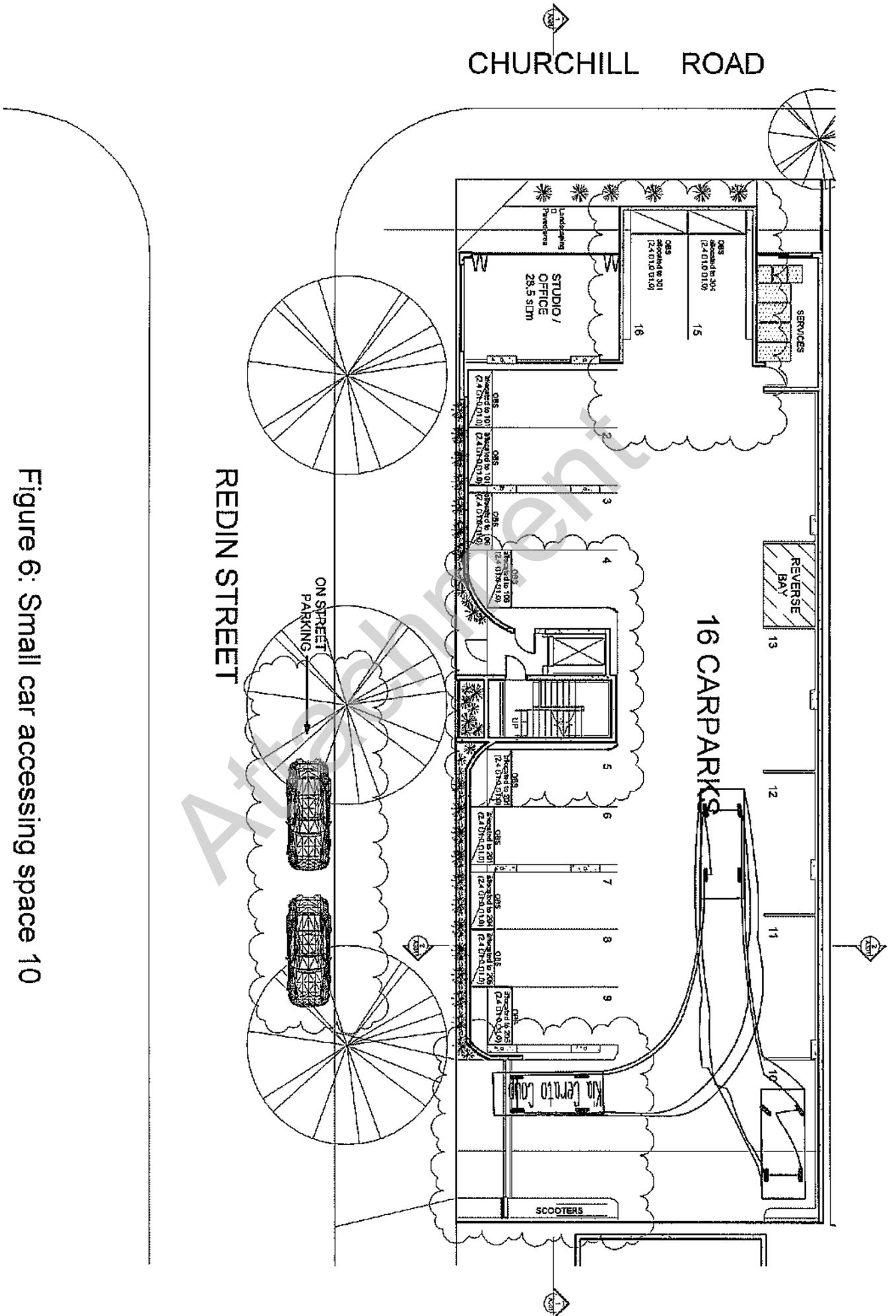


Figure 6: Small car accessing space 10

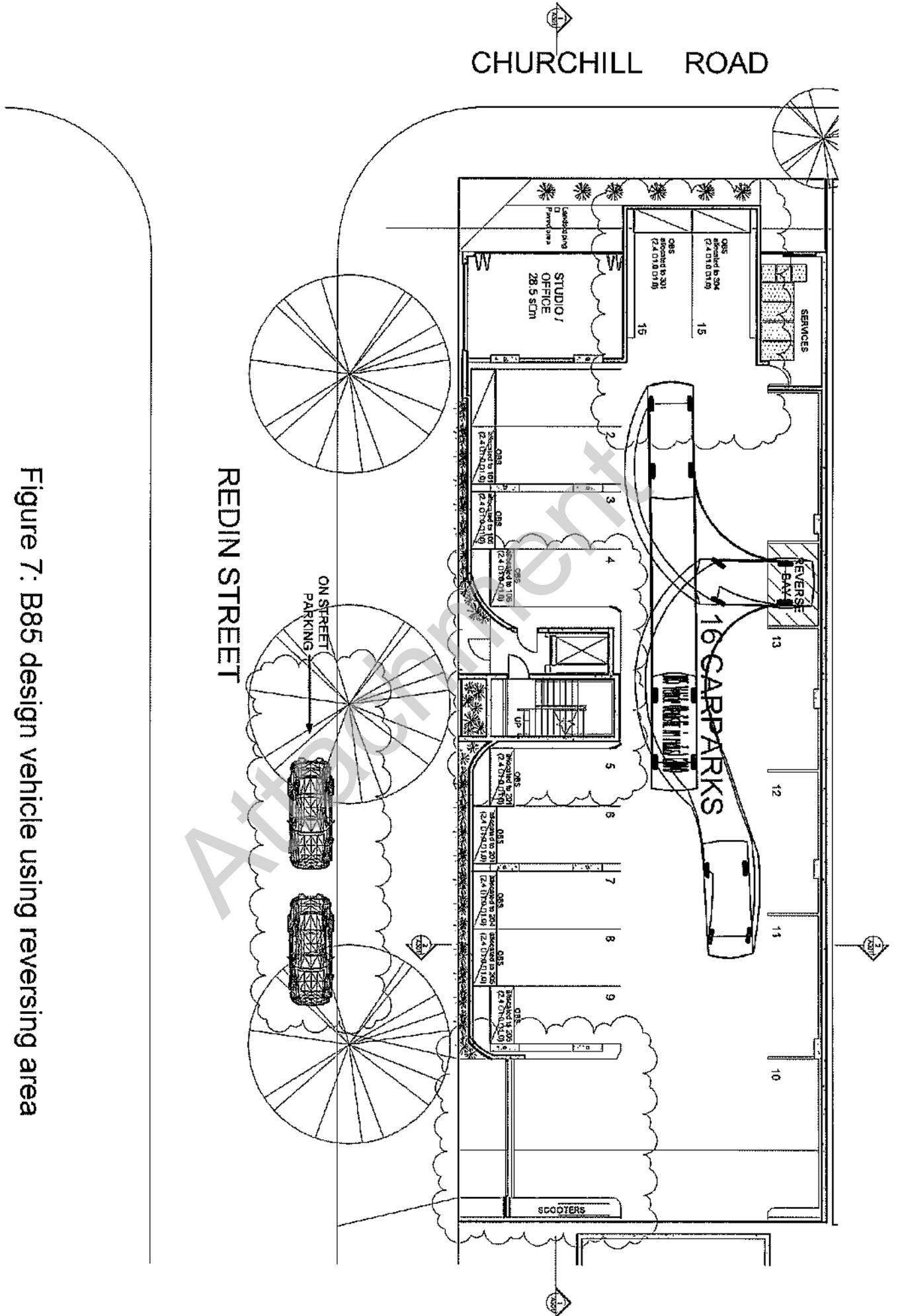


Figure 7: B85 design vehicle using reversing area

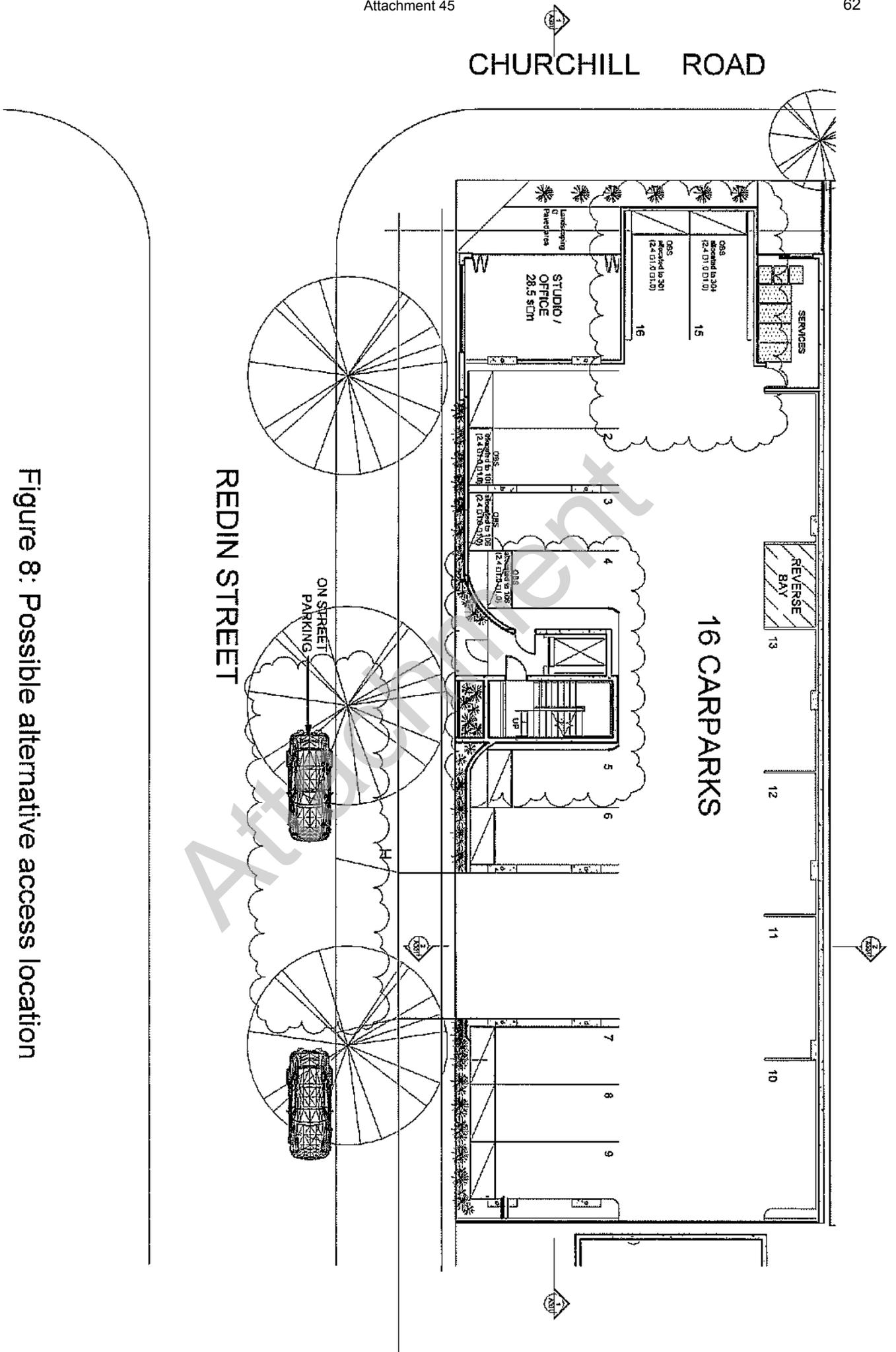


Figure 8: Possible alternative access location

GROUND FLOOR SCHEDULE

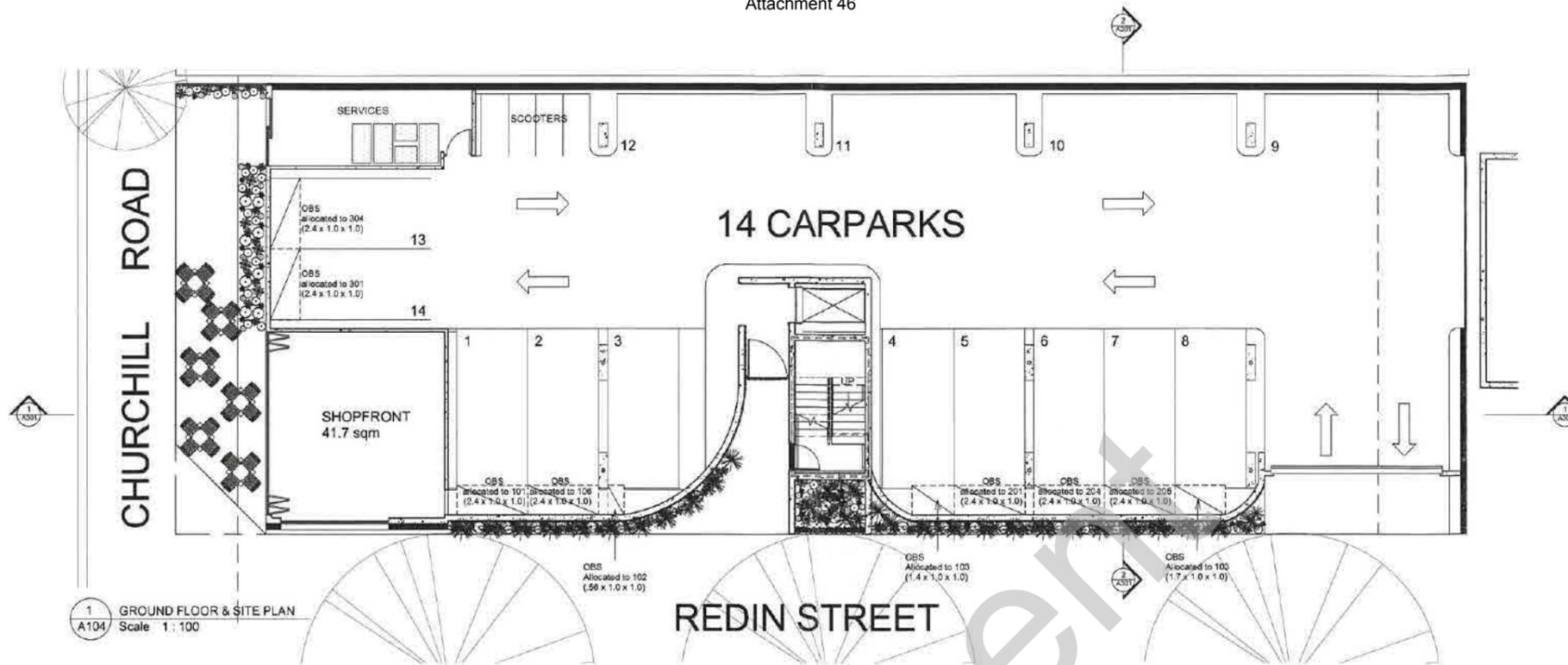
SITE AREA - 677m²
TOTAL CARPARKS 16 OFF

GROUND FLOOR

AREA SCHEDULE

CARPARK AND RESIDENTIAL ENTRY
LOBBY, LIFT + STAIR 39.0m²
SERVICES 19.9m²
CARPARK + ANCILLIARY 511.6m²
OBS - OVER BONNET STORAGE 20.46m³

GROSS FLOOR AREA (GROUND FLOOR)	611.7m ²
TOTAL GROSS APT. AREA	2102.7m ²



1 GROUND FLOOR & SITE PLAN
A104 Scale 1:100

APARTMENT SCHEDULE

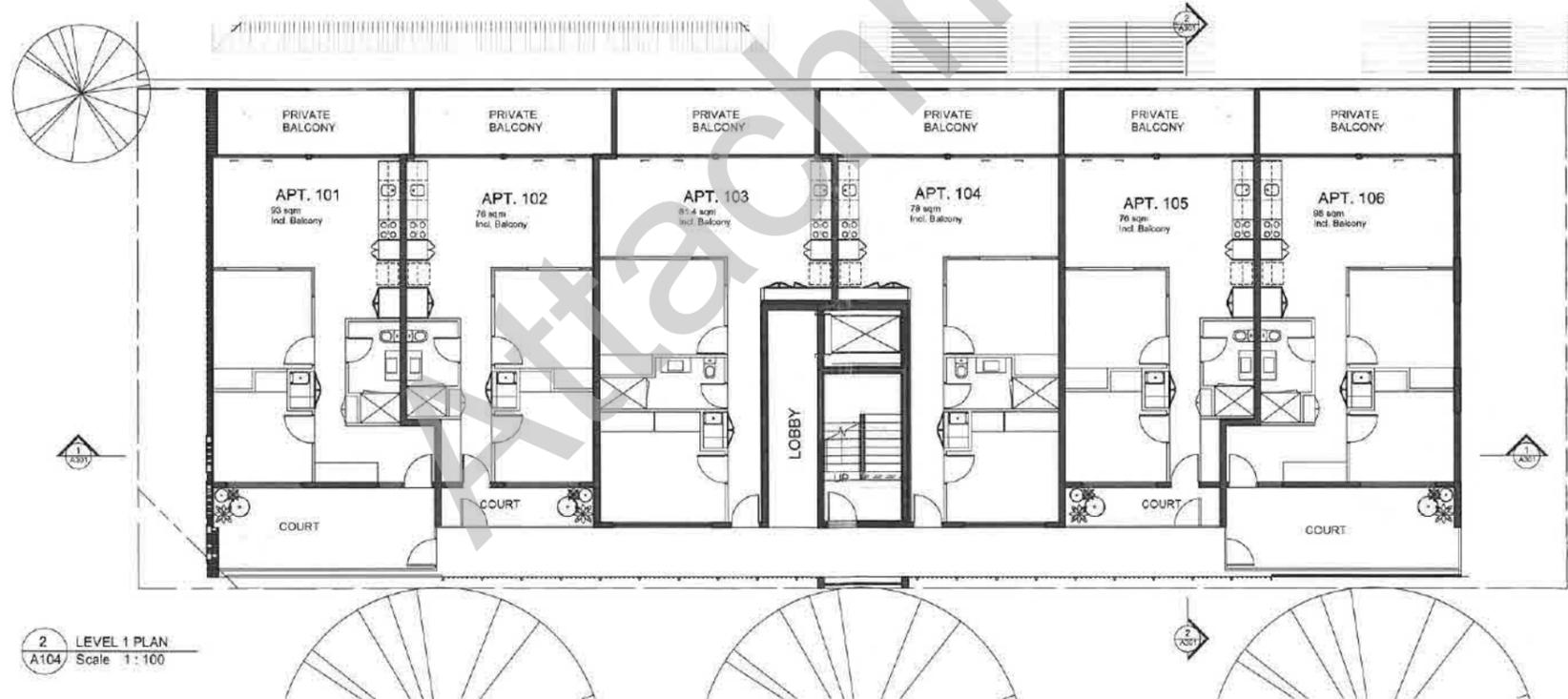
LEVEL 1 FLOOR BUILDING TOTAL 6 UNITS
16 OFF

APARTMENT LEVEL 1

AREA SCHEDULE

APARTMENTS (incl. balc) 504.3m²
LOBBY, LIFT & STAIR 29.3m²
WALKWAYS 42.2m²

GROSS FLOOR AREA (LEVEL 1)	573.1m ²
TOTAL GROSS APT. AREA	2102.7m ²



2 LEVEL 1 PLAN
A104 Scale 1:100

ITEM NO.: 6.1

TO: Development Assessment Panel (DAP) on 11 January 2016

FROM: Nathan Cunningham, Director Community, Planning & Communications

SUBJECT: Summary of Development Assessment Commission (DAC) Decisions and Proposals Greater than \$3 Million called in by the Coordinator-General

The summary of matters before and decisions by DAC together with proposals called in by the Coordinator-General is provided to the DAP for information purposes.

For the purpose of this report, the table below also includes matters before, considered or determined by the Inner Metropolitan Development Assessment Commission.

1. MATTERS BEFORE DAC

Development Application / Address	Nature of development	Process update
DA 050/237/2015 188 Churchill Road, Prospect	Land Division (Community Strata Title) This land division formalises an earlier land use consent granted by the DAC on 13 November 2014 for a Residential Flat Building comprising 15 Apartments and Roof Top Terrace on the subject land.	It is anticipated that the DAC will determine the application under delegation shortly.
DA 050/263/2015 44 Churchill Road, Ovingham	Land Division (Community Strata Title) This land division formalises an earlier land use consent granted by the DAC on 13 November 2014 for a Residential Flat Building comprising Ground Level Café, 18 Apartments and Roof Top Terrace on the subject land.	A variation application has been lodged and is currently being considered.
DA 050/438/2015 60-76 Main North Road, Prospect	Seven Storey Mixed Use Building (comprising motel, commercial tenancies and dwellings), with associated Basement Car Parking, Driveway and Landscaping	Category 2 public notification has ended. The application is currently being considered by DAC.
DA 050/500/2015 225 Prospect Road, Prospect	Two Four Storey Residential Flat Building comprising of 32 dwellings (16 dwellings in each building), with associated earthworks and landscaping.	Category 2 public notification has ended. The application is currently being considered by DAC.

189 Devonport Terrace, Prospect	Five Storey Residential Flat Building	Recently lodged with the DAC for assessment.
76-80 Hampstead Road, Broadview	Demolition of Existing Buildings and Construction of Petrol Filling Station and Shop with associated Canopy, Underground Tanks, Advertising Displays and Landscaping (Non-Complying)	<p>Recently lodged with the DAC for assessment. The application was referred to Council for comment on 23 December 2015 and will need to be referred to other agencies such as the Environment Protection Authority. Category 3 public notification is yet to commence.</p> <p>It is noted that because the application is non-complying in nature, Council's DAP will have the opportunity to both comment on the application through this referral process as well as undertaking its own assessment after the DAC has made its determination.</p>

2. RELEVANT DECISIONS BY DAC

No new proposals have been determined by the DAC.

3. MATTERS CALLED IN BY THE CO-ORDINATOR GENERAL

No new proposals have been called in by the Co-ordinator General.

ITEM NO.: 7.1
TO: Development Assessment Panel (DAP) on 14 December 2015
FROM: Nathan Cunningham, Director Community, Planning & Communications
SUBJECT: Summary of Court Appeals

The status of appeals is provided to the DAP for information purposes. Further clarification may be sought from staff during the meeting.

APPEALS

There are currently no appeals against development application decisions before the Environment, Resources and Development Court.