



# **Development Assessment Panel of City of Prospect**

**(Presiding Member: Mr David Blaikie)**

The meeting of the Development Assessment Panel will be held in the Civic Centre,  
128 Prospect Road, Prospect at **5.30pm Monday 15 June 2015**.

A handwritten signature in black ink, appearing to read 'Nathan Cunningham'.

**Nathan Cunningham**  
Director Community, Planning & Communications

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**Members:** Mr David Blaikie, Ms Alison Bowman, Mr David Cooke,  
Ms Cherie Gill, Mr Mark Groote, Ms Monica Lee,  
Mr Simon Weidenhofer

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## **A G E N D A**

1. **On Leave – Nil**
2. **Apologies – Nil**
3. **Confirmation of the Minutes of the Development Assessment Panel held on 11 May 2015.**
4. **Protocol**
  - 4.1 The Panel has adopted the protocol that only those agenda items on the Panel reports reserved by Members on a callover by the Presiding Member will be debated and the recommendations of all other items will be adopted without further discussion.

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## **5. Development Applications for Decision**

- 5.1 2A Richman Avenue, Prospect - Four Storey Residential Flat Building comprising 10 Dwellings with Associated Car and Bicycle Parking and Landscaping (DA 050/82/2015)

*(Pages 1 - 32, Recommendation pages 6 - 11)*

- 5.2 227-229 Prospect Road Prospect – Four Storey Residential Flat Building comprising 24 Dwellings with Associated Car and Bicycle Parking and Landscaping (DA 050/345/2014)

*(Pages 33 - 84, Recommendation pages 45 - 51)*

- 5.3 100 Churchill Road, Prospect - Variation to Development Application 050/467/2013 – Four Storey Residential Flat Building Comprising 24 dwellings (DA 050/116/2015)

*(Pages 85 - 112, Recommendation page 91)*

## **6. Other Reports**

- 6.1 Summary of Development Assessment Commission (DAC) Decisions and Proposals Greater than \$3 Million called in by the Coordinator-General

*(Pages 113 - 116)*

## **7. Matters Before the Environment, Resources and Development Court**

- 7.1 Summary of Court Appeals

*(Pages 117 - 118)*

## **8. Time, date and place of next meeting**

- 5.30pm Monday 13 July 2015 - Civic Centre, 128 Prospect Road, Prospect

## **9. Closure**

**AGENDA ITEM:** 5.1

**To:** Development Assessment Panel (DAP) on 15 June 2015

**From:** Scott McLuskey, Senior Development Officer Planning

**Proposal:** Four Storey Residential Flat Building comprising 10 Dwellings with Associated Car and Bicycle Parking and Landscaping (DA 050/82/2015)

**Address:** 2A Richman Avenue, Prospect (CT 5878/806)

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### **SUMMARY:**

Applicant: Trice c/o URPS

Planning Authority: Council

Referrals (Schedule 8): Not applicable

Public Notification: Category 2

Representations: 5 (Previously heard)

Submissions: 17

Respondent: (Previously heard)

Development Plan Version: Consolidated 12 February 2015

Zone and Policy Area: Urban Corridor Zone (Transit Living Policy Area)

Issues: Building Height, Car Parking, Residential Zone Interface, Density

**Recommendation:** Approval, subject to conditions

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### **1. EXECUTIVE SUMMARY**

- 1.1 The Development Assessment Panel (DAP) previously considered the subject proposal, a four storey apartment building, comprising 10 one and two bedroom dwellings at 2A Richman Avenue Prospect, at its meeting of 11 May 2015.
- 1.2 The DAP heard verbal presentations from nearby residents and their representatives, as well as from representatives of the applicant. Broadly, the DAP indicated support for the proposal while expressing a desire that the applicant reconsider several aspects of the development including landscaping, ground level street presentation, stormwater management and the waste storage area.
- 1.3 The applicant has subsequently provided amended proposal plans and additional details in responding to the concerns raised. The amended landscaping plan would provide improved planting areas and species selection, while the amended proposal plans would improve the appearance and interface between the street and the pedestrian entry in the

view of planning staff. A suitable stormwater management plan is now also provided. The proposal therefore warrants development plan consent.

## **2. BACKGROUND**

2.1 The DAP considered a proposal for the construction of a four storey residential flat building comprising 10 dwellings at their meeting of 11 May 2015. The application was deferred for consideration at a subsequent meeting of the DAP, pending the receipt of further information on the following matters:

1. *Landscaping detail, including consideration of the width of the landscaping strip adjacent the eastern boundary*
2. *Presentation of the building to the street at ground level, including consideration of materials, finishes and pedestrian interface*
3. *Stormwater capture, re-use, management and disposal details*
4. *Location of waste storage area.*

2.2 The report and attachments previously considered by the DAP at its 11 May 2015 meeting remain available for viewing on Council's website ([www.prospect.sa.gov.au](http://www.prospect.sa.gov.au)).

## **3. PROPOSAL**

3.1 In response to the matters described above, the applicant has provided further supporting documentation from Matthew King of URPS (refer **Attachments 1-3**), amended proposal plans (refer **Attachments 4-10**), amended landscaping plans (refer **Attachments 11-15**) and a stormwater management plan prepared by MLEI Engineers (refer **Attachments 16-21**).

## **4. PUBLIC NOTIFICATION**

- 4.1 Where an application is varied following the giving of notice under Part 6 of the Development Regulations 2008, the relevant authority should consider whether the variations made are substantial in nature. If the relevant authority is of the opinion that the variations are not substantial, any action otherwise required under Part 6 need not be repeated.
- 4.2 The variations made to the proposal include alterations to the entry area of the building, additional landscaping and further information regarding stormwater management. As these changes are not substantial, a new Category 2 notification process was not undertaken.
- 4.3 The author of each representation and submission was invited to review the response prepared by the applicant and has been invited to attend the meeting of the DAP at which the variations will be considered. They have also been advised that no party will have a formally recognised right to present to the panel on its reconsideration of the proposal.

## **5. PLANNING COMMENTARY**

- 5.1 The application involves building work and therefore an application to Council is required. The proposal is neither a complying nor a non-complying development with reference to Principles of Development Control 20 and 21 of the Urban Corridor Zone and is therefore to be considered on its merits against the relevant provisions of Council's Development Plan.

- 5.2 Pursuant to Section 35(2) of the *Development Act 1993*, a development that is assessed by the Council as being seriously at variance with the Development Plan must not be granted consent. To this end, the Panel must determine whether the proposal is seriously at variance with the Development Plan prior to making a decision on the application.
- 5.3 In determining the application, the Panel must assess the proposal against the relevant provisions of the Development Plan, while taking into consideration the valid representations received as well as the applicant's response. The Panel may refer to the other submissions received, but should consider carefully the weight that it places upon these submissions.
- 5.4 It is noted that the summary section of the previous report to the DAP incorrectly identified the relevant policy area as the Boulevard Policy Area. This typographical error did not influence the assessment of the proposal as described in the report. It is also acknowledged that density was not identified as an issue in the report summary, although representors had raised the proposal density as being of concern. Further discussion on density is subsequently provided herein, though it is noted that the DAP had not sought that the height or density of the proposal be revisited.

## **6. PLANNING ASSESSMENT**

### **6.1 Dwelling Density**

- 6.1.1 Given this matter was a focus of representor concerns to the recent DAP meeting and for the benefit of the DAP, further commentary is provided regarding the assessment of the proposal in relation to desirable residential densities within the Transit Living Policy Area of the Urban Corridor Zone.
- 6.1.2 In 2006, Planning SA prepared a handbook entitled *Understanding Residential Densities: A pictorial handbook of Adelaide examples* to assist in defining and understanding the residential densities described in the Planning Strategy for Metropolitan Adelaide. Relevantly, the following excerpt provides an overview of the various density thresholds (in dwellings per hectare) described in the strategy:

	<b>Approx Gross Density</b>	<b>Approx Net Density</b>
<b>Very Low Density</b>	< 11 dw/ha	< 17 dw/ha
<b>Low Density</b>	11-22 dw/ha	17-33 dw/ha
<b>Medium Density</b>	23-45 dw/ha	34-67 dw/ha
<b>High Density</b>	> 45 dw/ha	> 67 dw/ha

- 6.1.3 Further to this, the Department of Planning and Local Government maintain the South Australian Planning Policy Library, last updated in November 2011. As the Planning Policy Library is maintained for the purposes of informing amendments to Development Plans throughout South Australia, its content can be of assistance in later interpreting the final provisions that appear therein.
- 6.1.4 Relevantly, the Urban Corridor Zone module of the Planning Policy Library provides that this zone supports:

*"an innovative mix of medium (from around 45-70 dwellings per hectare net residential site density) and high density (70-200 dwellings per hectare net residential site density) residential and mixed use development..."* (emphasis added)

- 6.1.5 It is anticipated that the Transit Living Policy Area of the City of Prospect will be a medium density residential area supported by local shops, offices and community land uses (TLPA Objective 1). To achieve this broader objective, “*residential development will take place at medium to high densities, requiring the replacement of existing detached dwellings with apartment and terrace style dwellings and mixed use buildings, desirably two to three storeys in height*” (UCZ TLPA DCS).
- 6.1.6 Setting aside the analysis of building height, the southern portion of the Transit Living Policy Area presently comprises a net area of approximately 32,093m<sup>2</sup>. Through the use of aerial imagery and Council’s rate records, it has been established that 24 dwellings are present within this area. The existing net residential density of this area is therefore 7.47 dw/ha. This would therefore currently constitute a very low density.
- 6.1.7 Using these numbers we can establish that the Development Plan seeks an additional 124-225 dwellings to be constructed within this portion of the Transit Living Policy Area so as to achieve its desired character (in respect of density targets). Presently, 23,951m<sup>2</sup> of this area (or 74.6%) is used for non-residential purposes only.
- 6.1.8 Given that many sites within the Policy Area are not, and are unlikely to be, developed for residential purposes, it is clear that Objective 1 cannot be achieved without a higher density of development on some sites within the Policy Area.
- 6.1.9 At a net residential site density of 187 dwellings per hectare for the site of 2A Richman Avenue, the proposal would therefore achieve high residential development density, which is desirable in order to create an area of overall medium residential density. If the proposed building were constructed, the overall net residential density of the area would increase to 10.28 dw/ha (or very low density), well below the 45-70 dw/ha desired. It is therefore considered that this proposal will assist the Policy Area in heading towards its desired future character as articulated in the Council’s Development Plan.

## 6.2 Landscaping

- 6.2.1 The DAP previously resolved to defer the application so that the applicant could provide additional landscaping detail, including consideration of the width of the landscaping strip adjacent the eastern boundary.
- 6.2.2 Amended landscaping plans have been provided by the applicant (refer **Attachments 11-15**). The variations include a considerably greater extent of tree planting within the subject site, principally through the addition of either Ornamental Pear or Pinnacle Lilly Pilly plantings to the eastern boundary of the site. Landscaping areas forward of the building have been increased in size with additional detail provided on the anticipated height and canopy width of the species selected.
- 6.2.3 The amended landscaping plan would also extend a wire trellis system to the full height of the eastern elevation of the building. This is proposed to allow vines growing over the existing landscape canopy to continue growing up this wall of the building. It is noted that the successful growth of the eastern boundary plantings would also assist in softening the appearance of the building as viewed from the adjacent property. It is recommended that a condition be imposed regarding the continuous maintenance of all landscaped areas.

- 6.2.4 The applicant has proposed an example replacement street tree in the amended landscaping plan. It is noted that the removal and replacement of the street tree would be considered by Council's Infrastructure and Assets Officer through the standard Council process. At that time an assessment of the tree will be undertaken by Council's principal Street Tree Consultant to advise upon the cost of removal and of the contribution to be provided by the applicant for the lost amenity of the tree that will be used in its replacement. As this contribution amount may allow for the planting of multiple replacement trees, the pre-emptive acceptance of the applicant's suggestion may lead to a poorer outcome.
- 6.2.5 It is anticipated that if established in accordance with the landscaping plan provided the proposal would provide landscaping that will enhance the built form and provide an attractive transition between the public and private realms. Amendments made will soften the impact of this building (the first of the kind in this locality) and provide an appropriate green vertical element to the overall design.

### **6.3 Design and Appearance**

- 6.3.1 The DAP previously resolved to defer the application so that the applicant could reconsider the presentation of the building to the street at ground level, including consideration of materials, finishes and pedestrian interface.
- 6.3.2 In response, the applicant has altered the configuration of the entry gate and letterboxes such that the landscaping area adjacent the entry could be increased in width. The entry canopy would remain as previously proposed, though would now extend across the pedestrian access way to the planter box. A timber slatted screen of increased height and width would provide pedestrian level visual interest in a form coherent with the remainder of the façade of the building. The amended landscaping plan would provide a denser and more varied planting to the east of the vehicle access way.
- 6.3.3 While variations to the ground level of the building are reasonably subtle, increased and higher quality landscaping combine with an improved pedestrian connection to the footpath to improve the presentation of the building to the street at ground level for visitors and passers-by, without comprising the architectural merits of the remainder of the building's façade. Overall, the form and presentation of the building is of high quality and is supported.

### **6.4 Stormwater Management**

- 6.4.1 The DAP previously resolved to defer the application so that the applicant could provide details regarding stormwater capture, re-use, management and disposal details.
- 6.4.2 The applicant has prepared a stormwater management plan and accompanying calculations that provide for the satisfactory detention on, and disposal from, the subject site in the opinion of engineering staff. At the advice of engineering staff, it is recommended that an additional condition regarding the quality of water run-off from the car park be imposed.
- 6.4.3 Notwithstanding this, it is recommended that the applicant be encouraged by way of advisory note to further examine opportunities for rain-water re-use within the subject site during the preparation of working drawings.

## **6.5 Waste Management**

- 6.5.1 The DAP previously resolved to defer the application so that the applicant could reconsider the location of the waste storage area.
- 6.5.2 No amendment has been made to the size or location of the waste storage area. In response to the above, the applicant has identified that the waste storage areas had previously been minimised to the extent possible, and is located conveniently to encourage residents to make proper use of the screened area for the storage of waste (and the bins).
- 6.5.3 The applicant further notes that the waste storage area is located adjacent the driveway of the adjoining property, is well separated from adjacent areas of private open space and windows to the dwelling. While staff remain supportive of the approach to waste management, a condition requiring appropriate maintenance and cleaning of the waste storage area is recommended.

## **7. CONCLUSION**

- 7.1 Whilst not listed as a reason for deferral, staff have further analysed the appropriateness of the net residential density of the proposal and consider that it accords with the desires of the Council's Development Plan.
- 7.2 The variations to the proposal improve landscaped areas as viewed from the street and would further soften the building when viewed from the adjoining residential property. In concert with improvements to the materiality and configuration of pedestrian access areas, the variations provide an enhanced street level interface to the building.
- 7.3 A stormwater management plan has been provided that demonstrates appropriate detention and disposal of water in relation to the subject site. While the waste management area would remain unchanged, the applicant has provided sound reasoning for their selection of this location and an additional condition would further minimise any potential impact to the adjoining residential property.
- 7.4 Resultantly, and following the previous consideration of the proposal by the DAP at its meeting on 11 May 2015, the application is considered to be relatively consistent with the relevant provisions of the Prospect (City) Development Plan and warrants the granting of development plan consent, subject to appropriate conditions.

## **8. RECOMMENDATION**

It is recommended:

That with reference to the relevant provisions of the Prospect (City) Development Plan, the zoning of the land within which the proposed development is situated and the locality within which the land is situated, the Panel resolves that development application 050/82/2015 is not seriously at variance with the Development Plan and as such a decision shall be made on the merits of the application; and

That pursuant to the *Development Act 1993*, as amended, Development Plan Consent be approved to DA 050/82/2015 from Trice for a Four Storey Residential Flat Building comprising 10 Dwellings with Associated Car and Bicycle Parking and Landscaping at 2A Richman Avenue Prospect (CT 5878/806), subject to the following reserved matters, and draft conditions and notes that may be added to or altered following satisfactory resolution of reserved matters by Council administration:

**Conditions:**

1. The development shall take place in accordance with plans and details stamped by Council relating to Development Application Number 050/82/2015, except as modified by any conditions detailed herein. All works detailed in the approved plans and required by conditions are to be completed prior to the occupation of the approved development.
2. All driveways, parking and manoeuvring areas must be formed, surfaced with concrete, bitumen or paving, and be properly drained. The surfacing of the driveway and drainage shall be maintained to the reasonable satisfaction of Council thereafter.
3. The drainage system shall be designed, installed and maintained at all times thereafter to ensure that water from the site does not:
  - a) Flow or discharge onto adjoining properties;
  - b) Flow across the surface of footpaths or public ways;
  - c) Affect the stability of any building; or
  - d) Create unhealthy or dangerous conditions on the site or within any building.
4. Stormwater quality measures such as a gross pollutant trap shall be implemented to ensure that all surface run-off, stormwater or other liquid discharging from the site, is free of site contaminants including (but not limited to) oils, grease, fuels, rubbish, litter and silt.
5. The features and materials set out in Drawing Numbers 'A3.01', Revision 'A2', and 'A3.02', 'A3.03' and 'A3.04' Revision 'A1' approved herein are essential elements of the building and shall be established and maintained at all times to the reasonable satisfaction of Council.
6. Air-conditioning units and solar hot water heaters shall be provided with screening devices designed to complement the colours, materials and finishes of the building approved herein, and shall be sited to adequately screen the units from view to the reasonable satisfaction of Council. The noise generated by the units will not exceed applicable Environment Protection Authority guidelines during operation.
7. All landscaping areas shall be maintained at all times to the reasonable satisfaction of Council. The applicant or the persons for the time being making use of the subject land shall cultivate, tend and nurture the landscaping, and shall replace any landscaping that becomes diseased or dies.
8. The landscaping shall be planted in accordance with the approved plans prior to occupancy of the development.
9. Driveways, car parking spaces, manoeuvring areas and landscaping areas shall not be used for the storage or display of materials or goods including waste products and refuse.
10. All fences and waste storage areas (including associated screening devices) on the site must be maintained, kept tidy, free of graffiti and in good repair and condition at all times.
11. Permanently fixed clotheslines shall not be permitted within 1.5m of the edge of each balcony.
12. The building must be maintained, kept tidy, free of graffiti and in good repair and condition at all times.

13. All car parking spaces must be line-marked in accordance with the approved plans, with visitor spaces clearly marked, and to comply with the Australian/New Zealand Standard for Parking Facilities (Part 1: Off-street Car Parking (AS/NZS 2890.1:2004) prior to occupation.
  14. The surfacing of the car park, line marking and directional arrows shall be maintained to the reasonable satisfaction of Council at all times.
  15. Driveways, parking and manoeuvring areas shall be lit in accordance with the Australian Standard for Lighting for Roads and Public Spaces (AS1158.1 and AS1158.3) during the hours of darkness that they are in use and accessible by the general public.
  16. During construction of the development approved herein, measures will be implemented to ensure that the construction works do not result in an unreasonable impact on occupiers of adjacent properties or pollution of existing infrastructure through drag-out or stormwater runoff. Measures shall include as necessary:
    - a) A hard surface and controlled washing zone at the entry/exit points to the site, designed to reduce the potential for mud and material dragged out by construction vehicles; and
    - b) Containment of stormwater run-off within the site, which if being discharged into the stormwater system will be filtered to the satisfaction of Council; and
    - c) Reduction of the potential for dust and other airborne particles by the use of water sprinklers and/or other means of containment; and
    - d) The establishment of an appropriate storage compound for waste materials and litter. No building waste material shall be stored outside of the storage compound or similar industrial bin; and
    - e) All mechanical equipment shall be used in a manner to minimise the potential for noise pollution and ensure compliance with the requirements of the Environment Protection (Noise) Policy.
  17. Footpaths adjacent to the site are to be kept in a safe condition for pedestrians at all times during construction works. All driveways and footpaths traversed by vehicles using the site are to be maintained in a reasonable condition for the duration of the works, and are to be reinstated to the satisfaction of Council on completion of the works.
- No obstruction of the footpath or roadway may occur without the prior permission of Council. For further advice, please contact Council's Infrastructure and Environment Department on 8269 5355.
18. To ensure compliance with applicable standards as described in the Environment Protection (Noise) Policy established under the Environment Protection Act, construction activities shall only take place between the hours of 7:00am and 7:00pm, Monday to Saturday inclusive, and not on Sundays or public holidays.

**Advisory Notes:**

- (1) Pursuant to Section 86(1)(a) of the Development Act, 1993, you have the right of appeal to the Environment, Resources and Development Court against either 1) a refusal of consent or 2) any condition(s) which have been imposed on a consent. Any such appeal must be lodged with the Court within two (2) months from the day on which you receive this notification or such longer period as may be allowed by the Court.

The Environment, Resources and Development Court is located in the Sir Samuel Way Building, Victoria Square, Adelaide SA 5000 (Postal Address: GPO Box 2465, Adelaide SA 5001).

- (2) The development plan consent granted herein is effective for a period of twelve (12) months from the date of the decision. Unless Council extends this period, building rules consent is required within this time or the consent will lapse.

Any request for an extension of the operative period of the consent must be submitted to Council in writing, accompanied by the applicable fee.

- (3) Further application pursuant to the Local Government Act shall be made to Council's Infrastructure Assets and Environment Department for the proposed crossover prior to construction activities occurring.

Road/Kerbing/Footpath Works will need to be inspected by an Assets and Infrastructure Officer to determine they have met all relevant requirements. All work including line marking will be the responsibility of the applicant as will the reinstatement of any damaged Infrastructure / Services related to these works. All works will be carried out at the cost to the applicant.

- (4) During the preparation of working drawings you are encouraged to maximise the capture and re-use of rainwater within the building and associated landscaping.
- (5) Prior to the commencement of construction of the development herein approved, it is strongly recommended that you employ the services of a licensed Land Surveyor to carry out an identification survey of the subject land and to peg the true boundaries, to ensure that building work will be either on the true boundaries or the specified distance from the true boundaries of the subject land, as the case may be. Failure to correctly site the development on the land in accordance with the plans approved herein would constitute a breach of the *Development Act 1993*. Any amendments required to the approved plans as a result of the survey are to be submitted to Council for approval prior to works commencing.
- (6) You are encouraged to consult with adjoining property owners before commencing any work, to assist in minimising nuisance or inconvenience caused during construction.
- (7) You are required to give formal notification to, and consult with, the adjoining property owner if you are removing, replacing or altering an existing fence or building a freestanding wall along the common boundary that would, for all purposes, be a dividing fence (Section 5 of the *Fences Act 1975*).
- (8) If you (the building owner) are undertaking building work that affects the stability of other land or premises, namely:
  - an excavation which intersects a notional plane extending downwards at a slope of 1 vertical to 2 horizontal from a point 600mm below natural ground level at a boundary with an adjoining site (as depicted in figure 1); or
  - an excavation which intersects any notional plane extending downwards at a slope of 1 vertical to 2 horizontal from a point at natural ground level at any boundary between 2 sites (not being a boundary with the site of the excavation), where the boundary is within a distance equal to twice the depth of the excavation (as depicted in figure 2); or

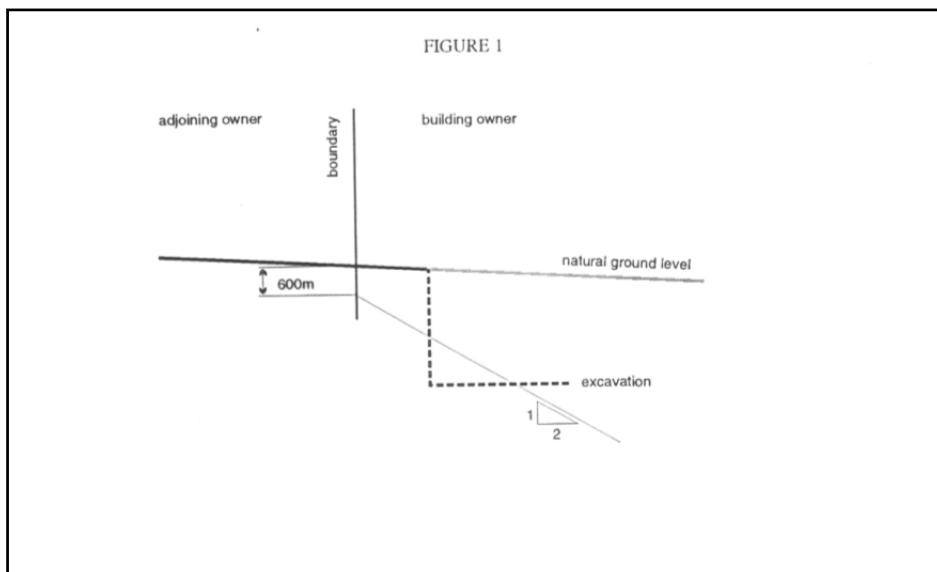
- any fill which is within 600mm of an adjoining site, other than where the fill is not greater than 200 millimetres in depth (or height) and is for landscaping, gardening or other similar purposes;

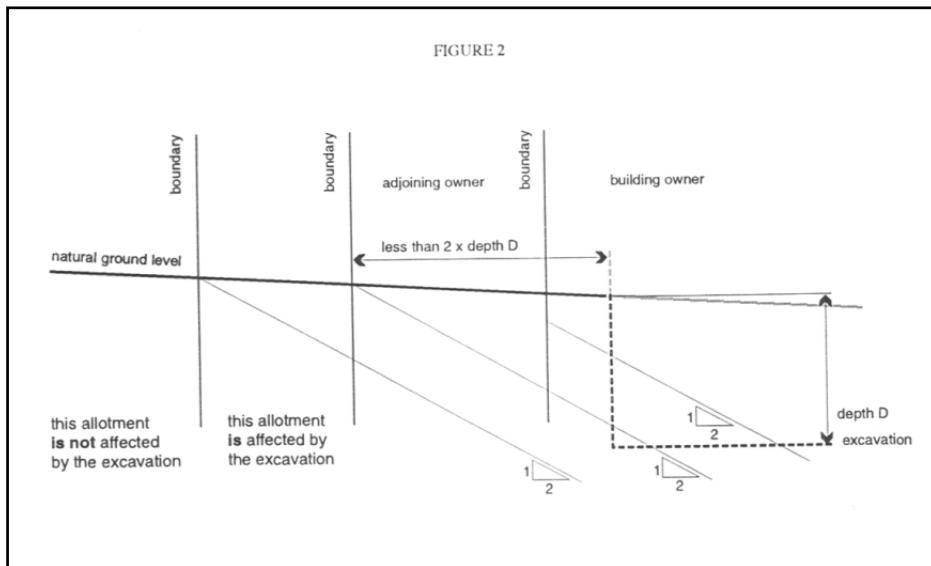
Then you (the building owner) must, at least 28 days before the building work is commenced:

- serve on the owner of the affected land or premises a notice of intention to perform the building work and describing the nature of that work; and
- you must take such precautions as may be prescribed to protect the affected land or premises and must, at the request of the owner of the affected land or premises, carry out such other building work in relation to that land or premises as that adjoining owner is authorised by the regulations to require.

If you fail to comply with these notification requirements, then you are guilty of an offence with a maximum penalty of \$10 000.

You may apply to the Court for a determination of what proportion (if any) of the expense incurred by you in the performance of the building work requested by the owner of affected land or premises (under subsection (b) above) should be borne by the owner of that land or premises, and you may recover an amount determined by the Court from the owner of the affected land or premises as a debt.





**Scott McLuskey**  
Senior Development Officer Planning

Ref: 2015-0011



1 June 2015

Mr Scott McLuskey  
Senior Development Officer – Planning  
City of Prospect

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By email: [scott.mcluskey@prospect.sa.gov.au](mailto:scott.mcluskey@prospect.sa.gov.au)

Dear Scott

**2A Richman Avenue Prospect – DA 050/0082/2015**

I refer to your letter dated 19 May 2015.

**CDAP Deferral Items**

The Council's Development Assessment Panel (CDAP) deferred the above application for further consideration in respect to the following:

1. *Landscaping detail, including consideration of the width of the landscaping strip adjacent to the eastern boundary.*
2. *Presentation of the building to the street at ground level, including consideration of materials, finishes and pedestrian interface.*
3. *Stormwater capture, re-use, management and disposal details.*
4. *Location of waste storage area.*

The proposal has been amended in order to respond and address the above matters. Additional information is provided regarding stormwater management. Copies of amended plans and the stormwater management plan were forwarded to your office on 29 May 2015. This correspondence is to be read in conjunction with that submitted information.

**Discussion**

The applicant's landscape architect, LCS Landscapes, originally proposed a vine or creeper system along the eastern boundary that would attach to the steel columns which attached to the building to create a landscape canopy. Following the CDAP's resolution at its last hearing, we have further reviewed this matter of landscaping.

The eastern boundary landscaping detail has been amended in so far as:

- The vines planted in the ground along the eastern boundary are retained and will continue to attach to the columns but are to now advance up and over this column structure and attach to the building façade onto a wire trellis system. The eastern elevation of the building will, over time, have a softened and 'green' appearance as it will be largely covered in creepers.
- In addition, LCS Landscapes has also advised there is sufficient space along the eastern landscape strip to support tree plantings along the boundary. Depending on planting conditions and final site selections, it is proposed to plan a row of either '*Pyrus calleryana Capital*' or '*Syzygium austral*'

*Pinnacle'*. Both species are well known for their excellent growth within restricted, narrow spaces around car parks and along fences. They both provide exceptional screening properties. In addition to the creepers, the proposed plantings, with heights that will range between 8 – 10 metres (approximately) when fully grown, will be ideal in further softening the appearance of the development when viewed from the east in particular, but also generally from the Richman Avenue streetscape and St Helens Reserve.

- Furthermore, in respect of landscaping:

- > A species called '*Murraya paniculata*' is proposed along the north boundary where a row of 2 metre high screening trees are to sit between the proposed bike racks and rainwater tanks and will be visible, albeit to a limited extent, to the street through the car park beyond the access ramp.
- > Further details are provided regarding the easternmost street fronting landscaping bed which will accommodate various low-level shrubs '*Lomandra tankia*' and a tree '*Lagerstroemia Natchez*' which will grow up to 6 metres in height and provides a wide, voluminous canopy spread of up to 7 metres.
- > The centralised landscaping bed has been increased in size to now approximately 3 metres in width in turn reducing hard surface paving and will accommodate a bed of low lying shrubs '*Zamia furfuracea*'.
- > The street tree to be replaced will be planting to the west of the proposed driveway entrance. The applicant's landscape architect recommends an advanced street tree in the form of a '*Koelreuteria paniculata*' planted within a constructed tree pit with a root director and deep watering provisions to enhance growth and stabilize long term health. Ultimately details of this street tree will be at the discretion of Council's arborists and we welcome any input Council's wishes to provide on this matter.

The above additions to the proposed landscaping will enhance the streetscape appearance of the development by providing more greenery and further softening the hard edges of building elements which the applicant's architect has in any event sought to reduce through articulation and materiality. Approximately now one third of the frontage will be landscaped with the remaining frontage space set aside for the mandatory driveway entrance, mail box facilitates, hydrant and pedestrian entry.

Further modifications to the streetscape presentation have occurred to improve street presentation including an approximately 6.3 metre wide timber screen which will sit behind the central landscape planter bed and screen the undercroft car park. This timber screen matches those provided on the façade upon the balconies on levels one and two. Other previous features will remain and the proposal will therefore appropriately engage and front the Richman Avenue streetscape, given the use of ample glazing/balconies, and the primary pedestrian entry point which is visible and connected to the footpath.

Overall, 'in-active' areas across the frontage have been minimised. The vehicle entry has been made as practically as small as possible under relevant Australian Standards. The waste storage area is not visible to the streetscape given it will sit behind the proposed landscaping on the eastern end of the site frontage. Its location will remain as per the previous scheme which is most appropriate for allowing residents to access and conveniently wheel waste bins to/from the road reserve. As previously noted, its size is minimised through a practical allocation of bins for an apartment development of this scale. I note this waste storage area is enclosed on all sides including 2.1 metre

fencing to the east and 1.8 metre high perforated screening on the west to maintain visibility of car movements in and out of the site.

With respect to stormwater, the applicant has instructed MLEI Engineers to prepare a suitable stormwater management plan for the site. This plan indicates how all stormwater will be drained, captured and discharged to the street. A 2,000 litre rainwater storage tank is provided for watering landscaping. Discharge rates will achieve Council's required 1 in 10 year ARI pre-development flow rate.

### Summary

The amended submission and provision of additional information addresses the pertinent concerns of CDAP. A concerted effort has been made by the applicant to increase the volume of landscaping both along the streetscape but in particular along the eastern boundary.

In-active areas across the facade have been minimised and the ground level is heavily landscaped with varied materials including render and timber and steel, provided to enhance appearance. Five separate balconies will overlook Richman Avenue to provide passive surveillance. The waste storage area remains as is, however this was in any event minimised in size/area and screened from view and not adjacent any private open space areas which may give rise to odour issues. A stormwater management plan is in place which achieves Council's requirements.

We trust the above addresses the concerns of the CDAP. We look forward to you presenting this further considered scheme to the next available CDAP meeting.

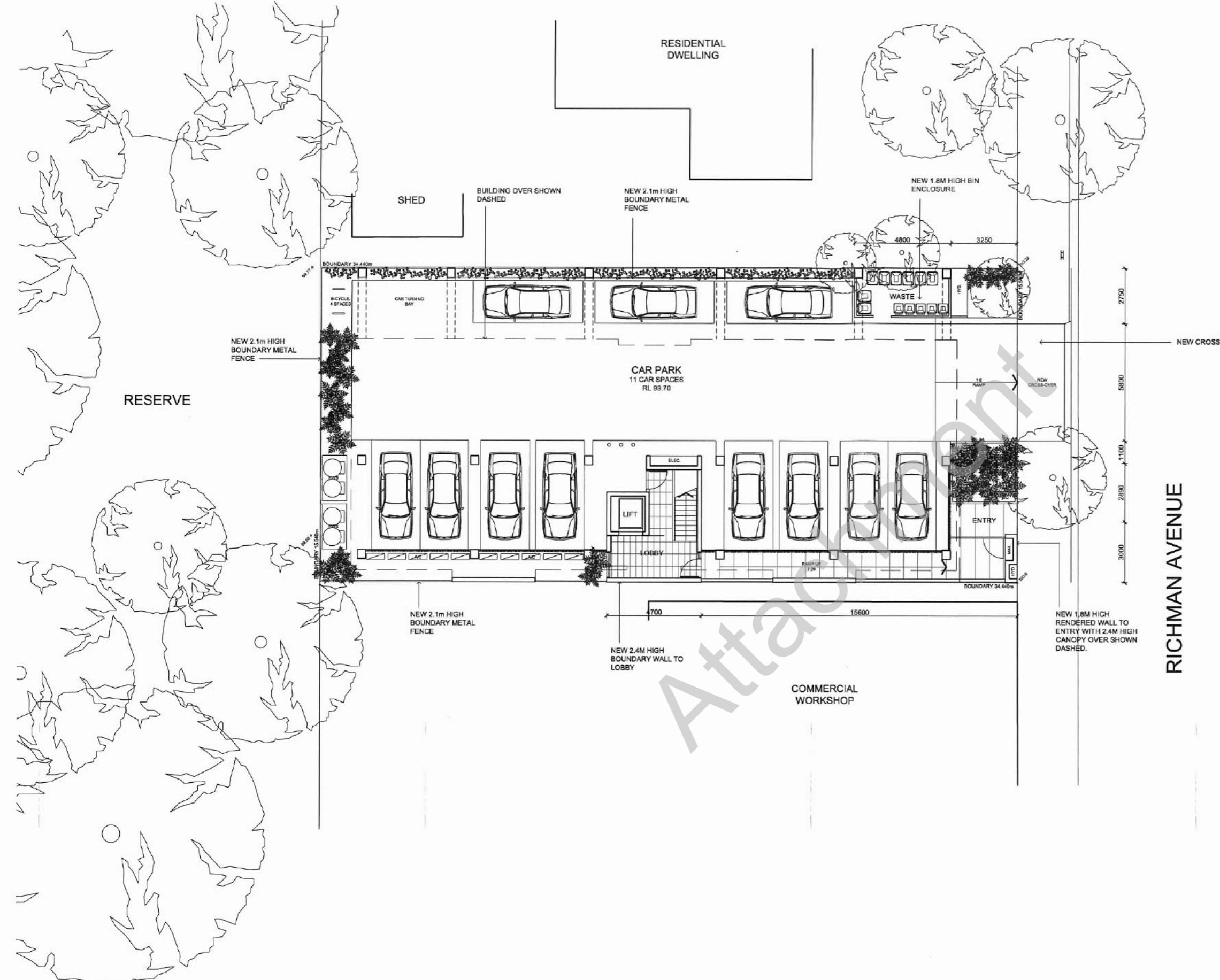
Should you have any questions, please do not hesitate to call the undersigned on 8333 7999.

Yours sincerely



Matthew King MPIA CPP  
Director

Enc

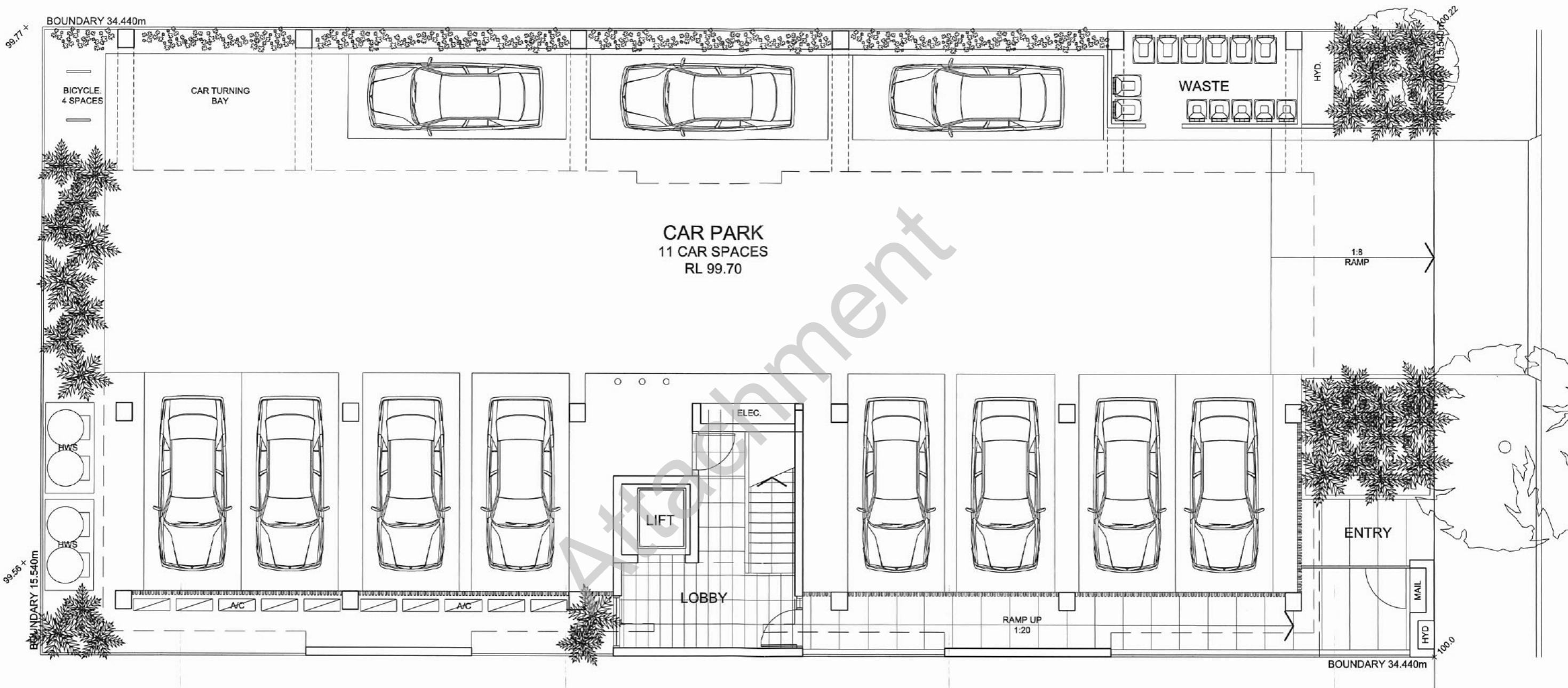


A2 PLANNING APPLICATION - REVISED 25.05.15  
 A1 PLANNING APPLICATION - REVISED 14.04.15  
 A PLANNING APPLICATION 03.03.15

**ENZO CARO SCIO ARCHITEC TURE**

16 Stephens Street Adelaide SA 5000  
 enzo@enzocaroscio.com  
 mobile. 0421 331 783

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A2 PLANNING APPLICATION - REVISED 25.05.15  
A1 PLANNING APPLICATION - REVISED 14.04.15  
A PLANNING APPLICATION 03.03.15

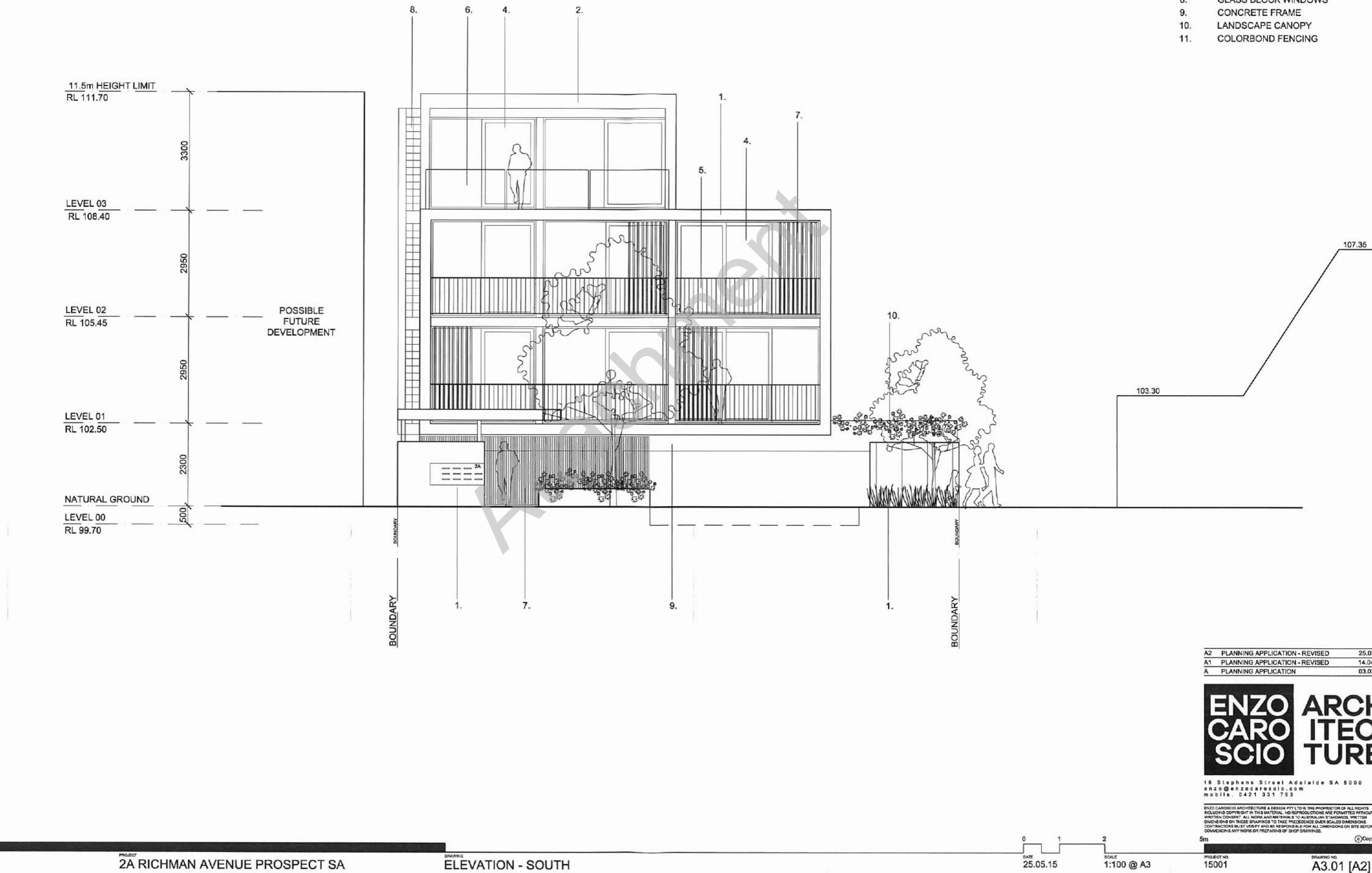
**ENZO ARCH  
CARO ITEC  
SCIO TURE**

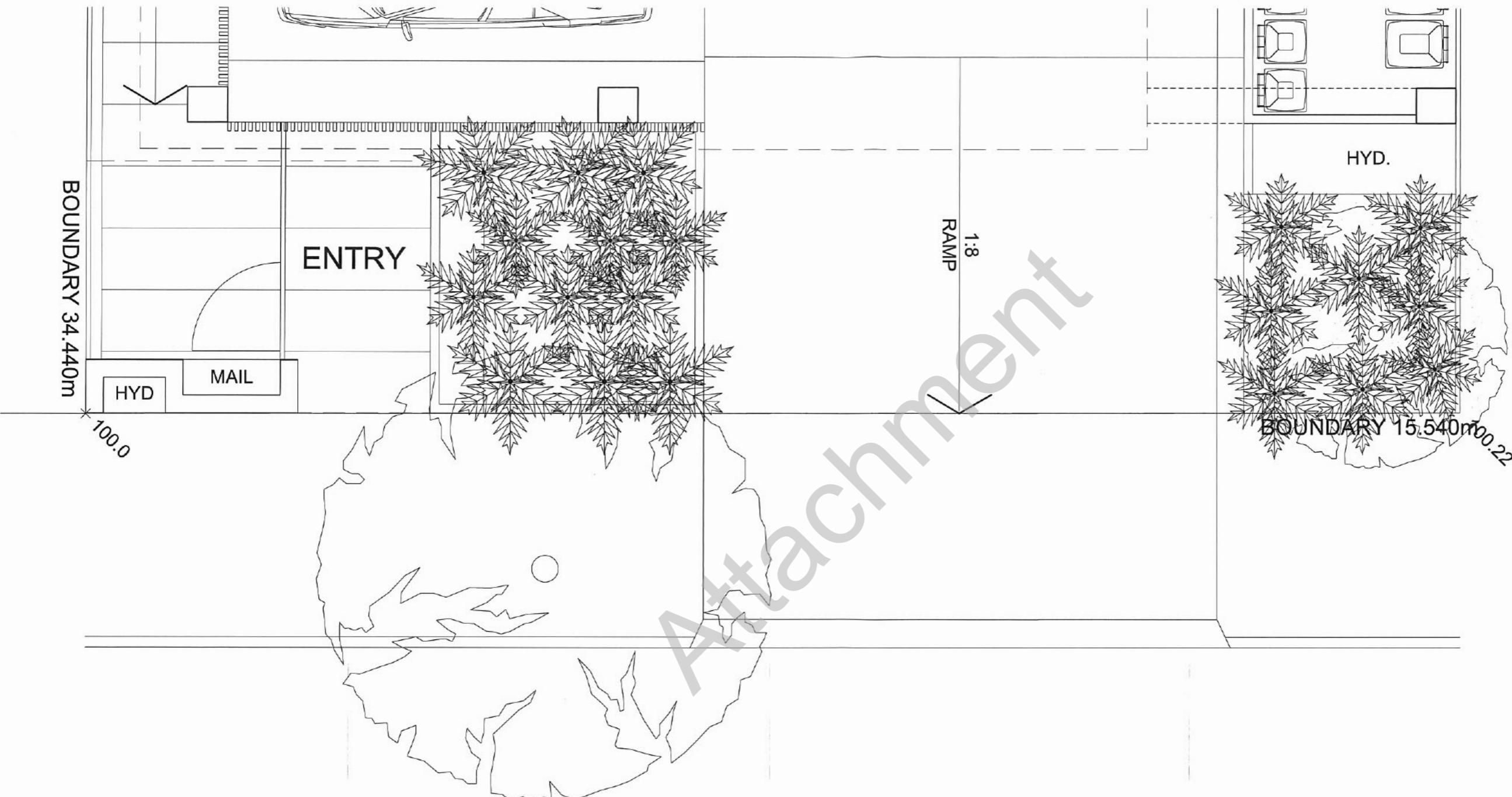
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0 1 2  
DATE 25.05.15 SCALE 1:100 @ A3 PROJECT NO. 15001 DRAWING NO. A2.01 [A2]

1. RENDER + PAINT FINISH
2. METAL CLADDING
3. PANEL CLADDING
4. ALUMINUM FRAMED GLAZING
5. METAL BALUSTRADE
6. GLASS BALUSTRADE
7. LOUVRED SCREEN
8. GLASS BLOCK WINDOWS
9. CONCRETE FRAME
10. LANDSCAPE CANOPY
11. COLORBOND FENCING





A2 PLANNING APPLICATION - REVISED 25.05.15

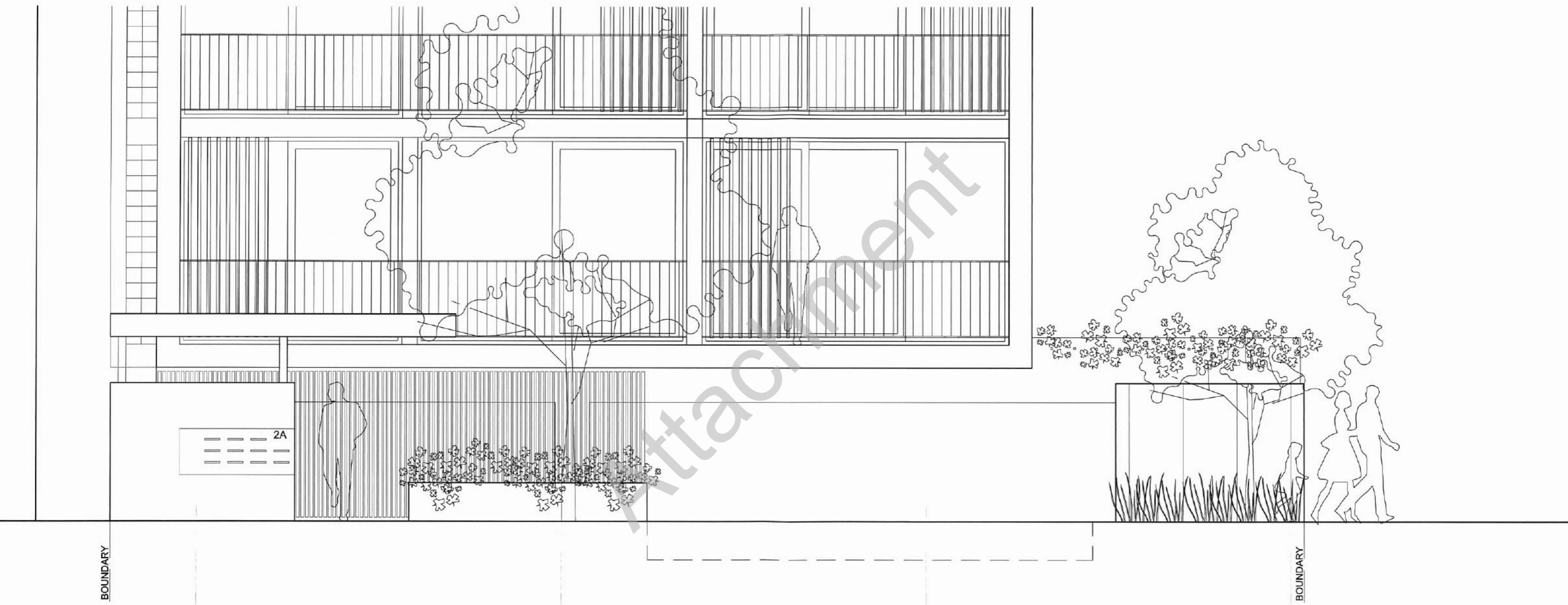
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0 2 4  
DATE 25.05.15 SCALE 50 @ A3  
PROJECT NO. 15001 DRAWING NO. A5.01 [A2]



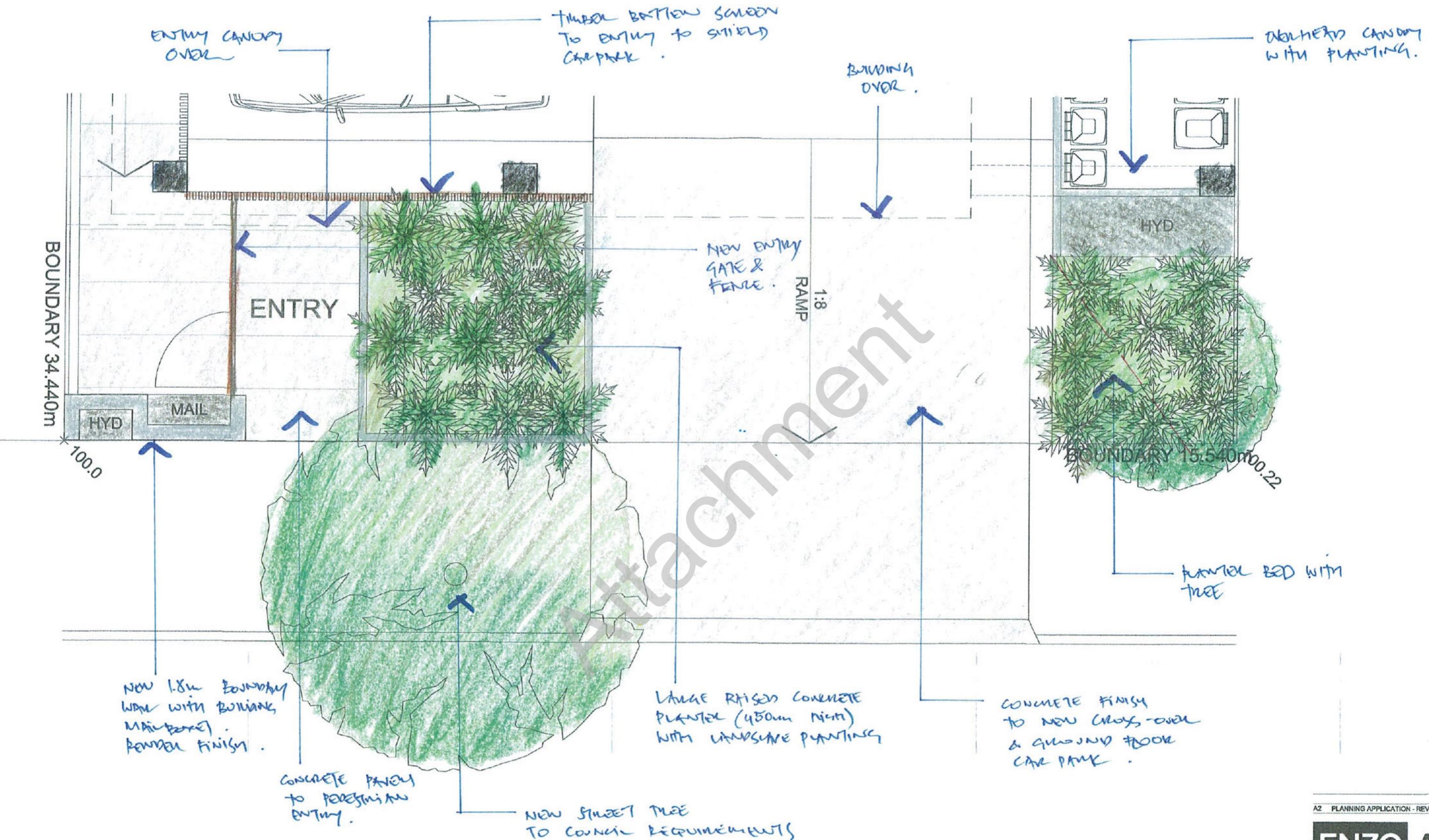
A2 PLANNING APPLICATION - REVISED 25.05.15

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DATE 25.05.15 SCALE 50 @ A3  
10m PROJECT NO. 15001 DRAWING NO. A5.02 [A2]



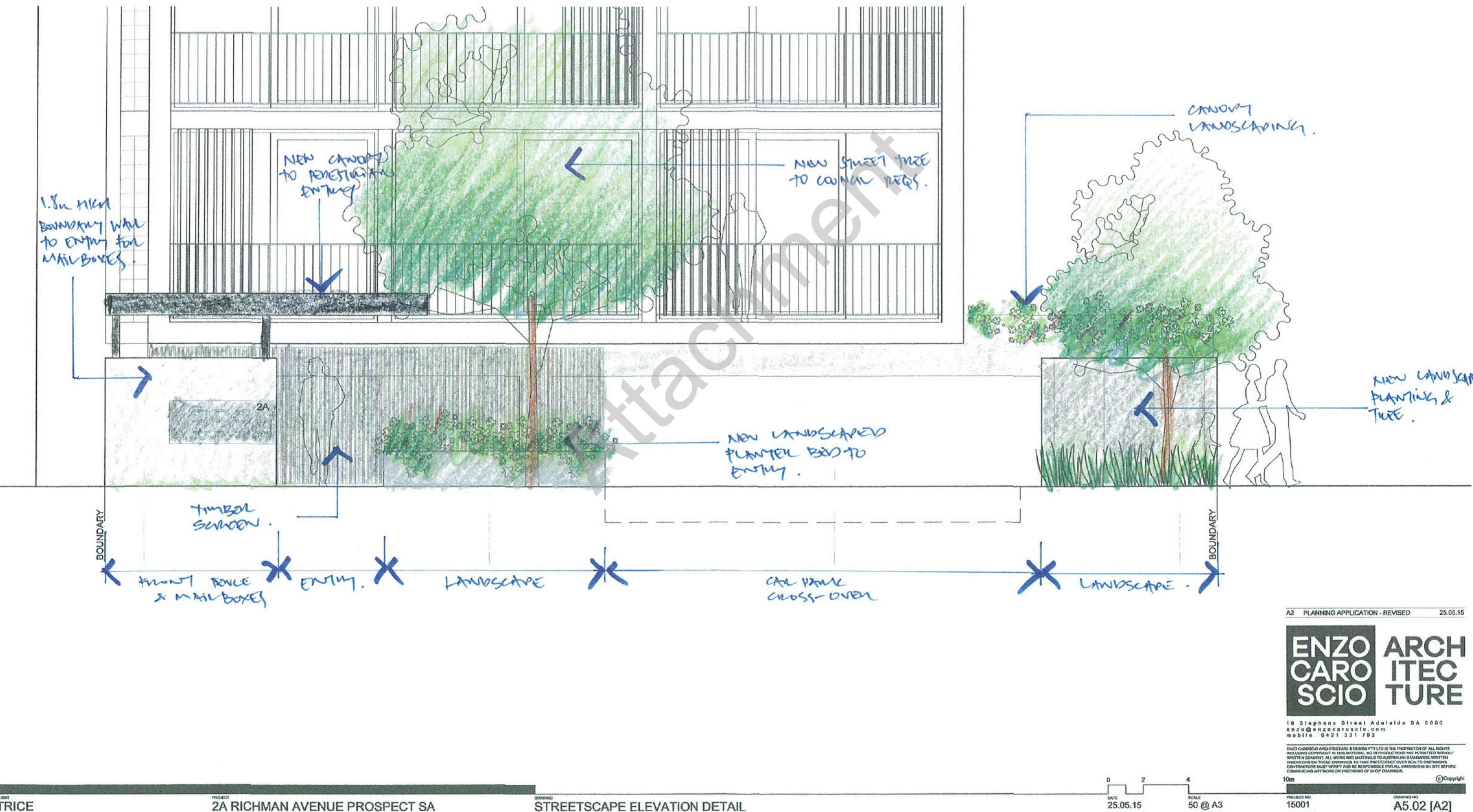
A2 PLANNING APPLICATION - REVISED 25.05.15

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 0 2 4  
 DATE 25.05.15 SCALE 50 @ A3  
 PROJECT NO. 15001 DRAWING NO. A5.01 [A2]



3 Creswell Road  
Largs North SA 5016  
tel 8249 9799 - fax 8249 9744  
enquiry@lcslandscapes.com.au

---

PREPARED BY  
STEVE KINDSTROM  
08 8249 9799 / 0448 075 035  
skindstrom@lcslandscapes.com.au

---

CLIENT  
Trice Project Management

DATE  
29 MAY 2015

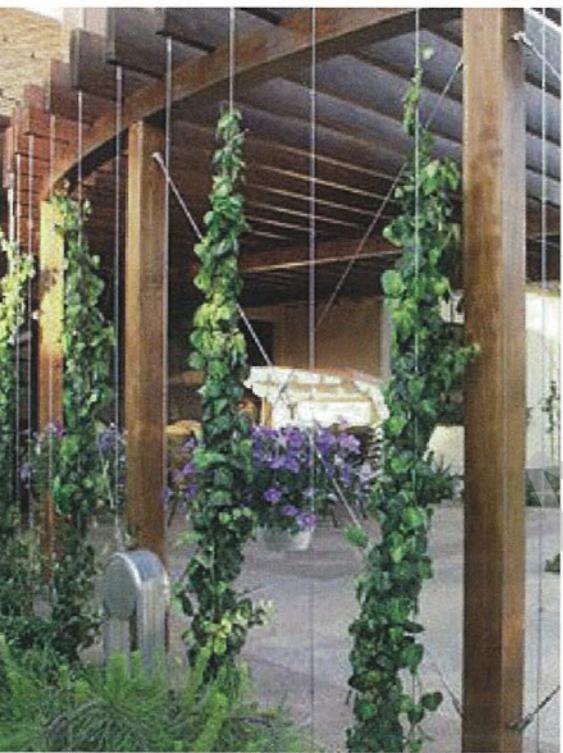
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Civil and Commercial  
Environmental  
Asset Maintenance  
Regional  
Recreational

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Attachment

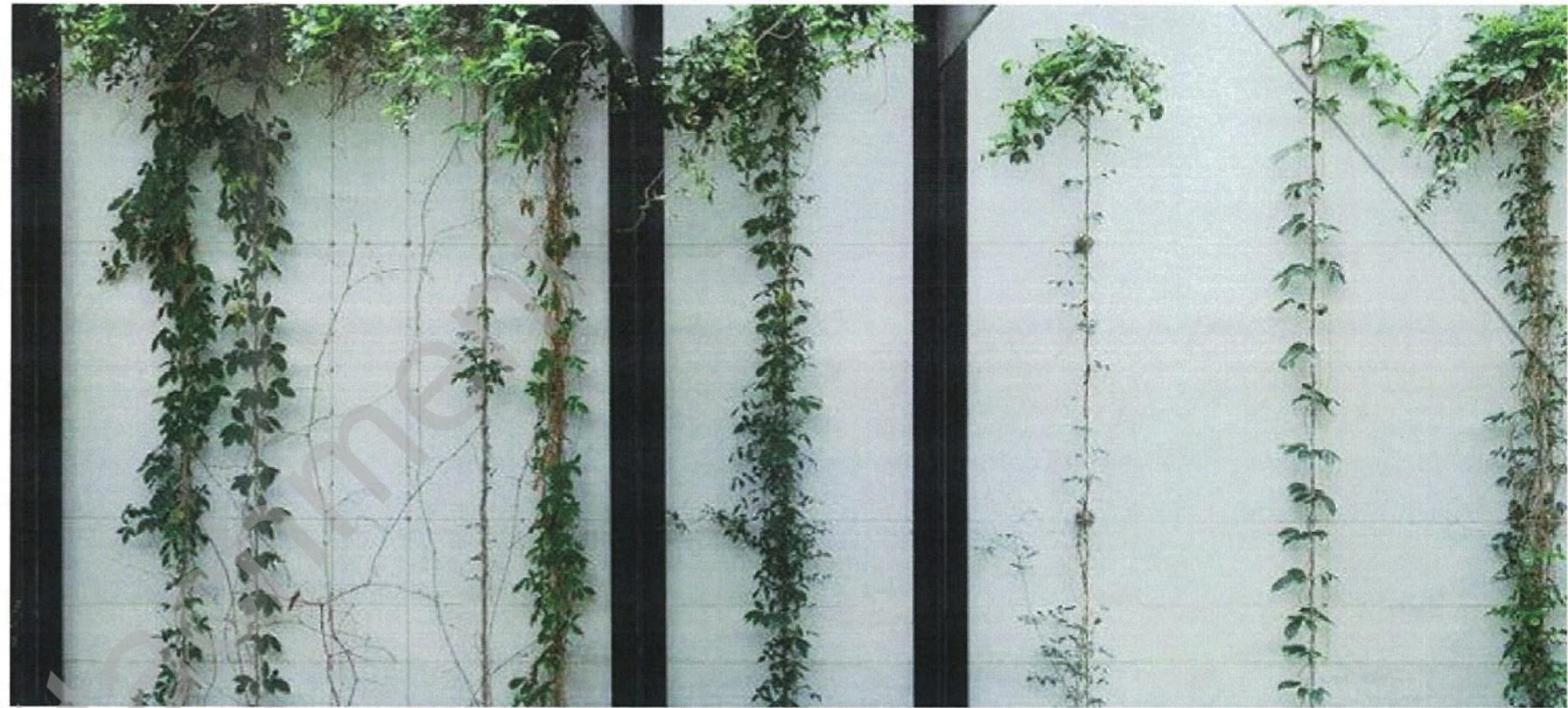




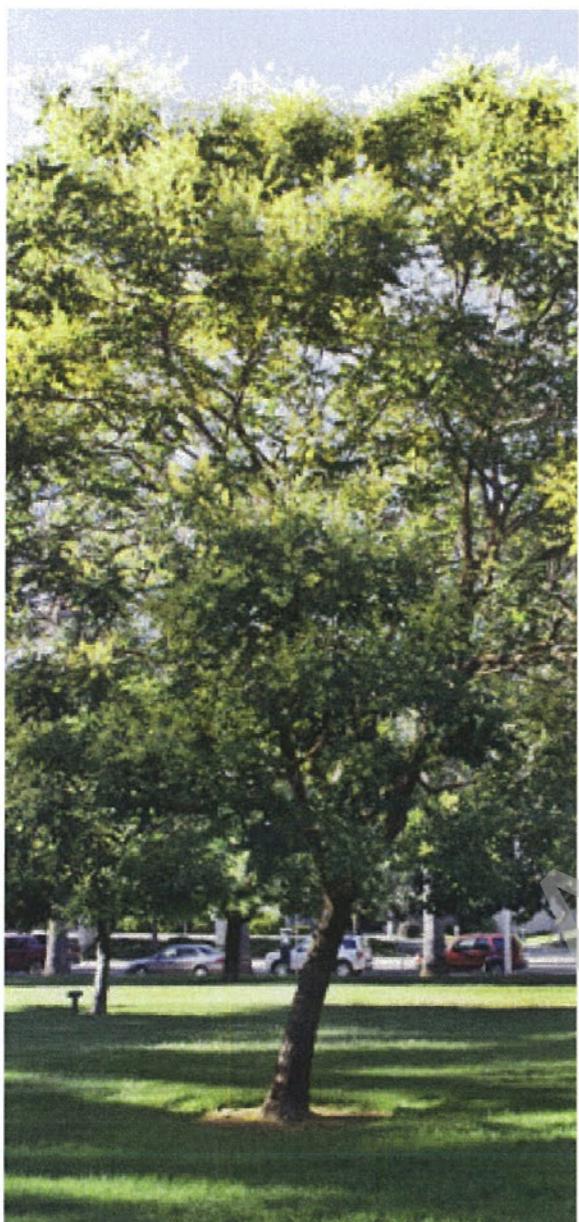
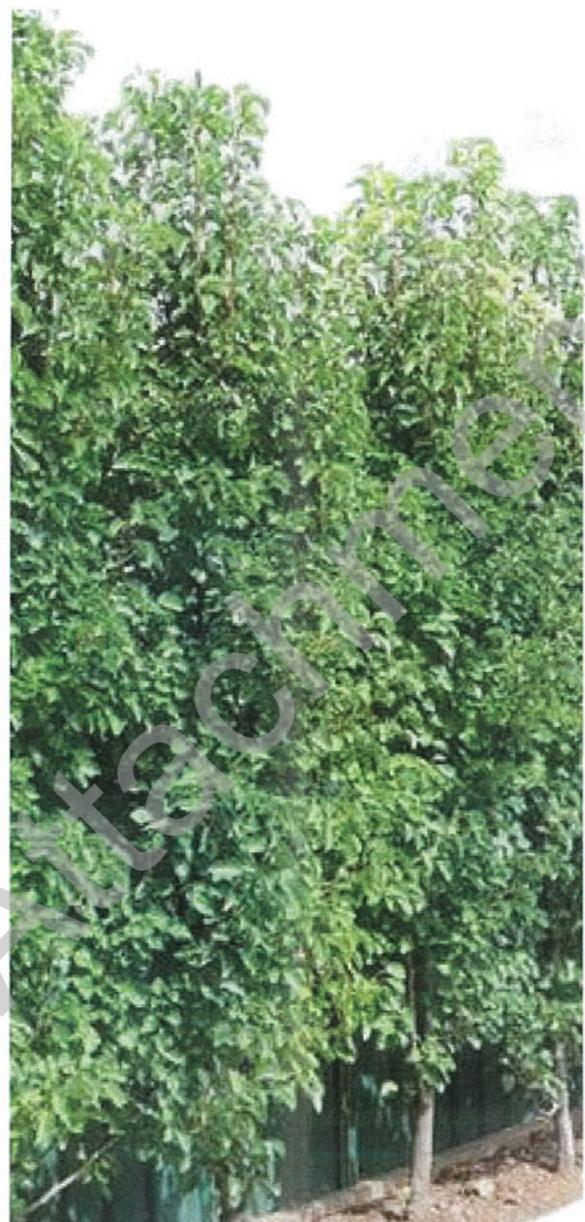
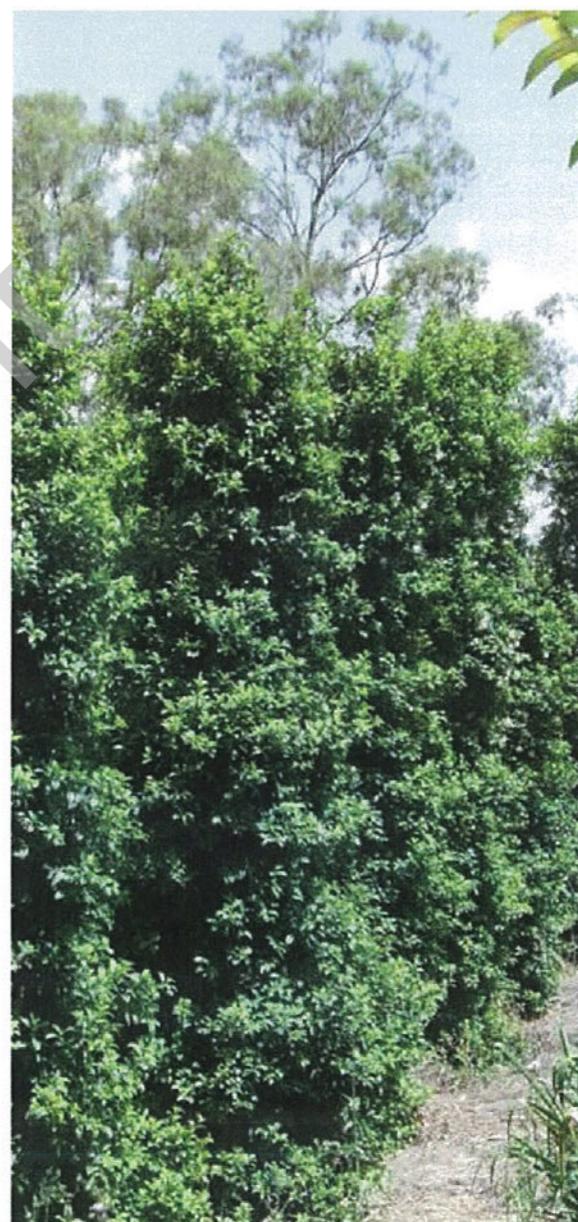
**contemporary  
considered  
structured  
refined**



EXTERIOR GREENING



## 1 PROSPECT APARTMENTS - PRECEDENCE

*Zamia furfuracea**lomandra 'tanika'**Trachelospermum jasminoides**Lagerstroemia 'Natchez'**Koelreuteria paniculata**Pyrus calleryana 'Capital'**Syzygium australe 'Pinnacle'**Vitis coignetiae*

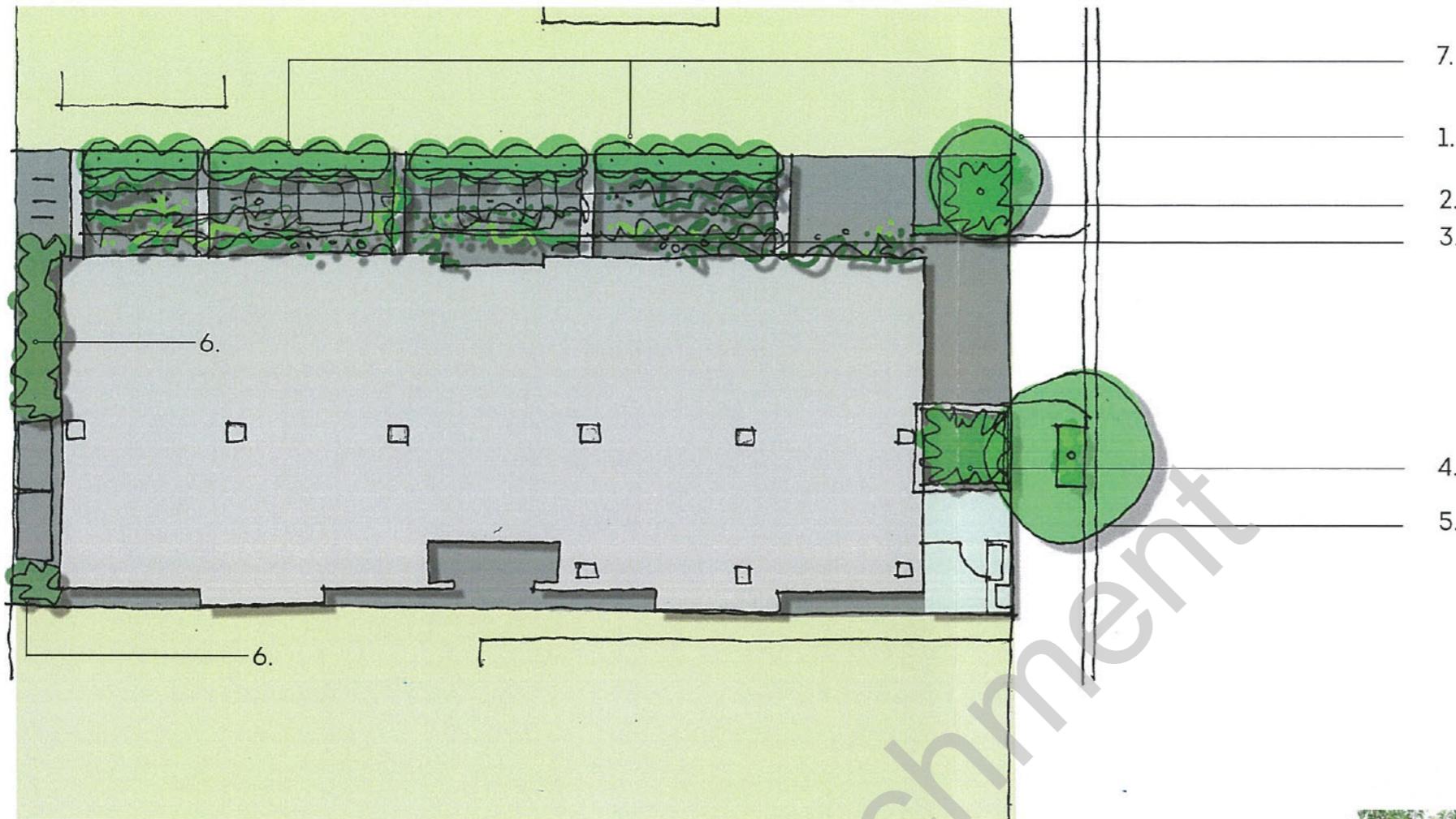
2

## PROSPECT APARTMENTS - PLANTING PALETTE

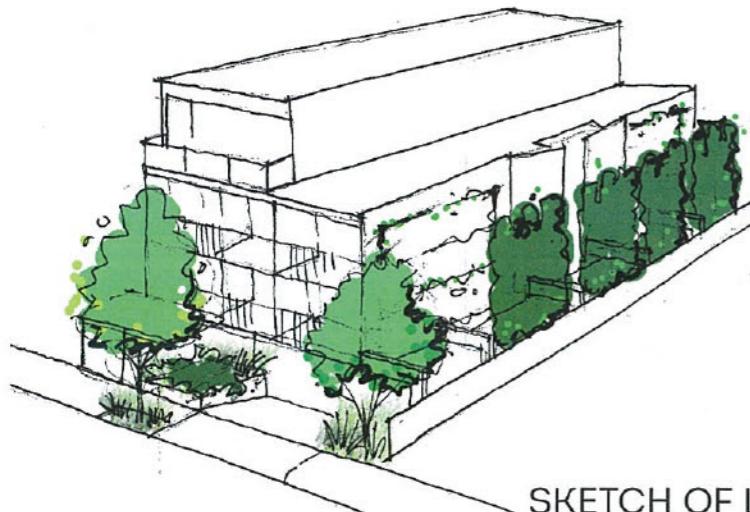
PROJECT: Landscape CONCEPT  
CLIENT: Trice Project Management

DATE: 29/05/2015  
DRAWING NUMBER: LS.011.15

SCALE: NTS  
© LCS LANDSCAPES ACN 102 505 180



APARTMENT GROUND FLOOR PLAN



SKETCH OF LANDSCAPE

## LEGEND

- |                                  |                   |
|----------------------------------|-------------------|
| 1. Lagerstroemia 'Natchez'       | 7m x 6mH          |
| 2. Lomandra 'tanika'             | 80cm x 80cm       |
| 3. Vitis coignetiae              | 10m x 10m approx. |
| 4. Zamia furfuracea '            | 80cm x 80cm       |
| 5. Koelreuteria paniculata       | 7m x 7mH          |
| 6. Murraya paniculata            | 2m x 2mH          |
| 7. Pyrus calleryana 'Capital' or | 3m x 10mH         |
| 8. Syzygium australe 'Pinnacle'  | 1.5m x 8mH        |



7.



1.



2.



3.



4.



8.



5.



6.

## 3

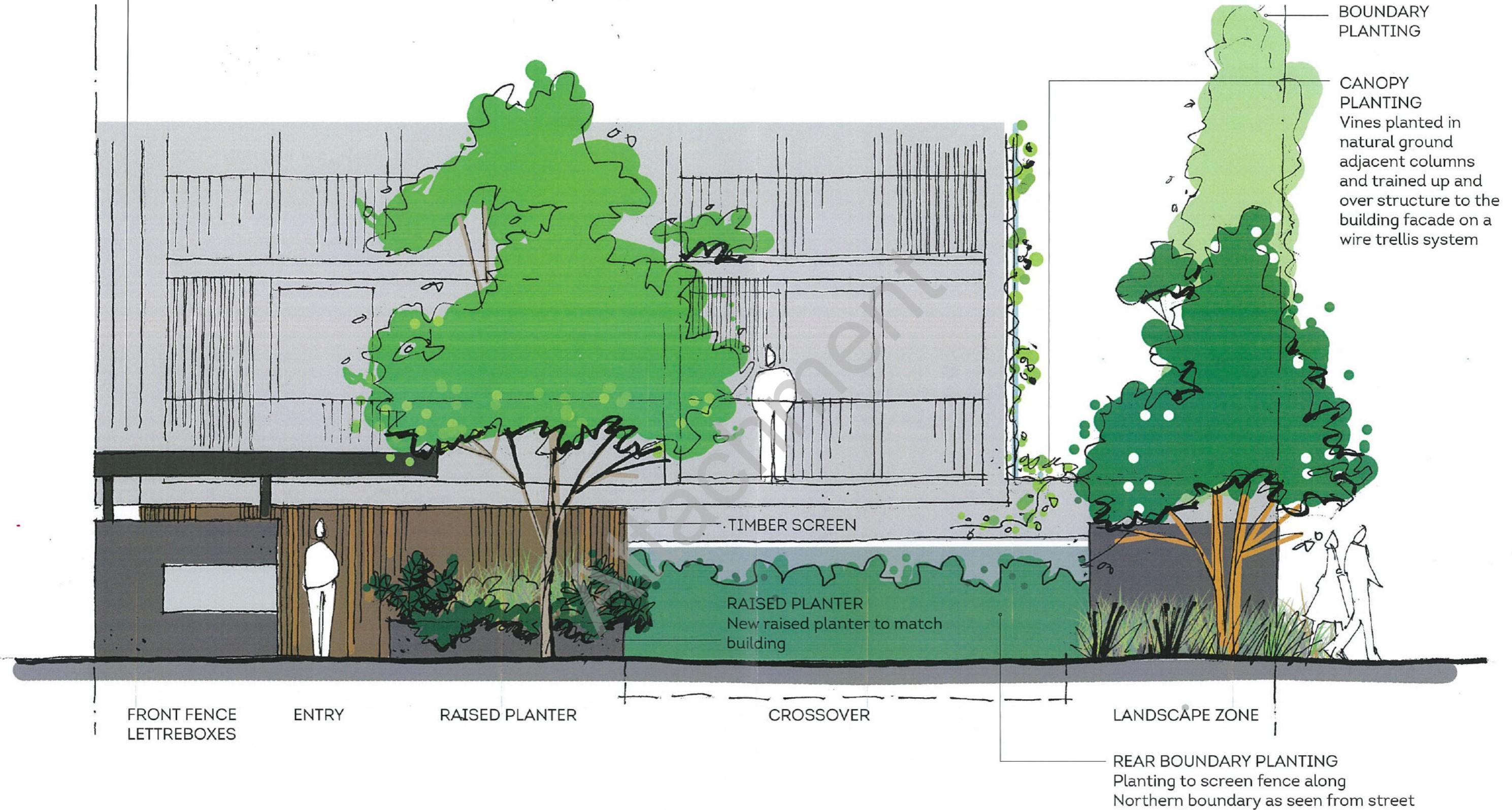
## PROSPECT APARTMENTS - INDICATIVE CONCEPT LAYOUT

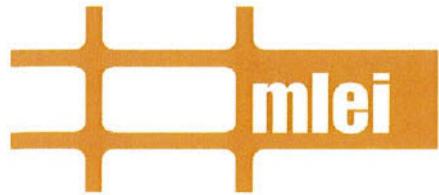
**ENTRY AREA**

Solid feature canopy over entrance area

**STREET TREE *Koelreuteria paniculata***

New advanced street tree located adjacent entry to apartments. New tree to be installed in a constructed tree pit with root director and deep watering provisions.

**4****PROSPECT APARTMENTS - STREETSCAPE ELEVATION**



CONSULTING  
ENGINEERS

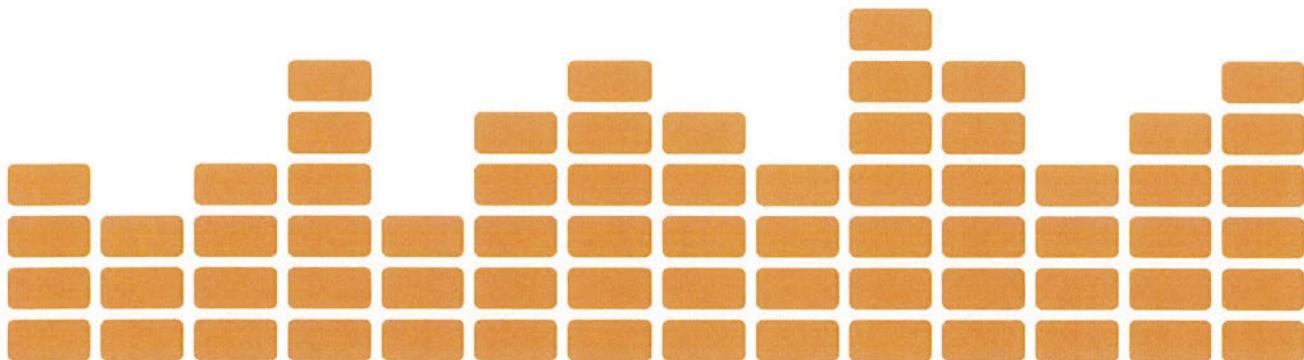
## CIVIL STORMWATER CALCULATIONS

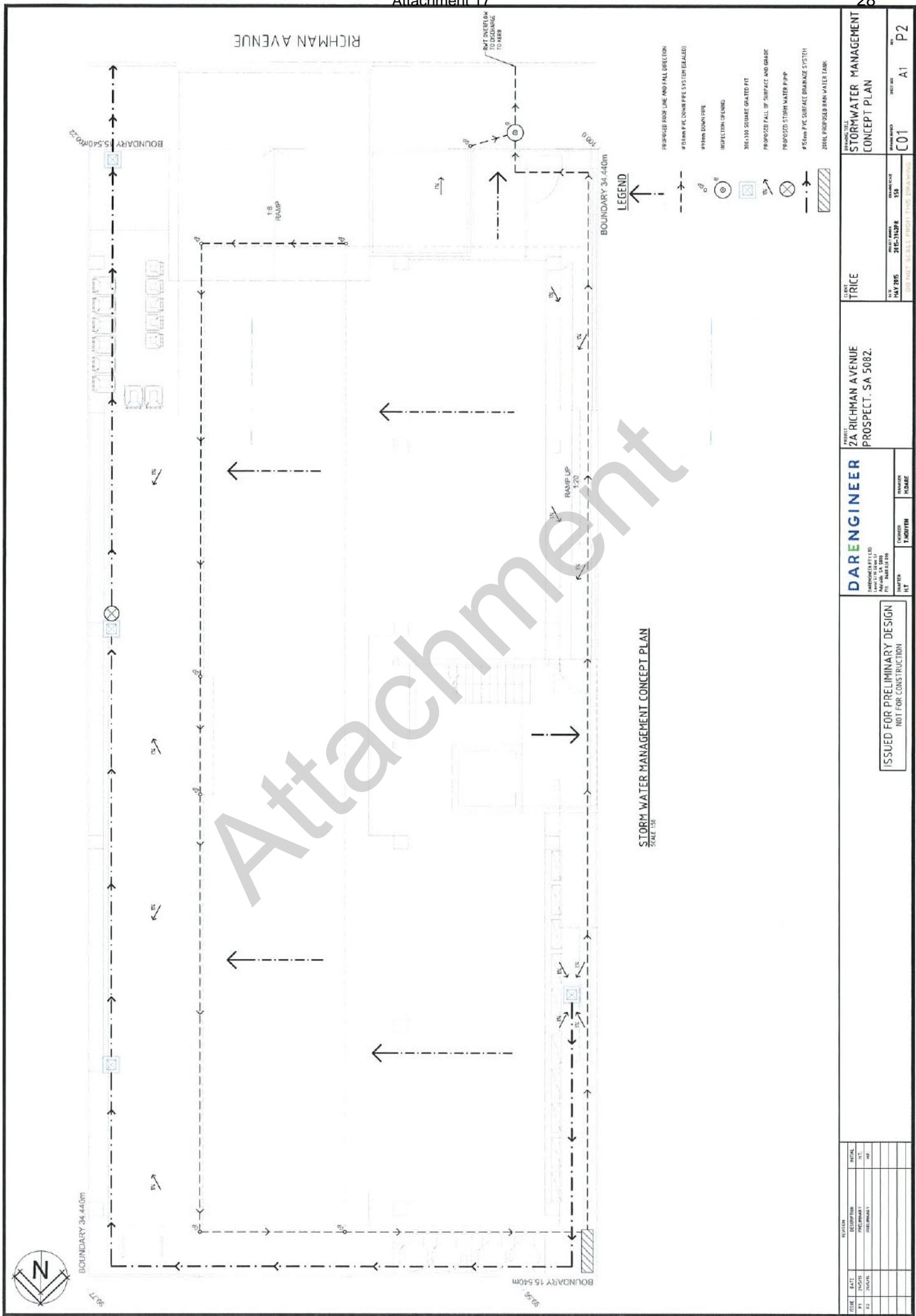
**2A Richman Avenue, Prospect**

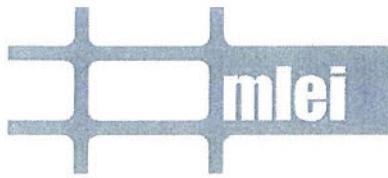
**Project reference: 2015-3942PR**

**ISSUE A**

DATE: 29/05/2015





CONSULTING  
ENGINEERSReference 2015-3942PR

Project 2A Richman Ave, Prospect  
 Designer H.P Checked by AR  
 Date 28/5/2015 Index 1

Pre-development flow:

$$\text{Site Area} = 535.2 \text{ m}^2$$

$$\text{Existing developments} = 200.53 + 57.42 = 257.95 \text{ m}^2$$

$$\text{Existing pervious area} = 535.2 - 257.95 = 277.25 \text{ m}^2$$

$$\text{Using BOM-IFD } I_{10}^{1hr} = 25.8 \text{ mm/hr}, I_{10}^{5min} = 97.7 \text{ mm/hr}$$

$$C_{10}^1 = 0.1 + 0.0133 (I_{10}^{1hr} - 25)$$

$$= 0.1 + 0.0133 (25.8 - 25)$$

$$= 0.11064 \quad \text{--- (1)}$$

$$C_{10}^1 = 0.95 + C_{10}^1 (1-\xi) \quad \text{where } \xi = \frac{A_{\text{development}}}{A_{\text{total}}} = \frac{257.95}{535.2} = 0.482$$

$$= (0.9 \times 0.482) + 0.11064 (1-0.482)$$

$$= 0.491$$

Eq 14.12 ARR

$$Q_{10} = 0.718 \times C_{10}^1 \times I_{10}^{5min} \times A_{\text{site}} \quad \text{Assume site time of concentration, } t_c = 5 \text{ mins}$$

$$= 0.718 \times 0.491 \times 97.7 \times (535.2 \times 10^{-6})$$

$$= 7.14 \text{ L/s.}$$

Eq 5.1 ARR

Post-development flow:

Taking a conservative approach, ignore small garden areas and assume entire post-development site is impervious

$$\therefore \xi = 1$$

$$\text{Total roof area} = ((13.17 \times 11.42) \times 2) + ((4.12 \times 0.62) \times 2) + (9.37 \times 3.6)$$

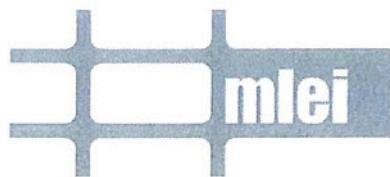
$$= 339.64 \text{ m}^2$$

$$\text{Impervious surface area} = 535.2 - 339.64 = 195.56 \text{ m}^2$$

$$\text{Using (1), } C_{10}^1 = 0.11064$$

$$\therefore C_{10}^1 = (0.9 \times 1) + 0.11064 (1-1) = 0.9$$

Assume time of concentration,  $t_c = 5 \text{ mins}$  for both the roof and surface drainage systems.

CONSULTING  
ENGINEERSReference 2015-3942PRChecked by ARProject 2A Richman Ave, ProspectDesigner H.P Date 28/5/2015 Index 2**Retention Sizing:**

Council requires that the 1:10yr ARI post-development flow be restricted to the 1:10yr ARI pre-development flow.

$$1:10\text{yr ARI pre-development flow} = 7.14 \text{ L/s}$$

Given that the surface drainage system will drain unrestricted to the roadway, this decreases the allowable outflow.

∴ Max detention volume required is 1840L (Refer 'Detention Calculations')

Attachment

## Intensity-Frequency-Duration Table

Location: 34.900S 138.600E Issued: 28/5/2015

Rainfall intensity in mm/h for various durations and Average Recurrence Interval

Average Recurrence Interval

Duration	1 YEAR	2 YEARS	5 YEARS	10 YEARS	20 YEARS	50 YEARS	100 YEARS
5Mins	42.8	57.8	80.8	97.7	121	155	185
6Mins	39.9	53.8	75.0	90.7	112	144	172
10Mins	32.2	43.3	60.0	72.3	88.9	114	136
20Mins	22.8	30.6	42.1	50.4	61.7	78.7	93.3
30Mins	18.2	24.4	33.3	39.8	48.6	61.8	73.1
1Hr	12.1	16.1	21.7	25.8	31.3	39.5	46.6
2Hrs	7.91	10.5	14.0	16.4	19.8	24.8	29.0
3Hrs	6.19	8.16	10.8	12.6	15.1	18.8	21.9
6Hrs	4.06	5.32	6.89	7.98	9.48	11.7	13.5
12Hrs	2.60	3.39	4.34	4.99	5.90	7.19	8.27
24Hrs	1.58	2.05	2.63	3.02	3.57	4.34	5.00
48Hrs	.895	1.16	1.50	1.73	2.06	2.52	2.90
72Hrs	.631	.822	1.06	1.22	1.45	1.78	2.05

(Raw data: 16.68, 3.54, 0.85, 34.99, 6.52, 1.6, skew=0.56, F2=4.47, F50=14.98)

© Australian Government, Bureau of Meteorology

## Site Detention Volume - 10 YEAR

Cro = Runoff Coefficient

### Tank size

Duration	mm/h	In flow L/s	Outflow			Out flow allowed	Qin - Qout	Detentions size for that duration
			Area (m2)	tc (mins)	Cro			
5	97.7	8.295707		4.776553		2.363447	5.93226	1779.678
6	90.7	7.701337		4.434323		2.705677	4.99566	1798.4376
7	86.1	7.310751		4.209429		2.930571	4.38018	1839.6756
8	81.5	6.920165		3.984535		3.155465	3.7647	1807.056
9	76.9	6.529579		3.759641		3.380359	3.14922	1700.5788
10	72.3	6.138993		3.534747		3.605253	2.53374	1520.244
11	70.1	5.952191		3.427189		3.712811	2.23938	1477.9908
12	67.9	5.765389		3.319631		3.820369	1.94502	1400.4144
13	65.7	5.578587		3.212073		3.927927	1.65066	1287.5148
14	63.5	5.391785		3.104515		4.035485	1.3563	1139.292
15	61.4	5.213474		3.001846		4.138154	1.07532	967.788
16	59.2	5.026672		2.894288		4.245712	0.78096	749.7216
17	57	4.83987		2.78673		4.35327	0.4866	496.332
18	54.8	4.653068		2.679172		4.460828	0.19224	207.6192
19	52.6	4.466266		2.571614		4.568386	-0.10212	-116.4168
20	50.4	4.279464		2.464056		4.675944	-0.39648	-475.776
21	49.3	4.186063		2.410277		4.729723	-0.54366	-685.0116
22	48.3	4.101153		2.361387		4.778613	-0.67746	-894.2472
23	47.2	4.007752		2.307608		4.832392	-0.82464	-1138.0032
24	46.2	3.922842		2.258718		4.881282	-0.95844	-1380.1536
25	45.1	3.829441		2.204939		4.935061	-1.10562	-1658.43
26	44	3.73604		2.15116		4.98884	-1.2528	-1954.368
27	43	3.65113		2.10227		5.03773	-1.3866	-2246.292
28	41.9	3.557729		2.048491		5.091509	-1.53378	-2575.7504
29	40.9	3.472819		1.996601		5.140399	-1.66758	-2901.5892
30	39.8	3.379418		1.945822		5.194178	-1.81476	-3266.568
31	39.3	3.336963		1.921377		5.218623	-1.88166	-3499.8876
32	38.9	3.302999		1.901821		5.238179	-1.93518	-3715.5456
33	38.4	3.260544		1.877376		5.262624	-2.00208	-3964.1184
34	37.9	3.218089		1.852931		5.287069	-2.06898	-4220.7192
35	34.5	2.929395		1.686705		5.453295	-2.5239	-5300.19
36	37	3.14167		1.80893		5.33107	-2.1894	-4729.104
37	36.5	3.099215		1.784485		5.355515	-2.2563	-5008.986
38	36.1	3.065251		1.764929		5.375071	-2.30982	-5266.3896
39	35.6	3.022796		1.740484		5.399516	-2.37672	-5561.5248
40	35.1	2.980341		1.716039		5.423961	-2.44362	-5864.688
41	34.7	2.946377		1.696483		5.443517	-2.49714	-6142.9644
42	34.2	2.903922		1.672038		5.467962	-2.56404	-6461.3808
43	33.7	2.861467		1.647593		5.492407	-2.63094	-6787.8252
44	33.3	2.827503		1.628037		5.511963	-2.68446	-7086.9744
45	32.8	2.785048		1.603592		5.536408	-2.75136	-7428.672
46	32.3	2.742593		1.579147		5.560853	-2.81826	-7778.3976
47	31.9	2.708629		1.559591		5.580409	-2.87178	-8098.4196
48	31.4	2.666174		1.535146		5.604854	-2.93868	-8463.3984
49	30.9	2.623719		1.510701		5.629299	-3.00558	-8836.4052
50	30.5	2.589755		1.491145		5.648855	-3.0591	-9177.3
51	30	2.5473		1.4667		5.6733	-3.126	-9565.56
52	29.5	2.504845		1.442255		5.697745	-3.1929	-9961.848
53	29.1	2.470881		1.422699		5.717301	-3.24642	-10323.6156
54	28.6	2.428426		1.398254		5.741746	-3.31332	-10735.1568
55	28.1	2.385971		1.373809		5.766191	-3.38022	-11154.726
56	27.7	2.352007		1.354253		5.785747	-3.43374	-11537.3664
57	27.2	2.309552		1.329808		5.810192	-3.50064	-11972.1888
58	26.7	2.267097		1.305363		5.834637	-3.56754	-12415.0392
59	26.3	2.233133		1.285807		5.854193	-3.62106	-12818.5524
60	25.8	2.190678		1.261362		5.878638	-3.68796	-13276.656

1840

**AGENDA ITEM:** 5.2

**To:** Development Assessment Panel (DAP) on 15 June 2015

**From:** Scott McLuskey, Senior Development Officer Planning

**Proposal:** Four Storey Residential Flat Building comprising 24 Dwellings with Associated Car and Bicycle Parking and Landscaping (DA 050/345/2014)

**Address:** 227-229 Prospect Road Prospect (CT 5240/49)

---

**SUMMARY:**

Applicant: Vartzokas Architects Pty Ltd

Planning Authority: Council

Referrals (Schedule 8): Department for Planning, Transport and Infrastructure

Public Notification: Category 1

Representations: Not applicable

Respondent: Not applicable

Development Plan Version: Consolidated 31 October 2013

Zone and Policy Area: Urban Corridor Zone (Transit Living Policy Area)

Issues: Car Parking, Private Open Space, Setbacks

**Recommendation:** **Approval, subject to conditions**

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**1. EXECUTIVE SUMMARY**

- 1.1 A four storey apartment building, comprising 24 two bedroom dwellings (three of which would be disabled compliant), 29 parking spaces and 16 bicycle spaces, is proposed at the key gateway intersection of Prospect and Regency Roads (227-229 Prospect Road Prospect).
- 1.2 The proposal is a Category 1 development and therefore was not publicly notified. It was referred to the Department of Planning, Transport and Infrastructure, who initially required changes to the proposal, but subsequently indicated their support subject to conditions. The design review concluded that the presentation of the building sets a good example for subsequent development but would benefit from further consideration with relation to energy efficiency.
- 1.3 The proposal provides suitable occupant amenity and private open space, provides a high level of visual interest which is critical for this high profile site and responds well to any concerns relating to its scale. Car and bicycle parking would be appropriately catered for on-site, while overlooking and overshadowing impacts would be addressed suitably in the context of the zone. The efficiency of mechanical heating and cooling

services would be reserved for further consideration. The proposal therefore warrants development plan consent.

## **2. BACKGROUND**

- 2.1 This report and proposal was on the agenda for the meeting of the Development Assessment Panel (DAP) at its meeting of 11 May 2015. The application was deferred for further consideration at a subsequent meeting of the DAP, pending receipt of clearer and more detailed proposal plans. These included further details relating to waste and stormwater management. The relevant excerpt from the minutes is as follows:

*That development application 050/345/2014 be deferred to a subsequent meeting of the Development Assessment Panel to enable the applicant to provide further information on the following matters and consider amending the application as necessary to resolve the following:*

1. *Clearer proposal plans and a 3D perspective of the proposed building*
2. *Details of the ground level plinth*
3. *Landscaping details, particularly within the area identified as required for road-widening*
4. *Details of the waste management methodology, including reconsideration of the location of the waste storage area and access to that area*
5. *Details of the method of stormwater management, including capture, re-use and disposal*

- 2.2 The clarity of the proposal plans was of principal concern to the DAP. It is noted that the amended proposal is largely similar to the previous proposal but provides the additional clarity and detail necessary to suitably assess the proposal. The report and its attachments therefore have been updated to incorporate discussion on the applicant's responses to the matters above.
- 2.3 A summary of the plans and correspondence comprising the applicant's response are provided at Section 4.2. Additional commentary in relation to net site density is also provided at Section 8.2 for the benefit of the DAP.

## **3. LOCALITY AND SUBJECT LAND**

### **3.1 Locality**

- 3.1.1 The locality comprises a mix of residential and commercial land uses incorporating dwellings, warehousing, offices, shops and a reserve to the north-west of the subject land. It is noted that the Prospect Road locality comprises mainly commercial uses, while Regency Road features a balanced mix of commercial and residential properties.
- 3.1.2 Residential development within the nearby area features a mix of original, low density dwellings and newer medium density dwellings of two storey construction. The site is located at the intersection of two substantial arterial roads, resulting in considerable noise intrusion from vehicles.
- 3.1.3 The broader locality, indicating the location of the subject land within the relevant Zone and Policy Area as described in Council's Development Plan is described in **Attachment 1**. Photographs of nearby properties are included at **Attachments 2-4**. It is noted that the southern adjoining property has been cleared in anticipation of future development, with development plan consent having been granted for 5 two storey dwellings.

### **3.2 Subject Land**

- 3.2.1 The subject land is located on the south-eastern corner of the intersection of Prospect Road and Regency Road. The land comprises one allotment with a total area of 1270m<sup>2</sup>, with a frontage of approximately 36m to Prospect Road and 29m to Regency Road. It is noteworthy that the usable area of the land is reduced by 280m<sup>2</sup> as a result of road widening requirements imposed by the Department of Planning, Transport and Infrastructure. The land slopes substantially from east to west to a difference of approximately 2 metres in height.
- 3.2.2 Existing site improvements include an office building to the south-eastern corner of the land, a freestanding advertising display and ancillary fencing and lighting to the Prospect and Regency Road frontages of the site. Existing vegetation is limited with much of the site bitumen sealed. No significant trees are on the subject land or within close proximity on adjoining allotments. The subject land is illustrated on [Attachment 5](#). Photographs of the subject land are also included for the DAP's reference (refer [Attachment 6](#)).

### **4. PROPOSAL**

- 4.1 The proposal comprises the construction of a four storey residential flat building incorporating 24 two-bedroom dwellings, three of which would be disabled compliant. 29 car parking spaces and 16 bicycle (including two scooter) parking spaces would be provided at ground level, along with a refuse area, entry foyer, fire exit and landscaping areas forward of the building.
- 4.2 A written submission from the applicant addressing the matters described in paragraph 2.1 are attached (refer [Attachments 7-8](#)). The proposal plans, including a 3-dimensional perspective, are attached (refer [Attachments 9-16](#)). It is noted that the plans have been reconfigured by staff to improve their clarity at A3 and A4 page sizes. Stormwater management plans prepared by Dr Anthony S. Hasham of Australian Consulting Engineers are also attached (refer [Attachments 30-33](#)).

### **5. REFERRALS**

#### **5.1 Internal (Advisory) Referrals**

- 5.1.1 An emphasis on high quality building and landscape design, with consideration of urban design principles is a fundamental component of any new development within the Urban Corridor Zone. Accordingly, the proposal was referred to Mr Julian Rutt of Lumen Studio for informal design review in accordance with Council's Design Review Process for Higher Density Development.
- 5.1.2 Briefly, the review (refer [Attachments 17-19](#)) identified the following:
- The design language is coherent, modern and employs appropriate materials, setting a good example for development in the area. Facades feature significant visual interest and represent a sound response to a prominent corner site.
  - While the building makes reasonable use of ideal solar orientation, several internal bedrooms will rely solely on borrowed light from internal spaces. Further, limited opportunities are available for cross-ventilation. Given this, Council may wish to seek further information on mechanical systems and solar energy capture.

- The limited setback of the building results in a limited scope for notable landscape plantings. While passive surveillance is aided by the corner pedestrian entry point and balcony orientations, the angled driveway accesses may encourage higher speed, and hence less safe, vehicle/pedestrian interactions on footpaths.

## 5.2 External (Legislated) Referrals

- 5.2.1 The proposal was referred to the Department for Planning, Transport and Infrastructure as required by Schedule 8 of the Development Regulations 2008. Pursuant to this Schedule, DPTI has the ability to direct Council in relation to some elements and the ability to make comment in regard to other elements. Their responses are attached ([Attachments 20-24 and 25-26](#)), as are the original plans considered by the DPTI ([Attachments 27-29](#)).
- 5.2.2 As the proposal is within a prescribed distance of an area possibly required for road widening purposes, DPTI has the power of direction in relation to building works within the prescribed distance. To this end, the DPTI requested in its original response that the building be setback 4.5m from both the Regency and Prospect Road boundaries of the site. The DPTI subsequently indicated satisfaction with the amended plans.
- 5.2.3 Council must also have regard to the comments of DPTI in relation to the proposed access and egress points to Regency and Prospect Roads. The DPTI indicated that it wished to see one-way traffic flow through the site, with ingress from Regency Road and egress to Prospect Road. Further, the DPTI wished to see both access points flared as close to a 70° angle as possible, noting that this would result in impacts to car parking provision on site. This outcome has been achieved through amendments to the proposal.
- 5.2.4 The DPTI have indicated that they are not opposed to the proposal in its amended form. The DPTI have directed that Council impose certain conditions if approval is granted and have recommended the imposition of others.
- 5.2.5 No other consultation with agencies was required.

## 6. **PUBLIC NOTIFICATION**

- 6.1 The application is a Category 1 form of development pursuant to Section 38 of the *Development Act 1993*, Schedule 9 of the *Development Regulations 2008* and Urban Corridor Zone Principle of Development Control 22.
- 6.2 A residential flat building is a Category 1 development unless it is located on land adjacent to the Residential Zone or Historic (Conservation) Zone, it would be three or more storeys (or 11.5 metres or more in height) and it would exceed the 'Building Envelope - Interface Height Provisions' (UCZ PDC 22).
- 6.3 The subject land is not located adjacent either the Residential Zone or the Historic (Conservation) Zone (refer [Attachment 1](#)), nor would the building exceed the interface height provisions, so is a Category 1 form of development for which no public notification is undertaken.

## **7. PLANNING COMMENTARY**

- 7.1 The application involves building work and therefore an application to Council is required. The proposal is neither a complying nor a non-complying development with reference to Principles of Development Control 20 and 21 of the Urban Corridor Zone and is therefore to be considered on its merits against the relevant provisions of Council's Development Plan.
- 7.2 Pursuant to Section 35(2) of the *Development Act 1993*, a development that is assessed by the Council as being seriously at variance with the Development Plan must not be granted consent. To this end, the Panel must determine whether the proposal is seriously at variance with the Development Plan prior to making a decision on the application.

## **8. PLANNING ASSESSMENT**

### **8.1 Land Use**

- 8.1.1 It is anticipated that development within the Urban Corridor Zone would enable a high quality mixed use urban environment that contributes to the economic vitality of the City of Prospect by increasing the density of housing, as well as the number and the diversity of businesses and other services offered to residents and the wider community (UCZ Desired Character Statement).
- 8.1.2 The above is reiterated by the following Objectives of the Urban Corridor Zone:
  - Objective 1:** *A mixed use zone accommodating a range of compatible non-residential and medium and high density residential land uses orientated towards a high frequency public transport corridor.*
  - Objective 2:** *Integrated, mixed use, medium and high rise buildings with ground floor uses that create active and vibrant streets with residential and commercial development above.*
  - Objective 3:** *A mix of land uses that enable people to work, shop and access a range of services close to home.*
- 8.1.3 Furthermore PDC 1 of the zone outlines the types of development, or a combination thereof, which are envisaged within the Zone. A residential flat building is one of the types of development listed, therefore the proposal is considered to be an appropriate type of development and is supported from a land use perspective.

### **8.2 Site Density**

- 8.2.1 The Transit Living Policy Area anticipates medium (45-70 dw/ha) to high (70-200dw/ha) density housing developments, to result in the area having an overall medium residential net density (45-70 dw/ha). This would primarily be in the form of apartment and terrace style dwellings along with mixed-use buildings to accommodate a diversity of dwelling types within the precinct (UCZ TLPA DCS).
- 8.2.2 In order to achieve this, the minimum residential site density for residential development within the Transit Living Policy Area is 45 dwellings per hectare net (UCZ PDC 5). Within the Prospect Estate Reserve Area, shown in Concept Plan Fig UrC/4, building heights and residential densities will be greater than elsewhere within the policy area to activate the reserve (UCZ TLPA DCS).

- 8.2.3 While the subject site is 1,270m<sup>2</sup> in total area, its usable area (as a result of possible future road widening) is 980m<sup>2</sup>. Thus the minimum net residential site density described by UCZ PDC 5 would be achieved through the provision of 5-6 dwellings. It is however desirable, given the site's location within Concept Plan UrC/4, that the site feature a density of 200 dw/ha or greater, with reference to those densities allowable elsewhere within the policy area.
- 8.2.4 The proposal is for 24 dwellings within the residential flat building, at a present net density of 189 dw/ha and an anticipated future net density of 245 dw/ha following road widening. The building would therefore feature a greater than generally desirable net residential site density, in accordance with the desired character statement. The dwelling density is therefore supported.

### **8.3 Site contamination and remediation**

- 8.3.1 The proposed development seeks to establish a more sensitive land use upon the site than that which has historically existed. It is noted that the previous use of the subject land is unlikely to have resulted in contamination of the land, particularly given that the surface of the land is almost entirely sealed and this has been the case for well over 20 years.
- 8.3.2 Generally, development should not proceed until site contamination has been adequately assessed and managed to ensure that the site is fit for the proposed use (Council Wide PDC 129). Based on the design/layout of the proposed development, including the extent of excavation and sealed surfaces at ground level of the building, it is expected that any unanticipated site contamination will be intrinsically managed in an appropriate manner through construction practices.
- 8.3.3 In this instance, it is not considered necessary that further investigation into site contamination be undertaken, nor that additional remedial measures be undertaken.

### **8.4 Traffic and Vehicular Movements**

- 8.4.1 It is anticipated that new developments will minimise the number of access points onto arterial roads, by providing vehicle access from side streets, rear access ways, via rights of way or common vehicle parking areas (UCZ PDC 11). Where this is not possible, development along arterial roads should rationalise the number of access points off arterial roads and provide sufficient on-site manoeuvring areas to enable all vehicles to enter and exit the site in a forward direction (Council Wide PDC 229).
- 8.4.2 The number, location and design of access points on public roads should be such as to minimise traffic hazards, queuing on roads, interference with the function of intersections and traffic control devices and intrusion of through traffic into adjacent residential streets (Council Wide PDC 210).
- 8.4.3 Access to the site would be entirely in accordance with the approach requested by the DPTI. This would entail a single width entry point to Regency Road and a single width egress point to Prospect Road, both of which would be flared at a 70° angle to the kerb line.
- 8.4.4 Sufficient areas would be provided on the site to ensure that all vehicles are capable of entering and exiting the site in a forward motion. The DPTI have recommended conditions that would ensure that the location of the access roller doors provide safe access and egress in accordance with the relevant Australian

Standard. It is anticipated that compliance with such conditions could be achieved.

8.4.5 With reference to the advice of the DPTI, it is concluded that the proposed access arrangements provide for safe and convenient vehicle movement, and satisfactorily respond to the relevant provisions of the Development Plan.

## **8.5 Design and Appearance**

- 8.5.1 It is anticipated that development within the Urban Corridor Zone would achieve a high standard of architectural design through careful building articulation and fenestration to all visible sides. Building facades should involve the careful use of a diversity of building materials to create a high quality building appearance.
- 8.5.2 Building heights and residential densities would increase within the area shown by Concept Plan Fig URC/4, within which buildings should be a minimum of three storeys and a maximum of four storeys. Further, buildings should abut Prospect and Regency Roads to emphasise the importance of this precinct. Balconies overlooking the streets are encouraged to provide passive surveillance (UCZ TLPA DCS).
- 8.5.3 The proposed building would be four storeys tall and achieve a maximum height of 14.3m above natural ground level. It would incorporate vertical and horizontal articulated features that utilise physical recession, colour and material contrasts to provide a modern design with an appropriate level of visual interest. While the building would generally abut the anticipated future footpath at ground level, landscaping and building articulation around the corner entry area represent an appropriate design outcome for the intervening period.
- 8.5.4 Northern and western facades would feature a substantial number of window openings and sizeable balconies, incorporating cohesive material variations to walls and balcony balustrades. The eastern façade is not highly articulated, but would utilise varied materials and finishes in concert with indentation to provide visual relief to its mass. The southern façade of the building would feature substantial balconies, high levels of fenestration and building articulation that follows the slope of the land.
- 8.5.5 While not entirely in accordance with the desired minimum setbacks (as set out in 6.6), the building has been designed to respond appropriately to the desirable outcomes relating to building massing, natural ventilation and sunlight penetration on what is a rather constrained site.
- 8.5.6 The external building materials and finishes have been designed to ensure an appropriate interface between the public and private realms. It is also considered that the architectural features of the proposed building, in combination with the diversity of materials and finishes would provide an appropriate level of visual interest and a high quality built form. The external form and presentation of the building is therefore supported. To ensure a high quality built outcome for the proposal, conditions reinforcing key design elements are recommended.

## **8.6 Setbacks**

- 8.6.1 Within the Transit Living Policy Area, the minimum setback from the primary road is 3m unless varied by the Concept Plans within Council's Development Plan. Within the area described by Concept Plan Fig URC/4, it is desirable that buildings abut their Prospect and Regency Road boundaries.

- 8.6.2 For allotments with a frontage width of 20 metres or less, there is no minimum setback for the first 2 levels of a building from a side boundary when adjoining another allotment, while a minimum 2m setback is required for all levels above this height (UCZ PDC 16 and 18).
- 8.6.3 While the building would generally abut the anticipated future footpath at ground level, minor setbacks would be provided for landscaping and building articulation around the corner entry area. Given the advice of the DPTI, setbacks have been considered against the future boundary of the site following the excise of land for road widening purposes as described on the proposal plans.
- 8.6.4 At all floors above ground level the building would be setback 2.5m from the northern boundary of the site. The eastern wall of the building would be located on the boundary or setback 1.5m, while the western wall would vary between being located on the boundary or setback up to 3.8m.
- 8.6.5 The southern boundary would vary in setback distance from 2m to the east of the site, to 4m towards the west of the site. Balconies would protrude forward of these setbacks to the northern and southern facades of the building, and would abut the western boundary of the site.
- 8.6.6 Intrusions within the 2m desirable setback distance to the southern boundary of the site would be limited to balconies and their associated balustrades. These balconies would follow the line of the wall, progressively stepping further from the southern boundary of the site as the height of the building increases above natural ground level. The entire eastern façade of the building would be within the 3m desirable setback distance to the eastern boundary of the site.
- 8.6.7 It is noted that the adjoining southern and eastern properties are within the Transit Living Policy Area of the Urban Corridor Zone. Both are therefore suitable for medium-high density development wherein the first two floors of buildings on both sites may be constructed to the boundary of the subject land. It is also noted that the adjoining land to the east has a notably higher ground level than the subject land. A future application on this site may very well entail a two storey wall that is 75% of the total wall height of the presently proposed building. It is noted that this adjoining property features extensive garaging to this boundary, though also features solar panels on the existing dwelling that are likely to be impacted in winter afternoons and late summer afternoons (approximately 5pm onwards).
- 8.6.8 While the proposal would not meet all relevant desirable setbacks, design features have been used to effectively break down the mass of these building facades, and the nature of development desirable on adjacent allotments is commensurate with the scale of the present proposal. While not entirely desirable, these departures of themselves would not warrant refusal of the application. The setbacks from Regency and Prospect Roads are appropriate with regard to the current location of the roadway and the likely future location following road-widening. The siting of the proposed building on the land is therefore supported.

## **8.7 Energy Conservation Measures**

- 8.7.1 It is desired that all dwellings provide adequate thermal comfort for occupants through passive design features such as orientation of windows, living areas and private open space, and cross-ventilation (Council-wide PDC 79).

- 8.7.2 It is anticipated that heating and cooling would be via individual gas hot water systems and air-conditioning units, which would be screened and located on the roof of the building. As identified in the design review, while some dwellings would be well-orientated, others will be greatly reliant upon mechanical heating and cooling.
- 8.7.3 On balance, the majority of dwellings would achieve adequate orientation and amenity. It is unlikely that a design solution could achieve all desirable passive thermal design characteristics.

## **8.8 Noise Attenuation**

- 8.8.1 It is anticipated that noise and air quality impacts should be mitigated through appropriate building design and orientation (UCZ Objective 1). Residential development on sites abutting roads with traffic volumes exceeding 3000 vehicles per day should be sited and designed to reduce the impact of traffic noise on occupants. Further, residential buildings should feature adequate separation between the habitable room windows and balconies of other buildings (Council Wide PDCS 111 and 161).
- 8.8.2 In addition to the above, the subject land is identified with Map Pr/1 (Overlay 5) for the purpose of noise and air emissions. It is outlined by PDC 1 of the Noise and Air Emissions Overlay that sensitive development located adjacent to high noise and/or air pollution sources should be additionally protected from these additional potential impacts.
- 8.8.3 It is also desirable that attached dwellings are designed to minimise the transmission of sound between dwellings, particularly between living areas and bedrooms (Council-wide PDC 93). To this end, it is noted that the layout of each dwelling does not naturally separate bedrooms from abutting living areas of adjoining dwellings.
- 8.8.4 The construction of the building would be undertaken in accordance with the Ministers Specification SA78B – Construction requirements for the control of external sound. Compliance with the Minister's Specification would be required as part of the Building Code of Australia (BCA). This notwithstanding, the layout of dwellings necessitates further assessment of internal walls to ensure an appropriate level of occupier comfort for all apartments.
- 8.8.5 To this end it is recommended that a condition be imposed requiring further consideration of noise attenuation during the preparation of working drawings, and requiring the provision of acoustic engineering advice for both internal and external noise sources. It is anticipated that adequate insulation against noise could be achieved without substantial changes to the design and layout of the building.

## **8.9 Private open space provision**

- 8.9.1 Private open space should be provided for each dwelling and should be located so that it is accessible directly from internal living areas (Council Wide PDC 148). Private open space areas located above ground level should have a minimum width of 2 metres and be directly accessible from a habitable room (Council-wide PDC 153). One bedroom dwellings should have a minimum of 8m<sup>2</sup>, while two bedroom dwellings should have a minimum of 11m<sup>2</sup> (Council Wide PDC 152).

- 8.9.2 All of the proposed dwellings would provide two bedrooms. Apartments 104, 204 and 304 would be provided with 8m<sup>2</sup> of private open space and apartments 105, 205 and 305 would be provided with 10m<sup>2</sup> of private open space. These would be below the desired minimum area for two bedroom dwellings.
- 8.9.3 Apartments 106, 206 and 306 would be provided with 12.5m<sup>2</sup> of private open space, while apartments 107, 207 and 307 would be provided with 15m<sup>2</sup> of private open space. Each would achieve the desired minimum area for two bedroom dwellings.
- 8.9.4 Apartments 108, 208 and 308 would be provided with 32m<sup>2</sup> of private open space, while apartments 101-103, 201-203, and 301-303 would be provided with approximately 22m<sup>2</sup> of private open space. Each would achieve substantially more than the desired minimum area for two bedroom dwellings.
- 8.9.5 All primary areas of private open space are directly accessible from and well integrated with living areas. It is also noted that all balconies would feature a 2m minimum dimension, with 21 of the 24 apartment balconies featuring a 2.5m minimum dimension.
- 8.9.6 The proposal would not provide 11m<sup>2</sup> of private open space in all instances, with 6 of the 24 apartments featuring either 8m<sup>2</sup> or 10m<sup>2</sup>. A wide range of balcony sizes, layouts and orientations would be provided within the building, many of which would be substantially in excess of the 11m<sup>2</sup> desirable minimum, providing diversity for prospective purchasers. Given this, the departure from the relevant principles is relatively minor in nature and the provision of balconies is supported in its current form.

## **8.10 Car Parking and Bicycle Parking**

- 8.10.1 Within the Urban Corridor Zone, it is anticipated that the provision of car and bicycle parking would be in accordance with Tables Pr/5 and Pr/6 of Council's Development Plan. It is also anticipated that on-site vehicle parking would not be visible from the primary street frontage through the use of design solutions such as locating parking areas behind the front building façade and screening undercroft parking areas with landscaping and articulated screening (UCZ Desired Character Statement).
- 8.10.2 Table Pr/6 outlines an anticipated demand of one bicycle park for every four dwellings, and one bicycle park for visitors for every ten dwellings. Therefore 9 bicycle parks should be provided.
- 8.10.3 With regard to the provision of car parking, 1 car parking space is desired for each 2 bedroom dwelling. An additional 0.25 spaces is desired per dwelling for visitor parking. Consequently, the anticipated car parking rate for the 24 dwellings would be 30 car parking spaces, comprised of 24 for occupants and 6 for visitor parking.
- 8.10.4 In total, 29 car parking spaces and opportunities for up to 16 bicycle (or scooter) parking spaces would be provided. It is noted that two of the car parking spaces would be disabled parking spaces in accordance with the relevant Australian Standard. Visitor car parking would be located conveniently adjacent the ingress point, while disabled car parking would be adjacent the foyer. Bicycle and scooter parking areas would be divided such that the majority are adjacent the foyer with additional spaces adjacent the ingress point.

8.10.5 It is anticipated that the additional supply of bicycle and scooter parking will offset the minor shortfall in car parking provided on-site and the proposed configuration is therefore supported.

## **8.11 Stormwater Management**

8.11.1 The provisions of Council's Development Plan suggest that site drainage should be designed to safely direct surplus flows to a public street without causing harm to adjoining properties (Council Wide PDC 97) and that all proposed developments should be designed to retain as much stormwater as possible, minimising the overflow to the kerb and water table (Council Wide PDC 98).

8.11.2 The applicant has prepared a stormwater management plan and accompanying calculations that provide for the satisfactory detention on, and disposal from, the subject site, in the opinion of engineering staff. At the advice of engineering staff, it is recommended that an additional condition regarding the quality of water run-off from the car park be imposed.

8.11.3 While the design is satisfactory, it is recommended that the applicant be encouraged by way of advisory note to further examine opportunities for rain-water re-use within the subject site during the preparation of working drawings.

## **8.12 Waste Management**

8.12.1 It is anticipated that new development would enable waste management options that provide adequate storage while screening these areas from public view. The design of driveway crossovers, parking areas, accessways and elements that interact with the public realm should also safely and efficiently accommodate the collection of waste and recycling materials.

8.12.2 Additionally, new developments should provide a dedicated area for the on-site storage, collection and sorting of recyclable materials and waste that is safe and convenient (Council Wide PDC 147, 169 and 170).

8.12.3 A communal waste system would be available for the provision of waste, green waste and recycling within an appropriately sized designated area of the car park. SUEZ Environment (formerly SITA Australia) have reviewed the proposal and confirmed that waste removal trucks would enter the site and collect bins from within the car parking area. To better facilitate this, the car park ceiling height has been raised by 300mm, while access to the waste storage area would now be internal only.

8.12.4 With reference to the *South Australian Better Practice Guide – Waste Management in Residential or Mixed Use Developments*, it can be anticipated that a high density apartment building comprising 24 two bedroom dwellings would generate 1,440L of general waste, 1,200L of recycling waste and 480L of organic waste per week.

8.12.5 The waste storage area would provide 2,200L of general waste storage and 1,920L of recycling waste, with no provision for organic waste. While these storage capacities are supported with reference to general and recyclable waste, the waste storage area must be provided with organic waste bins for the disposal of food organic waste from kitchen bio-baskets and for landscape maintenance purposes.

8.12.6 In the context of the above; the waste management plan and methodology is supported, subject to a condition requiring the provision of a minimum four additional 240L bins for the purposes of organic waste storage.

### **8.13 Overshadowing**

8.13.1 Generally, the design and location of buildings should enable direct winter sunlight into adjacent dwellings and private open space areas while minimising the overshadowing of windows of main internal living areas, upper-level private balconies that provide the primary open space area for a dwelling and solar collectors (Council-wide PDC 138).

8.13.2 The subject site, along with properties directly east and south of the subject site, is identified to be developed at a greater intensity than that of the existing built form. It is noteworthy that the adjacent southern site is presently vacant in anticipation of a future development.

8.13.3 Given that the subject site is not located adjacent a different zone, the overshadowing provisions that apply generally within the Council are less relevant to the proposed development. It is anticipated that the overshadowing impact would be consistent with that expected for new development in the Urban Corridor Zone.

### **8.14 Visual Privacy**

8.14.1 It is anticipated that a variety of measures should be used to minimise direct overlooking into adjacent internal living and private open space areas. Such measures should be integrated into the overall building design and should have minimal negative effect on the amenity enjoyed by the occupants of neighbouring dwellings (Council Wide PDC 139).

8.14.2 It is noted that the commonly used 1.7m and 1.8m high privacy screens for windows and balconies referred to in Council Wide PDC 90 are specifically excluded for buildings that are three or more storeys in height in the Urban Corridor Zone.

8.14.3 Notwithstanding this, the proposal would feature 1.5m high screening to balconies on the southern facade of the building. It can be anticipated that future development on the adjoining allotments may adopt a relatively similar approach and would be similarly capable of screening its northern facade without impact to the internal amenity enjoyed by occupants.

8.14.4 Given the above, and the anticipated development of the presently vacant adjoining allotment, no unreasonable privacy impacts upon adjacent properties or future occupants of the proposed building are anticipated.

## **9. CONCLUSION**

9.1 The proposal seeks to establish a medium to high density residential land use on the subject land. The building would be four storeys in height, which is the maximum height anticipated by Council's Development Plan for this site. Given the topography of the site, from the east the building would appear as considerably lower. The bold statement made with the building being the maximum height is appropriate for this key corner site.

- 9.2 The proposal would be generally consistent with the required setbacks, have adequate private open space, waste collection and thermal comforts in accordance with the Development Plan provisions. The floor plans proposed would provide functional and usable living spaces, and privacy and noise impacts would be moderated through good design and noise attenuation techniques.
- 9.3 Vehicular ingress would be provided from Regency Road while egress would be provided to Prospect Road, with appropriate car and bicycle parking areas provided. The proposal would provide suitable security and visual privacy outcomes, with overshadowing impacts minimised.
- 9.4 The application is therefore considered to be relatively consistent with the relevant provisions of the Prospect (City) Development Plan and warrants the granting of development plan consent, subject to the reserving of some matters for further assessment and appropriate conditions.

## **10. RECOMMENDATION**

It is recommended:

That with reference to the relevant provisions of the Prospect (City) Development Plan, the zoning of the land within which the proposed development is situated and the locality within which the land is situated, the Panel resolves that development application 050/345/2014 is not seriously at variance with the Development Plan and as such a decision shall be made on the merits of the application; and

That pursuant to the *Development Act 1993*, as amended, Development Plan Consent be approved to DA 050/345/2014 from Vartzokas Architects Pty Ltd for a Four Storey Residential Flat Building comprising 24 Dwellings with Associated Car and Bicycle Parking, and Landscaping at 227-229 Prospect Road Prospect (CT 5240/49), subject to the following reserved matters, and draft conditions and notes that may be added to or altered following satisfactory resolution of reserved matters by Council administration:

### **Conditions:**

1. The development shall take place in accordance with plans and details stamped by Council relating to Development Application Number 050/345/2014, except as modified by any conditions detailed herein. All works detailed in the approved plans and required by conditions are to be completed prior to the occupation of the approved development.
2. Prior to the grant of full development approval; an acoustic report from a qualified acoustic engineer shall be submitted to Council detailing any acoustic treatments or measures required to demonstrate that the proposal meets the requirements of the Development Plan with respect to Council Wide Principles of Development Control 92, 93 and 111.

Any required treatments or measures therein identified shall be established and maintained at all times to the satisfaction of Council.

3. All driveways, parking and manoeuvring areas must be formed, surfaced with concrete, bitumen or paving, and be properly drained. The surfacing of the driveway and drainage shall be maintained to the reasonable satisfaction of Council thereafter.
4. The drainage system shall be designed, installed and maintained at all times thereafter to ensure that water from the site does not:
  - a) Flow or discharge onto adjoining properties;

- b) Flow across the surface of footpaths or public ways;
  - c) Affect the stability of any building; or
  - d) Create unhealthy or dangerous conditions on the site or within any building.
5. Stormwater quality measures such as a gross pollutant trap shall be implemented to ensure that all surface run-off, stormwater or other liquid discharging from the site, is free of site contaminants including (but not limited to) oils, grease, fuels, rubbish, litter and silt.
  6. The features and materials set out in Drawing Number 'PDA - 02' Dated 'FEB'15' approved herein are essential elements of the building and shall be established and maintained at all times to the reasonable satisfaction of Council. A schedule of colours and finishes for these elements will be provided to the satisfaction of Council. These colours and finishes shall also be maintained at all times to the reasonable satisfaction of Council.
  7. Driveways, car parking spaces, manoeuvring areas and landscaping areas shall not be used for the storage or display of materials or goods including waste products and refuse.
  8. Permanently fixed clotheslines shall not be permitted within 1.5m of the edge of each balcony.
  9. To maximise the efficiency of waste recycling:
    - a) Provision shall be made for the separation of recyclable materials for collection and recycling, including paper, cardboard, glass and plastic containers, tins, and any other plastic that 'holds its shape';
    - b) Separate provision shall be made for the collection of food waste (food organics) and food-contaminated cardboard, paper or paper products, which are to be collected for composting; and
    - c) Paper attached to plastic, wax paper or chemically-treated/gloss cardboard will not be included with the materials collected for composting.
  10. Four 240L bins shall be provided within the 'Garbage Bins Enclosure' as described by the herein endorsed ground floor plan, for the purposes of organic waste storage and collection.
  11. Service vehicle movements shall occur outside of the peak periods associated with the use approved herein and in any event shall not occur before 6am or after 9pm on any day.
  12. The building must be maintained, kept tidy, free of graffiti and in good repair and condition at all times.
  13. All car parking spaces must be line-marked in accordance with the approved plans and to comply with the Australian/New Zealand Standard for Parking Facilities (Part 1: Off-street Car Parking (AS/NZS 2890.1:2004) prior to occupation.
  14. The surfacing of the car park, line marking and directional arrows shall be maintained to the reasonable satisfaction of Council at all times.
  15. Driveways, parking and manoeuvring areas shall be lit in accordance with the Australian Standard for Lighting for Roads and Public Spaces (AS1158.1 and AS1158.3) during the hours of darkness that they are in use and accessible by the general public.
  16. Wheel-stops or similar non-mountable devices shall be provided to each northern car park to prevent damage to the building.

17. Landscaping shall be provided in the areas described by the herein endorsed Ground Floor Plan. A substantial amount of landscaping shall be provided in the area described as Road Widening Policy by the herein endorsed Ground Floor Plan to the satisfaction of Council. Landscaping shall include low-lying shrubs and medium height trees (growing to minimum 4-6m height).
18. All landscaping areas shall be established and maintained at all times to the reasonable satisfaction of Council. The applicant or the persons for the time being making use of the subject land shall cultivate, tend and nurture the landscaping, and shall replace any landscaping that becomes diseased or dies. An automated drip irrigation or similar watering system shall be established and maintained to ensure that sufficient water is available to satisfy the needs of the landscaping species selected.
19. Air-conditioning units and solar hot water heaters shall be provided with screening devices designed to complement the colours, materials and finishes of the building approved herein, and shall be sited to adequately screen the units from view to the reasonable satisfaction of Council.
20. During construction of the development approved herein, measures will be implemented to ensure that the construction works do not result in an unreasonable impact on occupiers of adjacent properties or pollution of existing infrastructure through drag-out or stormwater runoff. Measures shall include as necessary:
  - a) A hard surface and controlled washing zone at the entry/exit points to the site, designed to reduce the potential for mud and material dragged out by construction vehicles; and
  - b) Containment of stormwater run-off within the site, which if being discharged into the stormwater system will be filtered to the satisfaction of Council; and
  - c) Reduction of the potential for dust and other airborne particles by the use of water sprinklers and/or other means of containment; and
  - d) The establishment of an appropriate storage compound for waste materials and litter. No building waste material shall be stored outside of the storage compound or similar industrial bin; and
  - e) All mechanical equipment shall be used in a manner to minimise the potential for noise pollution and ensure compliance with the requirements of the Environment Protection (Noise) Policy.
21. Footpaths adjacent to the site are to be kept in a safe condition for pedestrians at all times during construction works. All driveways and footpaths traversed by vehicles using the site are to be maintained in a reasonable condition for the duration of the works, and are to be reinstated to the satisfaction of Council on completion of the works.

No obstruction of the footpath or roadway may occur without the prior permission of Council. For further advice, please contact Council's Infrastructure and Environment Department on 8269 5355.
22. To ensure compliance with applicable standards as described in the Environment Protection (Noise) Policy established under the Environment Protection Act, construction activities shall only take place between the hours of 7:00am and 7:00pm, Monday to Saturday inclusive, and not on Sundays or public holidays.

**The following conditions are applied at the direction of the DPTI:**

1. All building works and the at grade car park shall be set back clear of the 4.5 metres road widening requirements along the Prospect Road and Regency Road frontages of the site.
2. The cantilevered awnings located adjacent to the proposed main entry that extend into the 4.5 metre strip requirements shall be designed to be removable so that they can be removed or modified should DPTI need to install, erect and/or maintain infrastructure in this area. On the direction of the Commissioner of Highways, the awnings shall be removed or modified at no cost to the department.

**The following conditions are applied at the recommendation of the DPTI:**

1. The access points to Regency Road and Prospect Road shall be constructed in accordance with Vartzokas Architects Pty Ltd, Sheet 1 Floor Plans, Job No. A213-464, Drawing No. PDA – 02, Plotted 25/2/15, dated FEB'15.
2. All access shall be gained via a one-way flow through the site with ingress movements (left in only) via Regency Road and egress movements (left out only) via Prospect Road only.
3. The access point/s shall be angled at 70 degrees to the kerb line and be appropriately signed and line marked in order to reinforce the desired traffic flow.
4. The ingress roller door shall be setback to maximise vehicular storage clear of the road widening strip.
5. The egress roller door shall be designed in accordance with AS/NZS2890.1:2004 Figure 3.3 Minimum Sight Lines For Pedestrian Safety.
6. All vehicles shall enter and exit the site in a forward direction.
7. Refuse collection shall be undertaken within private property and ensure vehicular movements through the car park are not impeded during collection times.
8. The Regency Road and Prospect Road crossovers shall provide a minimum of 1.0 metre separation from all roadside infrastructure.
9. Any portions of redundant crossover shall be closed and reinstated to Council's satisfaction at the applicant's cost prior to the habitation of any dwellings.
10. All bicycle facilities shall be designed in accordance with AS2890.3:1993 - Bicycle parking facilities.
11. Stormwater run-off shall be collected on-site and discharged without jeopardising the integrity and safety of Regency and Prospect Roads. Any alterations to the road drainage infrastructure required to facilitate this shall be at the applicant's cost.

**Advisory Notes:**

- (1) Pursuant to Section 86(1)(a) of the Development Act, 1993, you have the right of appeal to the Environment, Resources and Development Court against either 1) a refusal of consent or 2) any condition(s) which have been imposed on a consent. Any such appeal must be lodged with the Court within two (2) months from the day on which you receive this notification or such longer period as may be allowed by the Court.

The Environment, Resources and Development Court is located in the Sir Samuel Way Building, Victoria Square, Adelaide SA 5000 (Postal Address: GPO Box 2465, Adelaide SA 5001).

- (2) The development plan consent granted herein is effective for a period of twelve (12) months from the date of the decision. Unless Council extends this period, building rules consent is required within this time or the consent will lapse.

Any request for an extension of the operative period of the consent must be submitted to Council in writing, accompanied by the applicable fee.

- (3) Further application pursuant to the Local Government Act shall be made to the Infrastructure Assets and Environment Department for the proposed crossover prior to construction activities occurring.

Road/Kerbing/Footpath Works will need to be inspected by an Assets and Infrastructure Officer to determine they have met all relevant requirements. All work including line marking will be the responsibility of the applicant as will the reinstatement of any damaged Infrastructure / Services related to these works. All works will be carried out at the cost to the applicant.

- (4) Prior to the commencement of construction of the development herein approved, it is strongly recommended that you employ the services of a licensed Land Surveyor to carry out an identification survey of the subject land and to peg the true boundaries, to ensure that building work will be either on the true boundaries or the specified distance from the true boundaries of the subject land, as the case may be.

Failure to correctly site the development on the land in accordance with the plans approved herein would constitute a breach of the *Development Act 1993*. Any amendments required to the approved plans as a result of the survey are to be submitted to Council for approval prior to works commencing.

- (5) You are encouraged to consult with adjoining property owners before commencing any work, to assist in minimising nuisance or inconvenience caused during construction.

- (6) You are required to give formal notification to, and consult with, the adjoining property owner if you are removing, replacing or altering an existing fence or building a freestanding wall along the common boundary that would, for all purposes, be a dividing fence (Section 5 of the *Fences Act 1975*).

- (7) If you (the building owner) are undertaking building work that affects the stability of other land or premises, namely:

- an excavation which intersects a notional plane extending downwards at a slope of 1 vertical to 2 horizontal from a point 600mm below natural ground level at a boundary with an adjoining site (as depicted in figure 1); or
- an excavation which intersects any notional plane extending downwards at a slope of 1 vertical to 2 horizontal from a point at natural ground level at any boundary between 2 sites (not being a boundary with the site of the excavation), where the

boundary is within a distance equal to twice the depth of the excavation (as depicted in figure 2); or

- any fill which is within 600mm of an adjoining site, other than where the fill is not greater than 200 millimetres in depth (or height) and is for landscaping, gardening or other similar purposes;

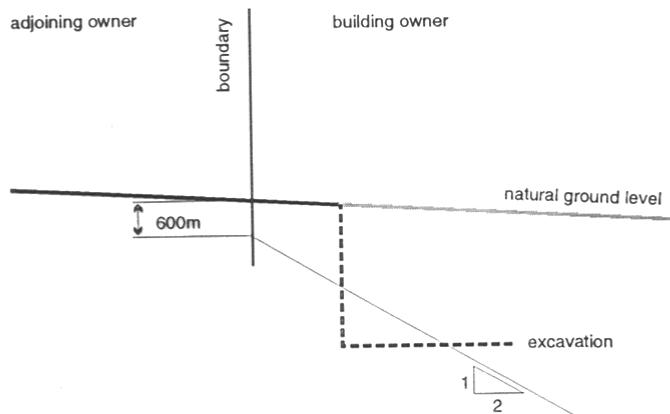
Then you (the building owner) must, at least 28 days before the building work is commenced:

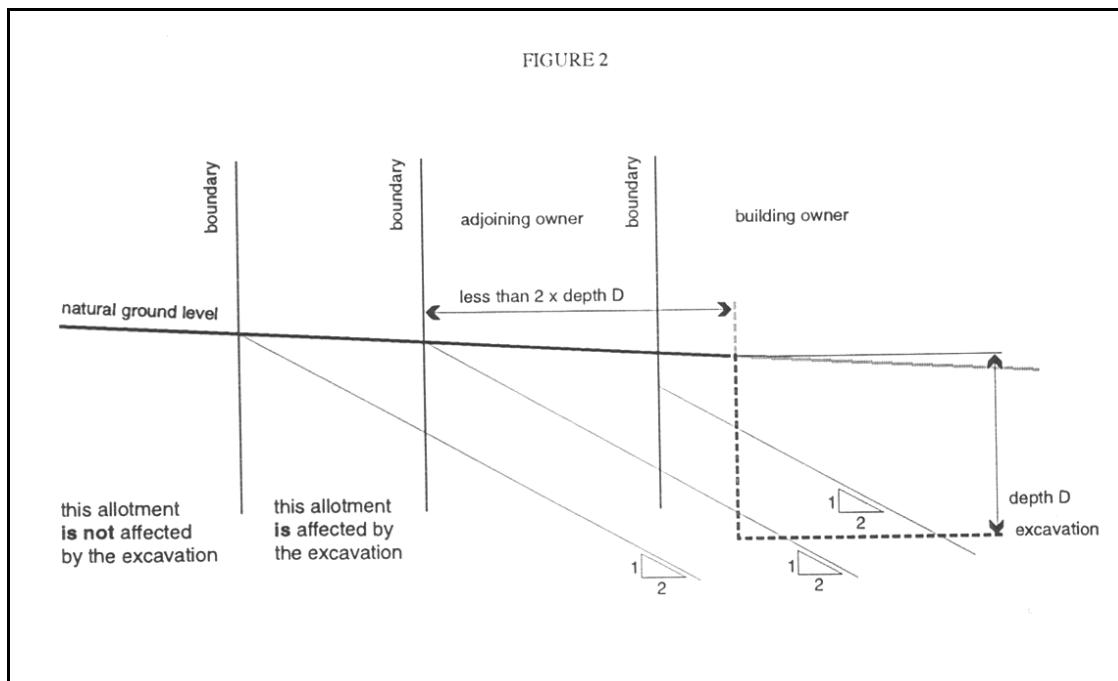
- serve on the owner of the affected land or premises a notice of intention to perform the building work and describing the nature of that work; and
- you must take such precautions as may be prescribed to protect the affected land or premises and must, at the request of the owner of the affected land or premises, carry out such other building work in relation to that land or premises as that adjoining owner is authorised by the regulations to require.

If you fail to comply with these notification requirements, then you are guilty of an offence with a maximum penalty of \$10 000.

You may apply to the Court for a determination of what proportion (if any) of the expense incurred by you in the performance of the building work requested by the owner of affected land or premises (under subsection (b) above) should be borne by the owner of that land or premises, and you may recover an amount determined by the Court from the owner of the affected land or premises as a debt.

FIGURE 1





- (8) The following notes provide important information for the benefit of the applicant and are required to be included in any approval:

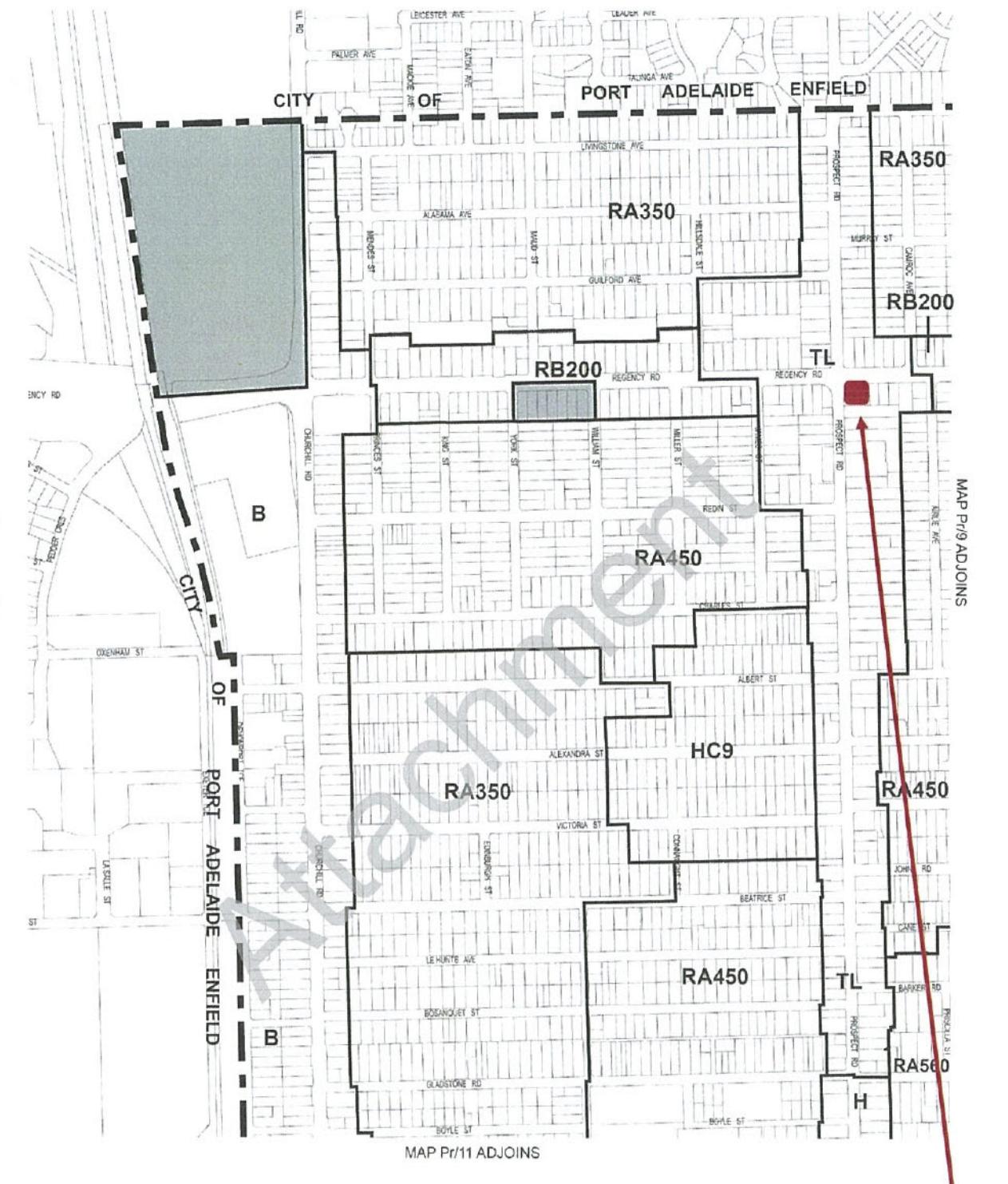
i. The Metropolitan Adelaide Road Widening Plan shows a possible requirement for a strip of land up to 4.5 metres in width from the Prospect Road and Regency Road frontages of this site for the future upgrading of the Prospect Road / Regency Road intersection, together with a corner cut-off. The consent of the Commissioner of Highways under the Metropolitan Adelaide Road Widening Plan Act is required to all building works on or within 6.0 metres of the possible requirement.

Subsequently, should Council approve the application in its current form, the applicant should fill out the attached consent form and return it to DPTI with 3 copies of the approved plans.

ii. Any future signage applications should be designed in accordance with DPTI "Advertising Signs - Assessment Guidelines for Road Safety" (August 2014). The document is available via the following link:

[http://www.dpti.sa.gov.au/\\_data/assets/pdf\\_file/0019/145333/DPTI-Advertising-Signs-Assessment-Guidelines.pdf](http://www.dpti.sa.gov.au/_data/assets/pdf_file/0019/145333/DPTI-Advertising-Signs-Assessment-Guidelines.pdf)

**Scott McLuskey**  
Senior Development Officer Planning



RA560	Residential Policy Area A650
RA450	Residential Policy Area A450
RA350	Residential Policy Area A350
RB200	Residential Policy Area B200
TL	Transit Living Policy Area
B	Boulevard Policy Area
H	High Street Policy Area
HC9	Historic Conservation Area 9 Policy Area

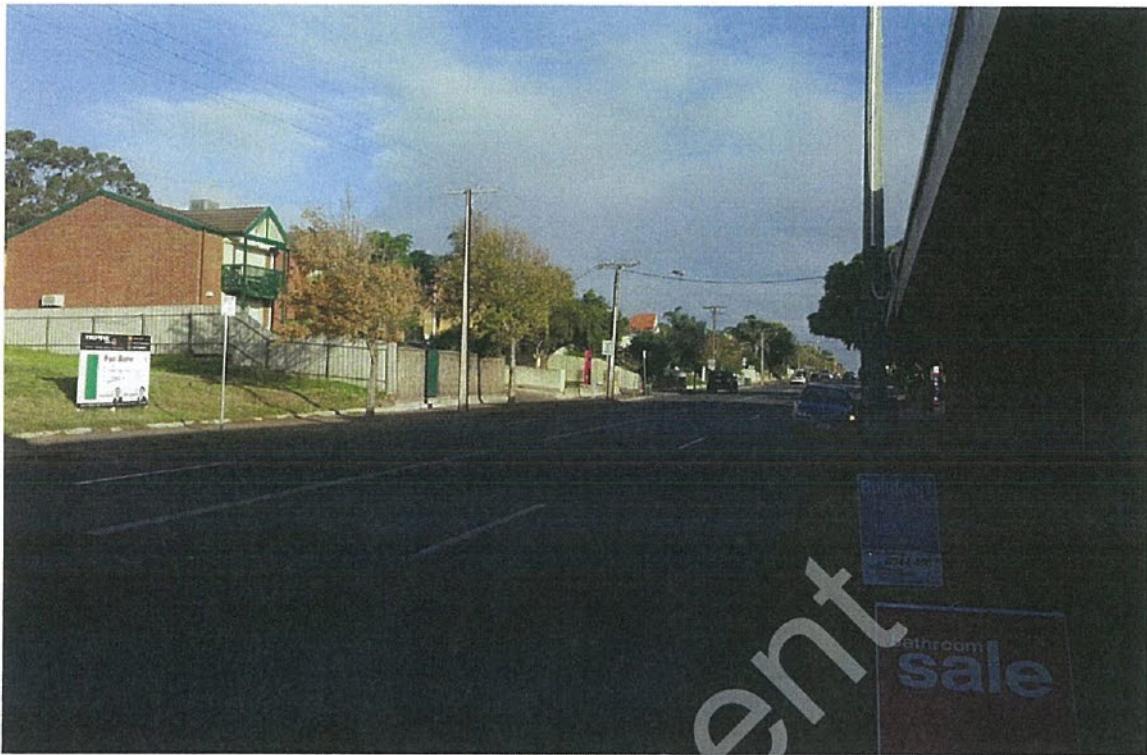




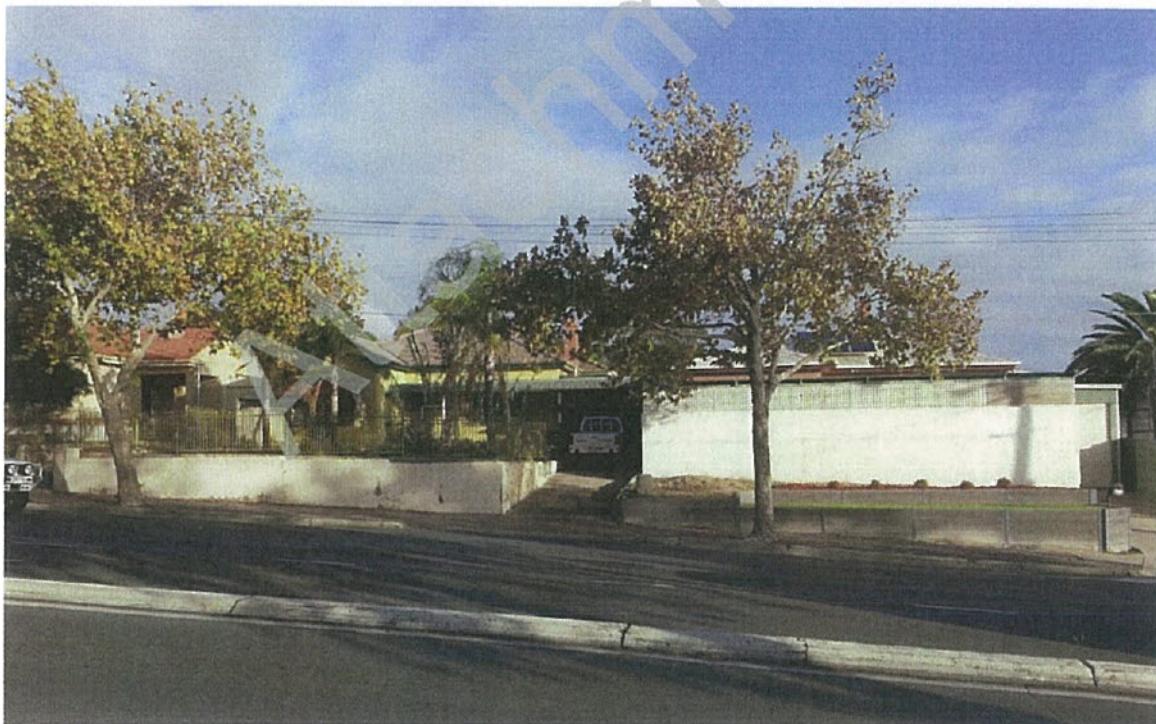
Adjacent property at 225 Prospect Road Prospect



Prospect Road streetscape facing north of subject site



Prospect Road streetscape facing south of subject site



Adjacent properties at 409 and 411 Regency Road Prospect



Regency Road streetscape facing east from subject site



Civic Centre  
128 Prospect Road  
Prospect SA 5082 AUSTRALIA  
Telephone: 08 8269 5355  
Email: admin@prospect.sa.gov.au

## Subject Site



Notes	Disclaimer
	<p>This map is a representation of the information currently held by the City of Prospect. While every effort has been made to ensure the accuracy of the product, Council accepts no responsibility for any errors or omissions. Any feedback on omissions or errors would be appreciated.</p>



Subject site, photograph taken from Regency Road facing south



Subject site, photograph taken from Prospect Road facing east



LEVEL 1/216 GLEN OSMOND ROAD, FULLARTON S.A. 5063 A.B.N. 40 897 503 620 TEL : (08)8379 9803 FAX : (08)8379 9811  
WEB : [WWW.VARTZOKASARCHITECTS.COM.AU](http://WWW.VARTZOKASARCHITECTS.COM.AU) EMAIL : [ADMIN@VARTZOKASARCHITECTS.COM.AU](mailto:ADMIN@VARTZOKASARCHITECTS.COM.AU)

**Attention Scott McLuskey,  
Senior Development Officer  
City of Prospect  
PO Box 171,  
PROSPECT SA 5082**

29th May 2015

Dear Scott,

**RE DEVELOPMENT APPLICATION DA 050/345/2014**

Please find attached the supporting information regarding the above-mentioned development application.

1. 3-D presentation of the building indicating the use of various material finishes providing variety and texture to the building visually, together with set-back and relief in the various building elements to create light and shade and minimise a monolithic or repeating built form.
2. Improved details of the ground floor footprint of the building on the site, indicating that the building has been sited as low into the existing site as possible to reduce overall building height and to provide ease of access from Regency Road, and ease of exit onto Prospect Road. Height of building 'plinth' along eastern boundary: retaining wall clarified adjacent fire exit at eastern boundary and indicated as low, and stepped to follow profile of adjoining site at eastern boundary.
3. Landscaping details, (plant selections and the like) included in the drawings.
4. The waste management system has been discussed at length with Eddie Georgonicas (Territory Manager) from Suez environment (Formerly SITA Australia).

The waste collection vehicles will be able to enter the site via Regency road, and move through the central aisle of the carpark of the building, stopping at or near the waste bin storage area, and then manually depositing the refuse and recyclables into the truck and then leave via the Prospect Road exit from the carpark. The collection vehicle is 3.4m high, which can be accommodated by providing a clear opening and clearance height in the carpark aisle of 4.4m minimum (at the Regency Road end) and increasing in height clearance toward the Prospect Road end of the carpark. We have adjusted the carpark to provide the shared disable carparking space adjacent to the waste bin storage area. This space will

2.

double as a shared disable space and provide ease of access to Suez staff in the collection of refuse and recyclables.

We have also moved the carparks on the corner of the carpark aisle to provide improved manoeuvrability for the collection vehicles as they turn.

We will maintain the 4.2 clearance and not create additional height to the building by ensuring that hydraulic and other services are incorporated into the non carpark aisle ceiling areas of the ground floor. (Eddie Georganicas can be contacted at Suez on 0418 195 434).

5. We enclose updated stormwater details, which have been updated from the original design post the 4.5m road widening.

If you require any further clarification in regard to any of these items, please do not hesitate to contact me in the first instance.

Scott, as you are aware this application has been with Council since early September last year, and following the extraordinary delay in resolving the issue of DAP and their road widening requirements, the project cannot afford further delay, as this is a serious risk of not proceeding.

Whilst I appreciate the DAP's concerns needed clarification, I would urge a more consultative approach at providing information to assist in their decision making process. Please forward this commentary to the DAP members prior to the next meeting, in order that they can assess our response, and in the event that they may require further clarifications we can provide these in time for their assessment at the June meeting. Alternatively, I ask that any outstanding concerns be attached as a condition of approval for the project.

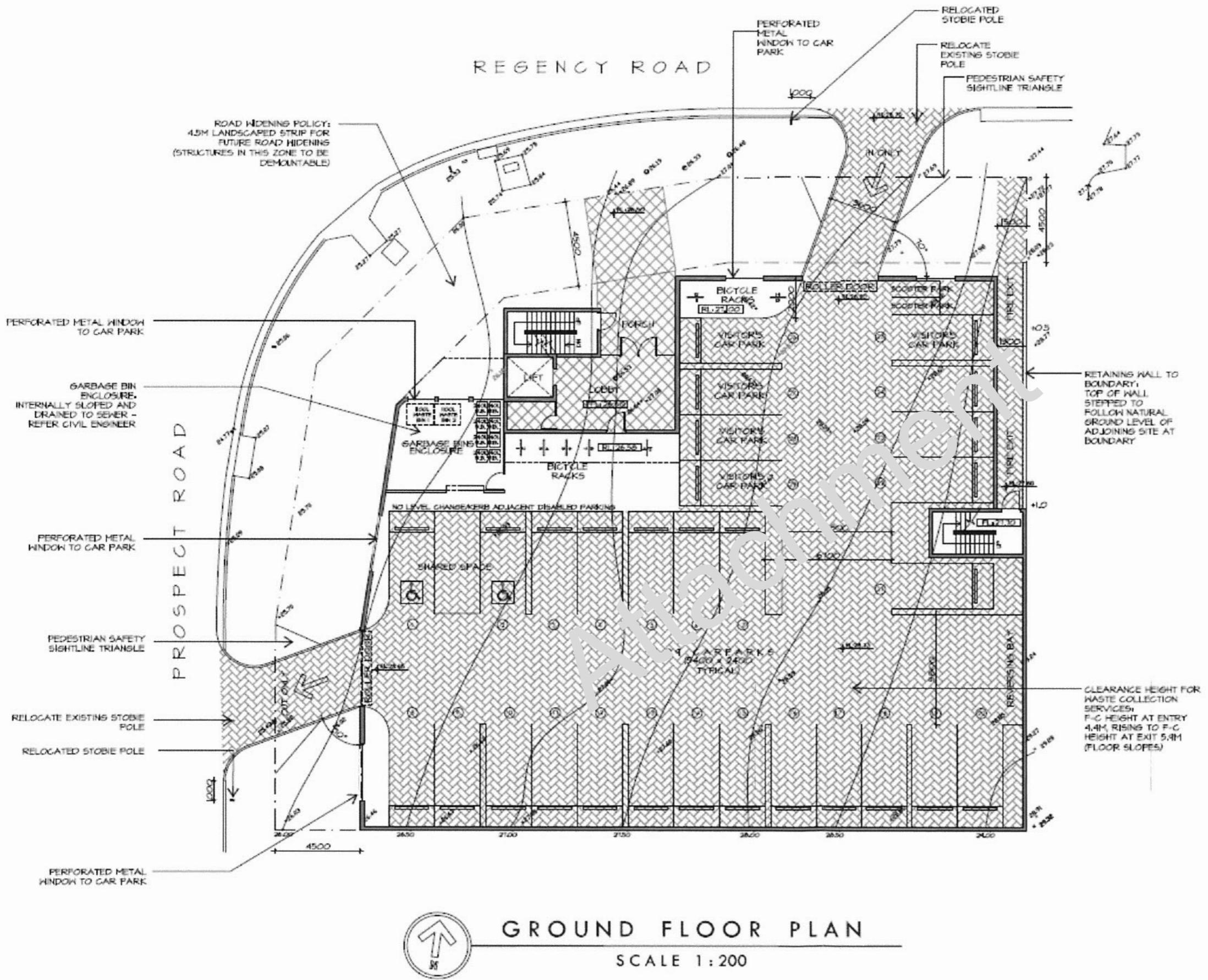
Regards

Vartzokas Architects Pty Ltd.



Tom Vartzokas B.Arch RAIA  
PRINCIPAL/MANAGING DIRECTOR

Cc Nazero



VARTZOKAS ARCHITECTS PTY LTD

Level 1/216 Glen Osmond Road  
Fullarton SA 5063  
Phone: 08 8379 9803  
Fax: 08 8379 9811  
e-mail: [admin@vartzokasarchitects.com.au](mailto:admin@vartzokasarchitects.com.au)

**Project PROPOSED APARTMENT DEVELOPMENT**

Address CNR PROSPECT & REGENCY ROAD, PROSPECT

Client NAZERO

FLOOR PLANS

Drawn: PY      Checked: TV      Scale: 1:200@A1

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Date: MAY'15 Plotted:29/05/2015 Sheet No: 1 of 2

Job No: N214-473 Drg No: PDA - 03

Verify all dimensions and set-outs prior to commencing work.  
Figure dimensions take preference. Do not scale drawings.

## AREA SUMMARY

<b>GROUND FLOOR</b>	<b>80.22</b>	<b>SQ.M</b>
LOBBY	31.01	SQ.M
STAIRCASE	32.88	SQ.M
PORCH	16.33	SQ.M
<b>FIRST FLOOR RESIDENTIAL</b>	<b>848.38</b>	<b>SQ.M</b>
LIVING AREA	606.80	SQ.M
CIRCULATION SPACE	111.08	SQ.M
BALCONIES	130.50	SQ.M
<b>SECOND FLOOR RESIDENTIAL</b>	<b>848.38</b>	<b>SQ.M</b>
LIVING AREA	606.80	SQ.M
CIRCULATION SPACE	111.08	SQ.M
BALCONIES	130.50	SQ.M
<b>THIRD FLOOR RESIDENTIAL</b>	<b>848.38</b>	<b>SQ.M</b>
LIVING AREA	606.80	SQ.M
CIRCULATION SPACE	111.08	SQ.M
BALCONIES	130.50	SQ.M
<b>TOTAL BUILDING AREA</b>	<b>2,625.36</b>	<b>SQ.M</b>
CARPARK AREA	870.35	SQ.M
LANDSCAPE AREA	18.42	SQ.M
EXTERNAL FIRE ESCAPE	19.41	SQ.M
<b>TOTAL SITE AREA (inc. road widening)</b>	<b>1,266.94</b>	<b>SQ.M</b>
<b>TOTAL SITE AREA (exc. road widening)</b>	<b>988.40</b>	<b>SQ.M</b>

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**SUMMARY**

TOTAL ON-SITE CARPARKS	29
TOTAL 2 BEDROOM 2 BATH APARTMENTS	12
TOTAL 2 BEDROOM 1 BATH APARTMENTS	12
TOTAL VISITORS CAR PARK	5
BICYCLE RACKS	10
DISABLED CARPARKS NEEDED	2



**Project PROPOSED APARTMENT DEVELOPMENT**

**Address** CNR PROSPECT & REGENCY ROAD, PROSPECT

**Client** NAZERO

**FLOOR PLANS**

Drawn: PY	Checked: TV	Scale: 1:200@A1
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Date: MAY'15	Plotted: 29/05/2015	Sheet No: 1 of 2
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Job No: N214-473	Drg No: PDA - 03
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Verify all dimensions and set-outs prior to commencing work.  
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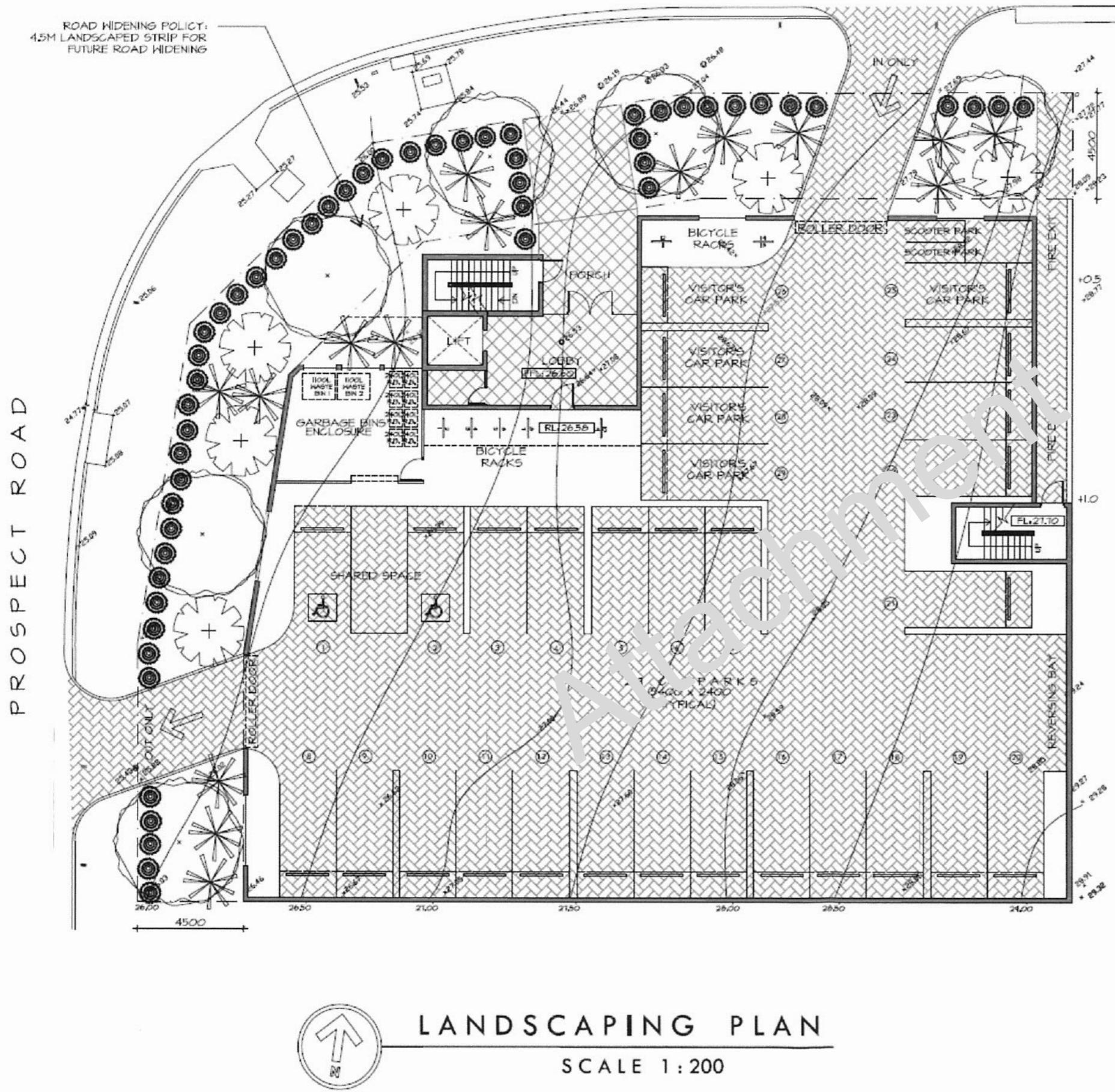
**AREA SUMMARY**

GROUND FLOOR	80.22 SQ.M
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PORCH	16.33 SQ.M
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**SUMMARY**

TOTAL ON-SITE CARPARKS	29
TOTAL 2 BEDROOM 2 BATH APARTMENTS	12
TOTAL 2 BEDROOM 1 BATH APARTMENTS	12
TOTAL VISITORS CAR PARK	5
BICYCLE RACKS	10
DISABLED CARPARKS NEEDED	2

## REGENCY ROAD

**VARTZOKAS ARCHITECTS PTY LTD**

Level 1/216 Glen Osmond Road  
Fullarton SA 5063  
Phone: 08 8379 9803  
Fax: 08 8379 9811  
e-mail: admin@vartzokasarchitects.com.au

**Project PROPOSED APARTMENT DEVELOPMENT**

**Address CNR PROSPECT & REGENCY ROAD, PROSPECT**

**Client NAZERO**

**Title FLOOR PLANS**

Drawn: PY	Checked: TV	Scale: 1:200@A1
Date: MAY'15	Plotted: 29/05/2015	Sheet No: 1 of 2

Job No: N214-473 Drg No: PDA - 03

Verify all dimensions and set-outs prior to commencing work.  
Figure dimensions take preference. Do not scale drawings.

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LANDSCAPE LEGEND	
TREE:	Pyrus calleryana 'Capital' (1.8m)
BUSH:	Callistemon 'Captain Cook' (up to 3m)
LOW BUSH:	Callistemon pityoides (up to 3m)
GRASS:	Bonga Borders Urlope (up to 900mm)



**PROPOSED APARTMENT  
DEVELOPMENT**

Address: 148 PROSPECT & REGENCY ROAD, PROSPECT

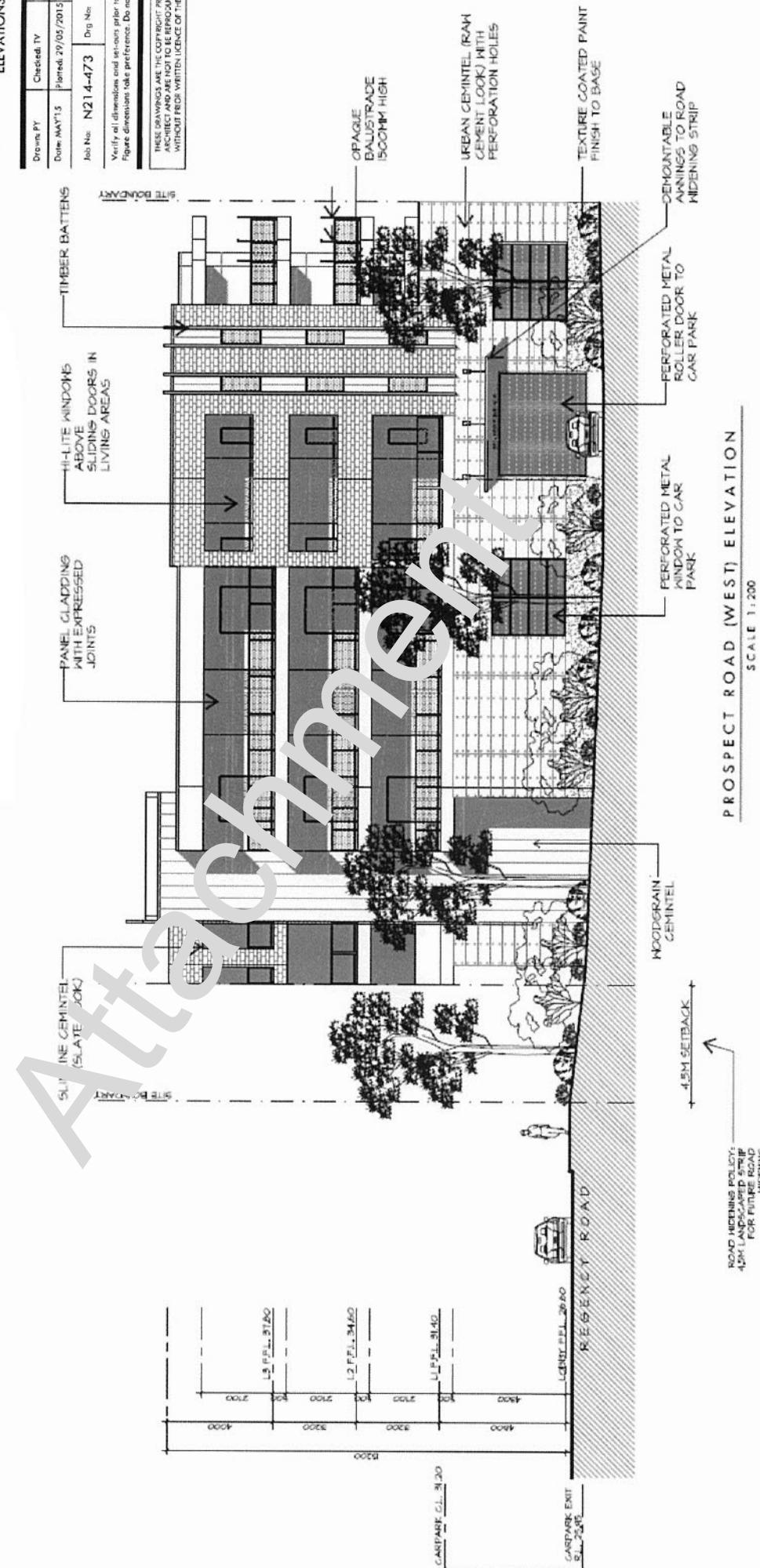
Client: NAZERO

**ELEVATIONS**

Drawn by:	Checked by:	Scale:
Date: May 5	Date: 29/05/2015	Sheet No. 2 of 2
Job No.: N214-473	Dwg No.:	PDA - 03

Verbal dimensions and sections prior to commencing work.  
 Square dimensions take preference. Do not scale drawings.

Attachment 13



**VARTZOKAS ARCHITECTS PTY LTD**  
 Level 1/216 Glen Osmond Road  
 Fullarton SA 5053  
 Phone: 08 8379 9803  
 Fax: 08 8379 9811  
 email: [vartz@optusnet.com.au](mailto:vartz@optusnet.com.au)

**PROPOSED APARTMENT DEVELOPMENT**  
 Cnr PROSPECT & REGENCY ROAD, PROSPECT

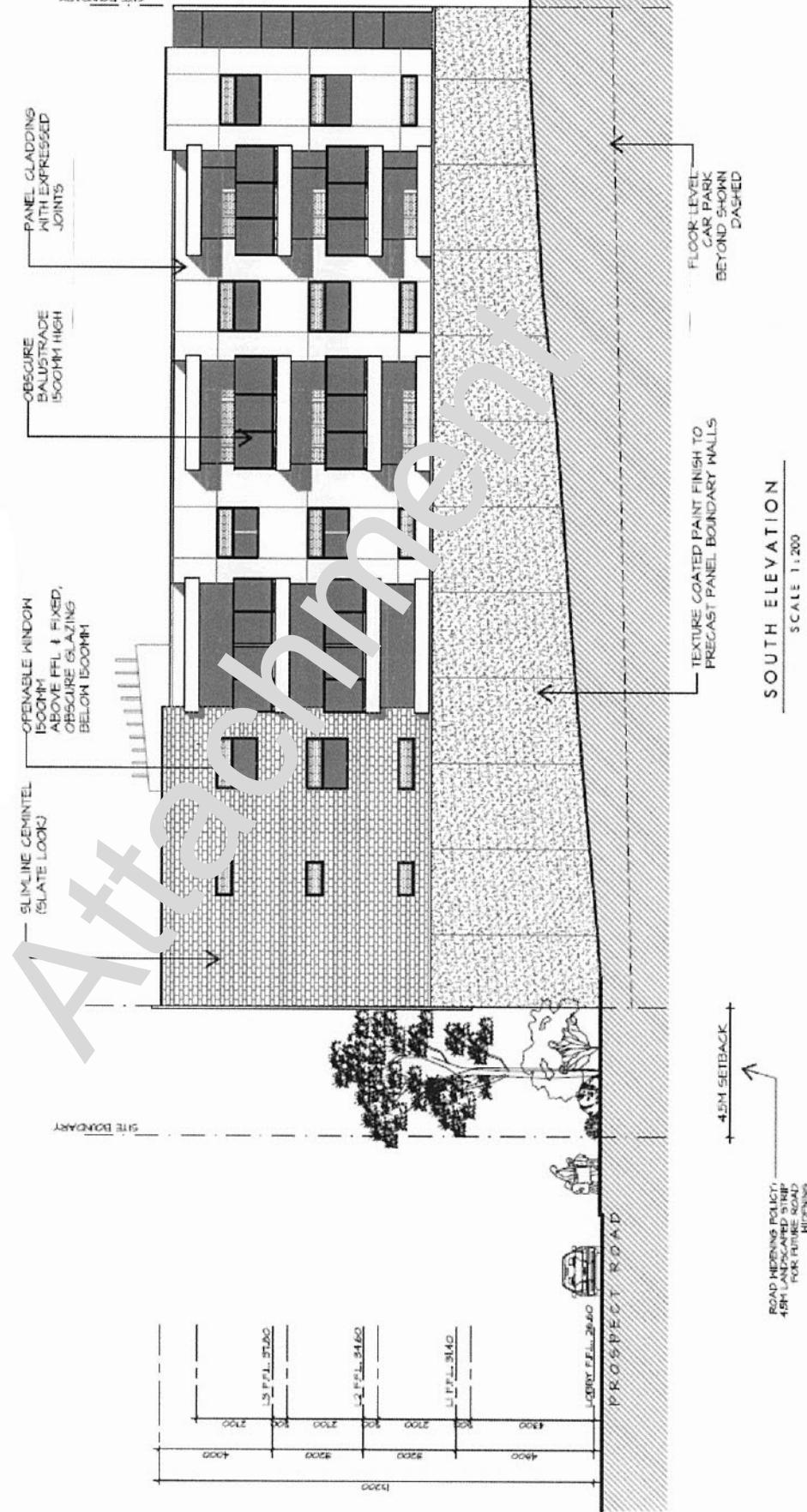
Project: NAZERO  
 Address: Client:

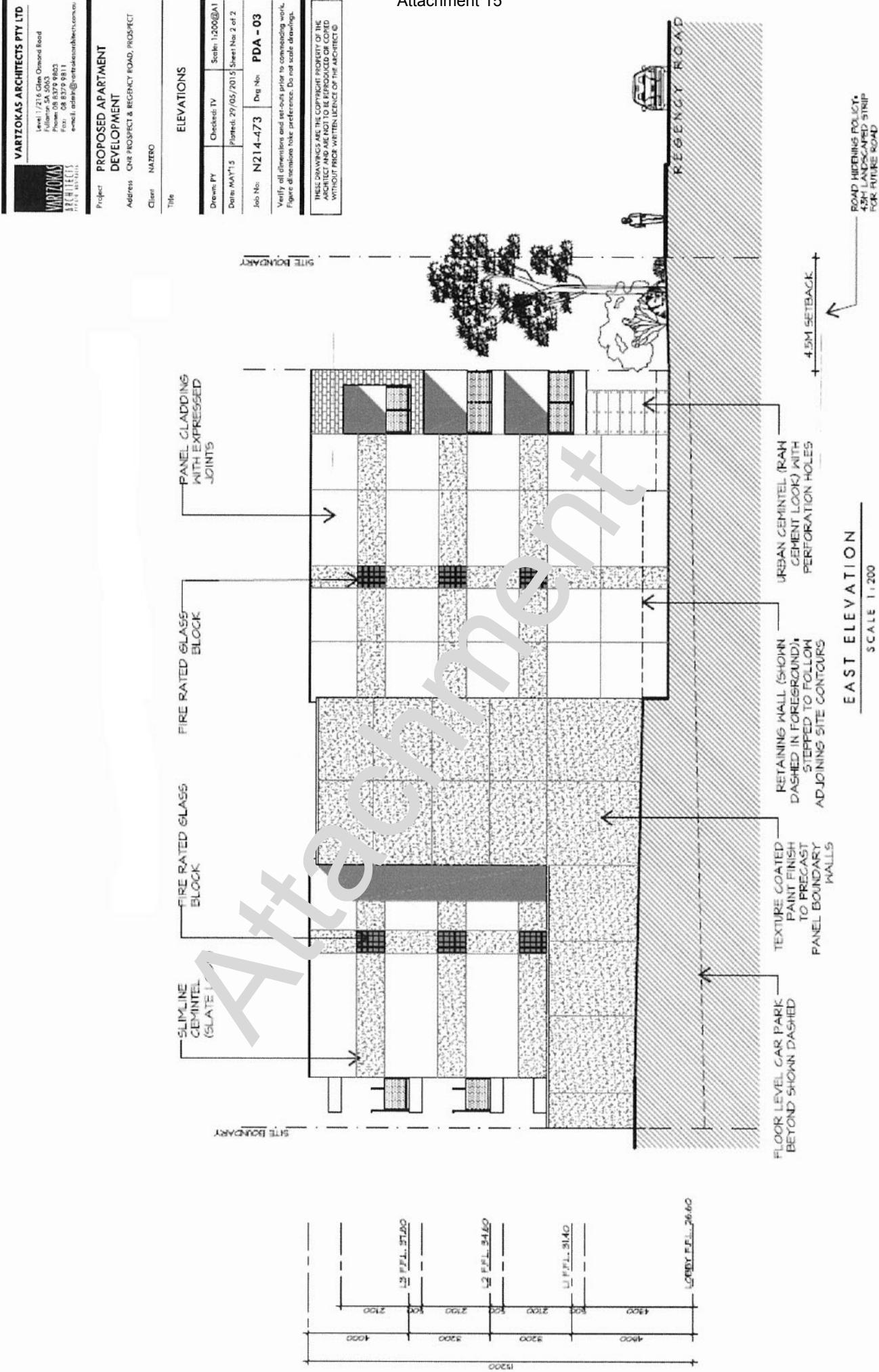
### ELEVATIONS

Drawn By:	Checked By:	Scale:
Date: MAY/15	Date: 29/05/2015	1:200 @A1
Job No:	Dra No:	Sheet No: 2 of 2
<b>PDA - 03</b>		

Verify all dimensions and set-outs prior to commencing work.  
 Figure dimensions take preference. Do not scale Drawings.

Attachment 14







**NZR APARTMENTS** 227-229 PROSPECT ROAD, PROSPECT  
© COPYRIGHT VARTZOKAS ARCHITECTS PTY LTD

LUMENSTUDIO  
Julian Rutt  
B.Arch (hons), AIA  
jrutt@adam.com.au  
0422 305 099

27 March 2015

By email: scott.mcluskey@prospect.sa.gov.au

**City of Prospect**  
Development Services  
128 Prospect Road  
Prospect SA 5082

**Attention: Scott McCluskey**  
**PRIVATE AND CONFIDENTIAL**

Dear Scott

**DESIGN REVIEW: 227-229 PROSPECT RD, PROSPECT**

Regarding the information provided on Vartzokas drawings PDA-01 and 02 for the proposed apartment building at 227-229 Prospect Road Prospect, and in the framework of providing design review advice, I offer the following comments;

At four storeys (two levels of apartments with ground level car park) the proposal meets the desire to increase density along major thoroughfares. The building's scale is appropriately broken down, noting that its size is (currently) at odds with its context of adjoining properties. There may be some impacts on neighbouring properties with the proposal being built close to the boundaries.

The building makes reasonable use of ideal orientation with access to northern light though aspect varies between apartments, while views gained from the raised corner have largely been used to advantage where possible though some internal bedrooms rely solely on borrowed light from internal spaces.  
Limited ability to use cross breezes is possible though there is no detail on auxiliary heating/cooling.

Landscaping is mostly limited to ground floor entry area and largely not possible to much of the ground floor due to the building footprint/overhang being maximised, separation of car park achieved by screening and walling at ground level and a review of the effect of the angled driveway/crossover of footpath upon the speed of vehicles may be warranted.

This proposal exhibits merit but could benefit from investigation and integration of environmental design principles.

Yours sincerely



Julian Rutt  
Architect, AIA

**LUMENSTUDIO**  
**Julian Rutt**  
B.Arch (hons), AIA  
jrutt@adam.com.au  
0422 305 099

26 March 2015  
**227-229 PROSPECT RD PROSPECT**

#### **Context**

*Good design responds to and contributes to its context. Context can be defined as the key natural and built features of an area.*

No detail provided on context related to adjoining properties and/or uses (domestic, commercial etc) though existing online street views indicate largely low-rise detached residential and commercial buildings in the vicinity and some double storey residential units to the south. The proposal of a four storey development meets aim of increasing density along the major corridors, this being on a prominent corner on an intersection of two major roads.

#### **Scale**

*Good design provides an appropriate scale in terms of the building height relative to the width of the street and height of the surrounding buildings.*

The overall size, height and bulk of the proposal appears to be suitable, the facades sufficiently broken down to minimise expansive wall areas lacking relief and incorporate an appropriate amount of modulation and openings for medium density design of this scale.

#### **Built form**

*Good design achieves an appropriate built form for a site and the buildings purpose, in terms of building alignments, proportions, building type and the manipulation of building elements.*

With significant visual interest to the facades with a sound response to a corner site on a prominent corner, the proposal has the potential to positively add to the precinct and provide a new precedent to influence future development in the area. The raised corner and views have been used to potential in the building through the provision and extent of the balconies to the apartments. Negative impact of the ground floor car park has been partly limited through screening and a continuous façade/wall.

#### **Density**

*Good design has a density appropriate for a site and its context, in terms of dwelling yields (or number of units or residents).*

Proposed density is appropriate for the site area provisions and desired future density along urban corridors. Noted to be at odds with current context as there is no existing local precedent for this desired scale of development, yet.

#### **Resource, Energy & Water efficiency**

*Good design makes efficient use of natural resources, energy and waste throughout its full life cycle, including construction.*

Little to no information provided generally. Ideal solar orientation for North facing apartments though these are limited to 3 out of 8 per floor; apartments 4, 5 & 6 will not receive any direct sunlight. Internal layout is not conducive to allowing cross breezes. No information on water harvesting/reuse, heating cooling, hot water heating or energy generation.

**Landscape**

*Good design recognises that together landscape and buildings operate as an integrated and sustainable system, resulting in greater aesthetic quality and amenity for both occupants and the adjoining public domain.*

Minor consideration given to entry area around corner, limited buffer zone, no apparent treatment to plantings, though noted that site density and footprint has been maximised.

No mention on paving surface, eg bitumen vs water permeable pavers.

**Amenity**

*Good design provides amenity through the physical, spatial and environmental quality of a development.*

Potential privacy issues from units facing south addressed with 1500mm high screening, though noted that some of the existing neighbouring properties are already two storey medium density dwellings. Many bedrooms have limited access to natural daylight via borrowed light internally from the living space. Balconies are of satisfactory size. No external storage area for apartments noted.

Unsure of whether the car park will comply with National Construction Code requirements for minimum 50% open space on two sides for natural ventilation given the layout and roof over some of the car park. Apartment allocated for disabled use is one of the smaller/tighter layouts.

**Safety and security**

*Good design optimises safety and security, both internal to the development and for the public domain.*

Passive street surveillance aided by presence of balconies and activity/entrance at corner with minimised areas of limited visibility/hiding places, though no information provided about site lighting.

Angled driveway entrances off main roads may encourage higher speed vehicle entry and exit over footpaths.

**Social Dimensions**

*Good design responds to the social context and needs of the local community in terms of lifestyles, affordability and access to the social facilities.*

2 bedroom apartments throughout proposed development are a sought deviation from the local standard detached single housing, though limited demographic mix available within the building; no one or three bedroom apartments. Disabled compliant apartment commendable if more than current National Construction Code (NCC) requirements, though may not be ideal unit(s) in building to allocate.

**Aesthetics**

*Quality aesthetics require the appropriate composition of building elements, textures, materials and colours and reflect the use, internal design and structure of the development.*

The modern design with appropriate material employment is appropriate for this type of development with the design language appearing to be fairly coherent around the entire building and setting a good example for future development in the area. Elevations provide some idea of appearance though this would be helped by additional inclusion of 3d perspectives including the neighbouring buildings and streetscape from the intersection.

In reply please quote 2015/00161/01, Process ID: 330302  
 Enquiries to Reece Loughron  
 Telephone (08) 8226 8386  
 Facsimile (08) 8226 8330  
 E-mail dpti.luc@sa.gov.au



**Government of South Australia**  
 Department of Planning,  
 Transport and Infrastructure

**SAFETY AND SERVICE  
 DIVISION**

GPO Box 1533  
 Adelaide SA 5001

Telephone: 61 8 8226 8222  
 Facsimile: 61 8 8226 8330

13/04/2015

Mr Scott McLuskey  
 City of Prospect  
 PO Box 171  
 PROSPECT SA 5082

Dear Scott,

#### SCHEDULE 8 - REFERRAL RESPONSE

<b>Development No.</b>	050/345/14 – Amended Plans
<b>Applicant</b>	Vartzokas Architects Pty Ltd
<b>Location</b>	Lot 86 in FP 109951, 227-229 Prospect Road (cnr Regency Road), Prospect
<b>Proposal</b>	4 storey residential flat building (containing 24 dwellings) and car parking

I refer to the above development application forwarded to the Safety and Service Division of the Department of Planning, Transport and Infrastructure (DPTI) in accordance with Section 37 of the *Development Act 1993*. The proposed development involves development adjacent a main road as described above.

The following response is provided in accordance with Section 37(4)(b) of the *Development Act 1993* and Schedule 8 of the *Development Regulations 2008*.

#### THE PROPOSAL

The development involves the demolition of all existing structures and the construction of a four storey residential building comprising 24 dwellings. Vehicular access is proposed via both Regency Road and Prospect Road.

#### CONSIDERATION

##### Road Widening

The Metropolitan Adelaide Road Widening Plan shows a possible requirement for a strip of land up to 4.5 metres in width from the Prospect Road and Regency Road frontages of this site for the future upgrading of the Prospect Road / Regency Road intersection, together with a corner cut-off. The consent of the Commissioner of Highways under the Metropolitan Adelaide Road Widening Plan Act is required to all building works on or within 6.0 metres of the possible requirement. Current planning indicates that land from the site may be required for the future upgrade of the Prospect Road / Regency Road intersection; however this upgrade is presently unfunded. As such, DPTI will be enforcing the road widening requirement.

DPTI has reviewed the Floor Plans and Elevations provided by Vartzokas Architects Pty Ltd (refer Sheet 1 Floor Plans, Job No. A213-464, Drawing No. PDA – 02, Plotted 25/2/15, dated FEB'15 and Sheet 2 Elevations Plans Job No. N214-473, /DOCS AND FILES# 9342007

Drawing No. PDA – 02, Plotted 26/2/15, dated FEB'15). It is noted that the majority of building works are located outside of this area, however it appears that the awnings adjacent to the proposed main entry extend into the 4.5 metre strip requirements. In this instance DPTI is prepared to allow the encroachment provided that the cantilevered awnings are designed to be removable so that should DPTI infrastructure need to be installed, erected and/or maintained within this area the awnings can be easily removed or modified at that time at no cost to this department.

It is pointed out that the 4.5 metre strip of land should be set aside for road purposes at the time a land division application is lodged.

### **Access & Car Parking**

DPTI has been in consultation with the applicant in regards to the preferred access location and design in regards to minimising impacts to the adjacent signalised intersection. DPTI recommended a one-way flow through the site (from Regency to Prospect Road) with all movements being undertaken on a left turn in and left turn out basis only, with the access points being located as far as practicable from the signalised intersection. DPTI is pleased that the applicant has been able to amend the design to incorporate these requirements.

To further reinforce the desired flow through the site the Regency Road access point should be flared on the entry side only and the Prospect Road access point should be flared on the departure side only.

#### Regency Road Ingress

In regards to the proposed ingress, while the location is supported, the final design should be amended so that the roller door is set back further into the site in order to maximise vehicular storage clear of the future carriageway (after road widening). Additionally, the roller door should be increased in width to ensure convenient entry movements into the closest parks (25 and 26). It is noted that setting the roller door further back may require the scooter parking to be relocated further into the bay adjacent the eastern wall.

#### Prospect Road Egress

The design of the egress to Prospect Road should ensure that AS/NZS 2890.1:2004 Figure 3.3 Minimum Sight Lines For Pedestrian Safety is incorporated into the design. This may also require the roller door to be widened and relocated in order to fit the revised building design.

#### Parking

In regards to the car park design, all access points should be clearly signed and line marked to reinforce the desired traffic flow through the site. DPTI is satisfied that the proposed development is providing sufficient car parking at rates described in Council's development plan for the Urban Corridor and other parking guidelines.

Additionally, all access points and manoeuvring areas should be designated as common property or rights of way at the land division stage.

#### Refuse Collection

DPTI notes that a dedicated storage area for bins will be provided on site. It is strongly recommended that Council ensure that a refuse vehicle can service this area internally without the need for individual tenants to have refuse bins collected

from the footpath. Council should also ensure that if a refuse vehicle will service this storage area that the vehicle will not impede internal traffic flows which may result in anti-directional movements and thus result in vehicular conflict.

#### **Pedestrian Access**

Council should ensure that suitable pedestrian facilities are included in the design of the Regency Road and Prospect Road access points.

#### **Illuminated signage**

DPTI appreciates that no signage requirements have been established for the proposed development. However given the proximity of the adjacent signalised intersection any future signage should be designed in accordance with DPTI "Advertising Signs - Assessment Guidelines for Road Safety" (August 2014) to ensure road safety is maximised along the adjacent section of Regency Road and Prospect Road.

### **CONCLUSION**

In-principle, no objections are raised subject to the following conditions being attached to any approval.

### **ADVICE**

The planning authority is directed to attach the following conditions to any approval:

1. All building works and the at grade car park shall be set back clear of the 4.5 metres road widening requirements along the Prospect Road and Regency Road frontages of the site.
2. The cantilevered awnings located adjacent to the proposed main entry that extend into the 4.5 metre strip requirements shall be designed to be removable so that they can be removed or modified should DPTI need to install, erect and/or maintain infrastructure in this area. On the direction of the Commissioner of Highways, the awnings shall be removed or modified at no cost to the department.

The planning authority is advised to attach the following conditions to any approval:

1. The access points to Regency Road and Prospect Road shall be constructed in accordance with Vartzokas Architects Pty Ltd, Sheet 1 Floor Plans, Job No. A213-464, Drawing No. PDA – 02, Plotted 25/2/15, dated FEB'15.
2. All access shall be gained via a one-way flow through the site with ingress movements (left in only) via Regency Road and egress movements (left out only) via Prospect Road only.
3. The access point/s shall be angled at 70 degrees to the kerb line and be appropriately signed and line marked in order to reinforce the desired traffic flow.
4. The ingress roller door shall be setback to maximise vehicular storage clear of the road widening strip.
5. The egress roller door shall be designed in accordance with AS/NZS2890.1:2004 Figure 3.3 Minimum Sight Lines For Pedestrian Safety.
6. All vehicles shall enter and exit the site in a forward direction.

7. Refuse collection shall be undertaken within private property and ensure vehicular movements through the car park are not impeded during collection times.
8. The Regency Road and Prospect Road crossovers shall provide a minimum of 1.0 metre separation from all roadside infrastructure.
9. Any portions of redundant crossover shall be closed and reinstated to Council's satisfaction at the applicant's cost prior to the habitation of any dwellings.
10. All bicycle facilities shall be designed in accordance with AS2890.3:1993 - Bicycle parking facilities.
11. Stormwater run-off shall be collected on-site and discharged without jeopardising the integrity and safety of Regency and Prospect Roads. Any alterations to the road drainage infrastructure required to facilitate this shall be at the applicant's cost.

The following notes provide important information for the benefit of the applicant and are required to be included in any approval:

- i. The Metropolitan Adelaide Road Widening Plan shows a possible requirement for a strip of land up to 4.5 metres in width from the Prospect Road and Regency Road frontages of this site for the future upgrading of the Prospect Road / Regency Road intersection, together with a corner cut-off. The consent of the Commissioner of Highways under the Metropolitan Adelaide Road Widening Plan Act is required to all building works on or within 6.0 metres of the possible requirement.

Subsequently, should Council approve the application in its current form, the applicant should fill out the attached consent form and return it to DPTI with 3 copies of the approved plans.

- ii. Any future signage applications should be designed in accordance with DPTI "Advertising Signs - Assessment Guidelines for Road Safety" (August 2014). The document is available via the following link:

[http://www.dpti.sa.gov.au/\\_data/assets/pdf\\_file/0019/145333/DPTI-Advertising-Signs-Assessment-Guidelines.pdf](http://www.dpti.sa.gov.au/_data/assets/pdf_file/0019/145333/DPTI-Advertising-Signs-Assessment-Guidelines.pdf)

Yours sincerely,

A/GENERAL MANAGER, OPERATIONAL SERVICES  
For COMMISSIONER OF HIGHWAYS

**Encl:** Copy of the MARWP consent form.

A copy of the decision notification form should be forwarded to [dpti.developmentapplications@sa.gov.au](mailto:dpti.developmentapplications@sa.gov.au)

**METROPOLITAN ADELAIDE ROAD WIDENING PLAN ACT, 1972****APPLICATION FOR CONSENT OF THE COMMISSIONER OF HIGHWAYS  
FOR BUILDING WORK**

Government of South Australia  
Department of Planning,  
Transport and Infrastructure

Owner's Name \_\_\_\_\_

Applicant's Name \_\_\_\_\_  
(If not owner)

Relationship to Owner (Lessee, Prospective Purchaser, Etc.) \_\_\_\_\_

Postal Address \_\_\_\_\_

Postcode \_\_\_\_\_

**Location of Proposed Building Works (Full Details Required):**

L.T.O. Plan No.: \_\_\_\_\_ Allotment No.: \_\_\_\_\_ Section: \_\_\_\_\_

Hundred: \_\_\_\_\_ House No.: \_\_\_\_\_

Street Name \_\_\_\_\_ Suburb: \_\_\_\_\_

Certificate of Title \_\_\_\_\_

Volume \_\_\_\_\_ Folio \_\_\_\_\_

Nature of Proposed Building Work \_\_\_\_\_

**Proposed Location of Building Work on Land:**

Distance from existing front road boundary to nearest point of building \_\_\_\_\_

Distance from existing side boundary (if on corner) to nearest point of building \_\_\_\_\_

Total estimated cost of proposed building work \$ \_\_\_\_\_

Estimated cost of proposed building work on land to which the Act applies \$ \_\_\_\_\_

**Signature(s):** \_\_\_\_\_ **Date** \_\_\_\_\_  
(Applicant) (Owner or Authorised Agent)

**Notes:**

1. *Three (3) copies of the approved site plan describing fully the nature and extent of the proposal must be forwarded with this application form together with one copy of the notification of the relevant authority's decision to approve the development under the Development Act 1993.*
2. *The Act requires that the consent of the Commissioner of Highways must be obtained for any building work on land to which the Act applies.*
3. *The Act applies to all land shown on the Metropolitan Adelaide Road Widening Plan as possibly required for road widening and all land within six metres of the boundary of that land.*

PLEASE RETURN TO:

COMMISSIONER OF HIGHWAYS  
DEPARTMENT OF PLANNING, TRANSPORT AND INFRASTRUCTURE  
GPO BOX 1533  
ADELAIDE SA 5001

For further information regarding the Metropolitan Adelaide Road Widening Plan Act,  
please contact DPTI on (08) 8343 2303 or (08) 8343 2306.

In reply please quote 2014/01922/01, Process ID: 302755  
 Enquiries to Reece Loughron  
 Telephone (08) 8343 2569  
 Facsimile (08) 8343 2725  
 E-mail dpti.luc@sa.gov.au

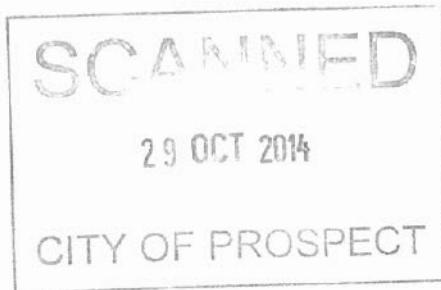


**Government of South Australia**  
 Department of Planning,  
 Transport and Infrastructure

27/10/2014

Mr Scott McLuskey  
 City of Prospect  
 PO Box 171  
 PROSPECT SA 5082

Dear Scott,



**SAFETY AND SERVICE  
 DIVISION**

77 Grenfell Street  
 Adelaide SA 5001

GPO Box 1533  
 Adelaide SA 5001

Telephone: 61 8 8343 2222  
 Facsimile: 61 8 8343 2585

### SCHEDULE 8 - REFERRAL RESPONSE

<b>Development No.</b>	050/345/14
<b>Applicant</b>	Vartzokas Architects
<b>Location</b>	Lot 86 in FP 109951, 227-229 Prospect Road (cnr Regency Road), Prospect
<b>Proposal</b>	Four storey residential flat building containing 30 dwellings.

I refer to the above development application forwarded to the Safety & Service Division of the Department of Planning, Transport and Infrastructure (DPTI) in accordance with Section 37 of the *Development Act 1993*. The proposed development involves development adjacent a main road as described above.

The following response is provided in accordance with Section 37(4)(b) of the *Development Act 1993* and Schedule 8 of the *Development Regulations 2008*.

### THE PROPOSAL

The development involves the demolition of all existing structures and the construction of a four storey residential building comprising 30 dwellings. Vehicular access is proposed via both Regency Road and Prospect Road.

### CONSIDERATION

#### Road Widening

The Metropolitan Adelaide Road Widening Plan shows a possible requirement for a strip of land up to 4.5 metres in width from the Prospect Road and Regency Road frontages of this site for the future upgrading of the Prospect Road / Regency Road intersection, together with a corner cut-off. The consent of the Commissioner of Highways under the Metropolitan Adelaide Road Widening Plan Act is required to all building works on or within 6.0 metres of the possible requirement.

Planning indicates that land from the site may be required for the future upgrade of the Prospect Road / Regency Road intersection; however this upgrade is currently unfunded. As such, DPTI will be maintaining the road widening requirement and directs that the proposed development be set outside of the 4.5 metres strip

requirement. The road widening strip requirement must be clear of all building works including all car parking and balconies.

DPTI is willing to assist the applicant with reviewing any amended proposal and establishing a building and car park footprint that is consistent with the future intersection upgrade requirements for this site.

### Road Safety

The site abuts two arterial roads, with Regency Road carrying approximately 27,700 vehicles and Prospect Road carrying approximately 16,100 vehicles per day. Regency Road has a 60km/h speed limit and Prospect Road has a 50km/h speed limit.

The proposed development intends to retain access to both roads with slight relocation and widening of the access points closer to the signalised intersection. Whilst DPTI is supportive of the one-way flow through the site (ingress from Regency Road and egress to Prospect Road), the relocation of accesses closer to the traffic signals is not supported. Any amended plans must show the access points located adjacent the eastern and southern property boundaries, with reduced widths to encourage one-way movements.

The car park layout should also be designed to reinforce a one-way flow through the site and in accordance with AS/NZS 2890.1:2004 and 2890.6:2009.

### CONCLUSION

DPTI does not support the proposed development in its current form. Amended plans showing:

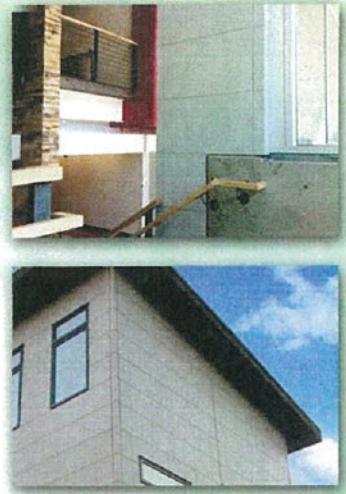
1. All building works and at grade car parking set back clear of the 4.5 metre road widening strip requirements along the Prospect Road and Regency Road frontages of the site;
2. All access gained via a one-way flow through the site with ingress movements via Regency Road and egress movements via Prospect Road only;
3. The access points reduced in width to encourage one-way movements and located adjacent the eastern and southern property boundaries in order to minimise impacts to the operation of the adjacent intersection; and
4. All car parking at the ingress located 6.0 metres clear of the 4.5 metre strip requirement in order to provide sufficient area for vehicles to store clear of the road once the road widening strip is used for future road upgrades;

Should be provided to Council and thence DPTI for further review and comment. Should amended plans not be forthcoming, DPTI directs Council to refuse the application.

Yours sincerely,

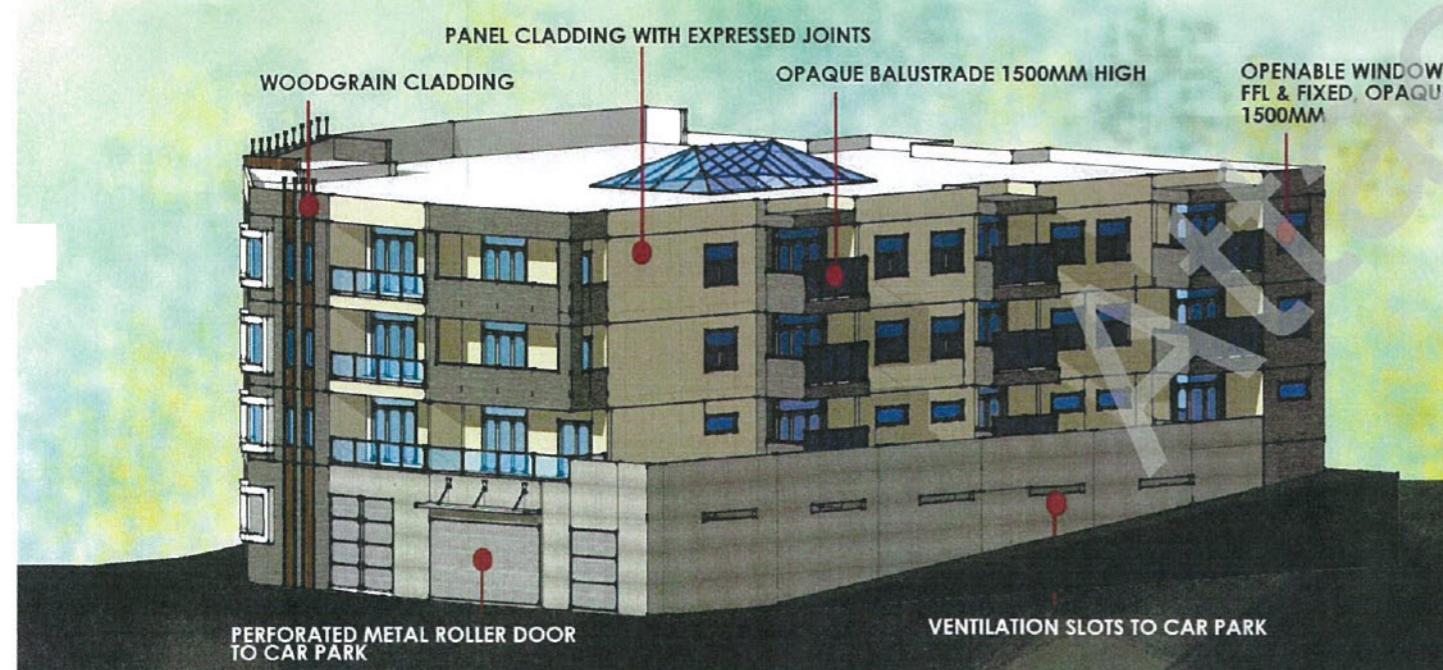
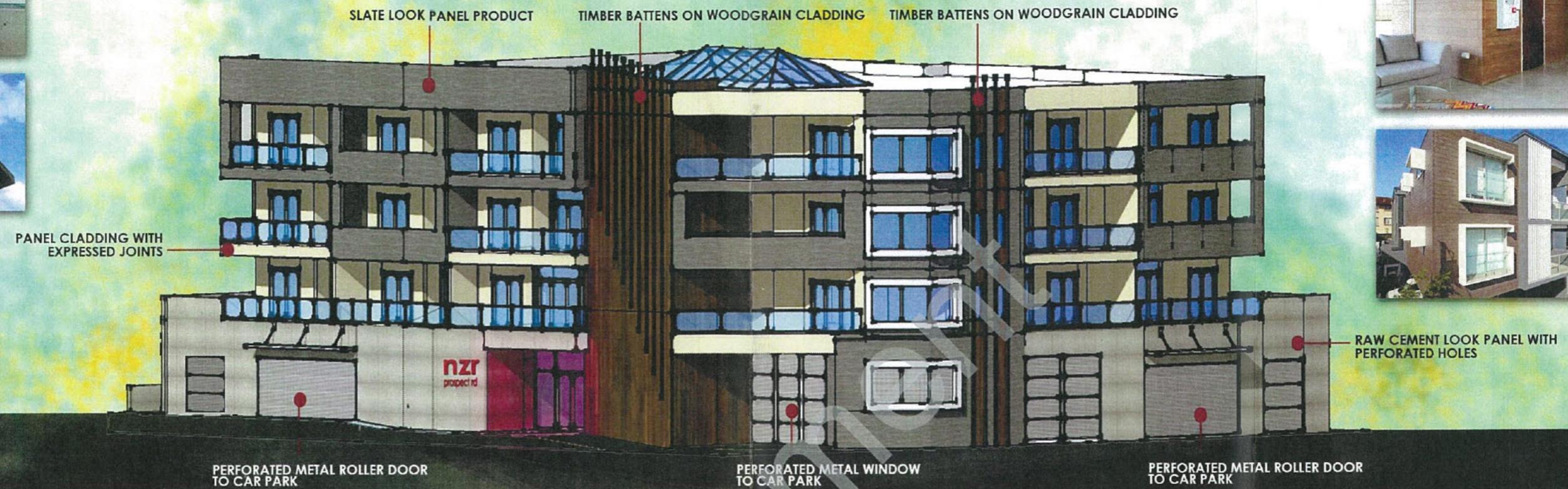


for  
**MANAGER, TRAFFIC AND ACCESS STANDARDS**  
**for COMMISSIONER OF HIGHWAYS**



RAW CEMENT LOOK FOR BASE OF BUILDING

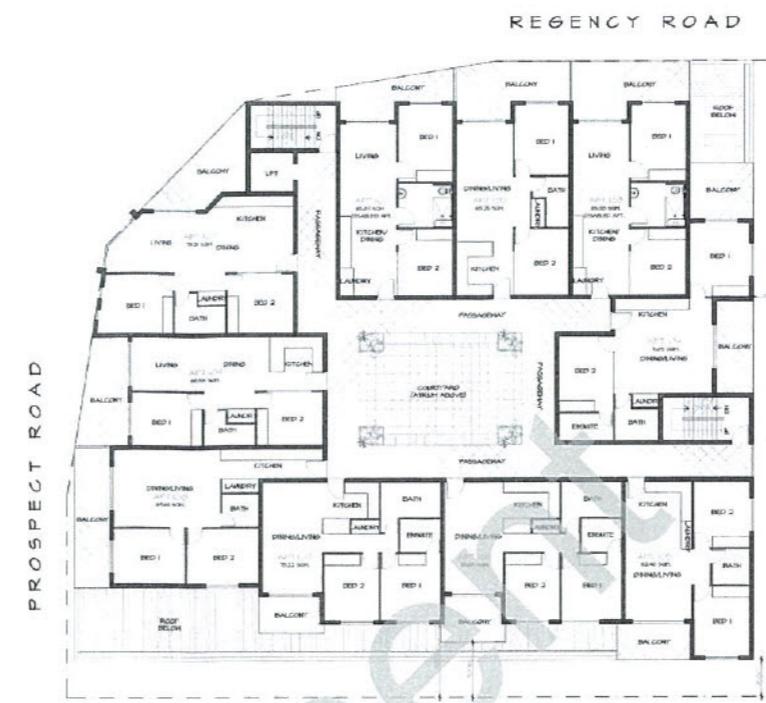
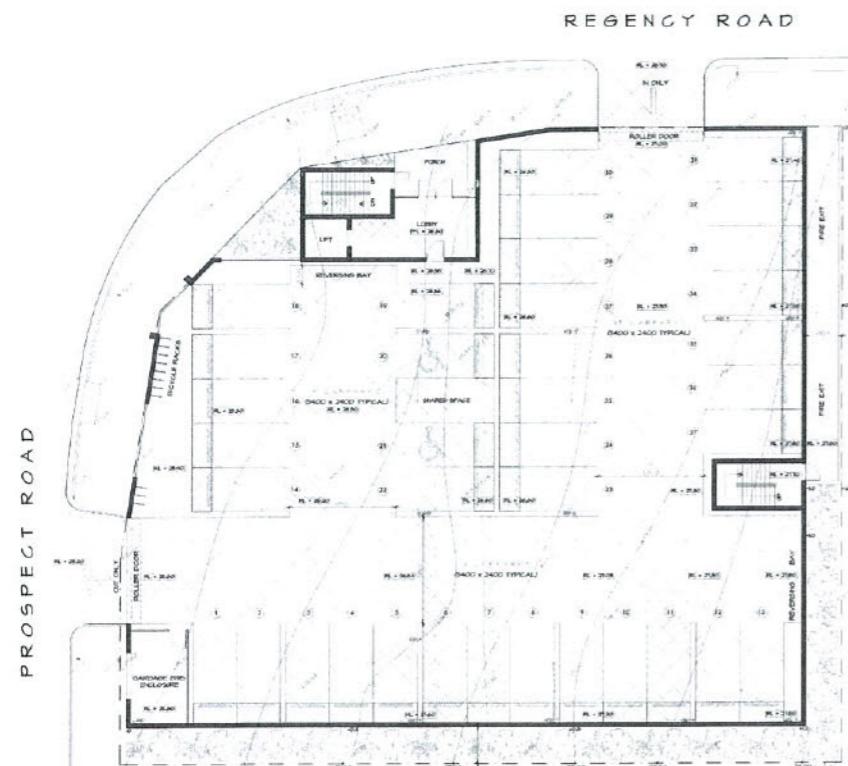
WOODGRAIN CLADDING AS FEATURE MATERIAL



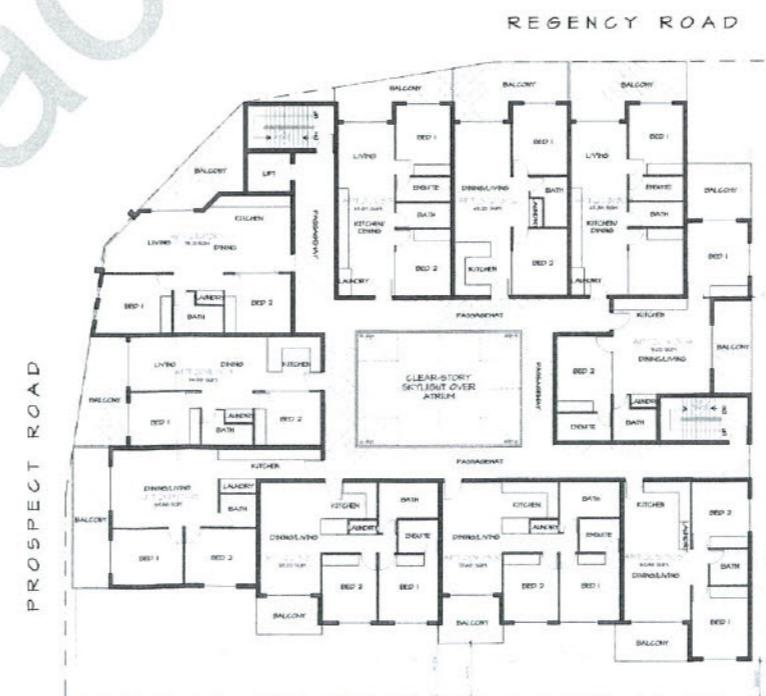
SLATE LOOK PANEL TO BALCONY BOX OUT

CNR PROSPECT AND REGENCY ROAD, PROSPECT NAZERO APARTMENTS  
PROPOSED RESIDENTIAL DEVELOPMENT

SUPERSEDED

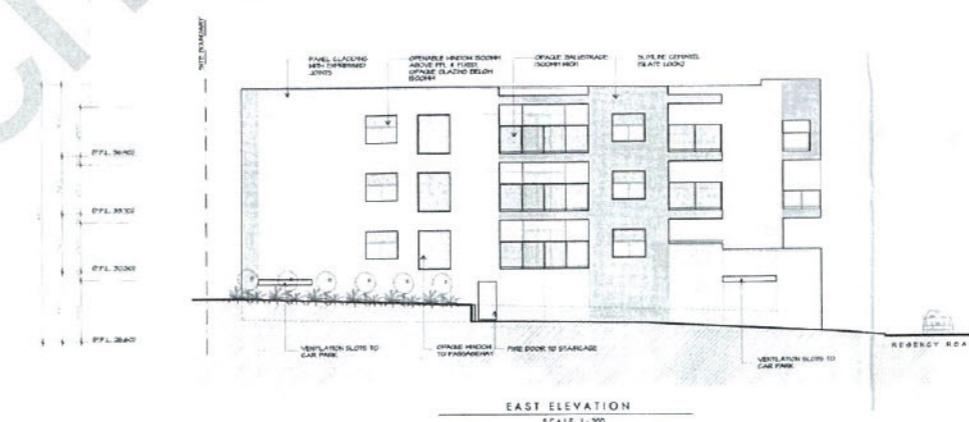
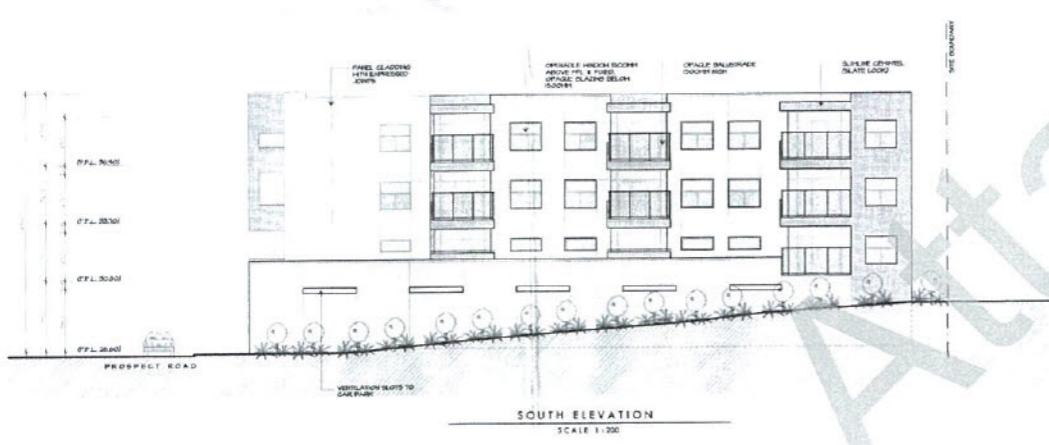
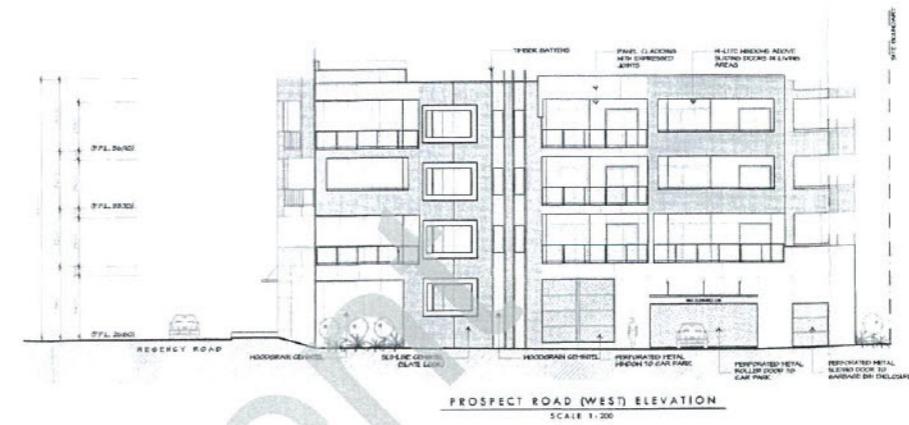
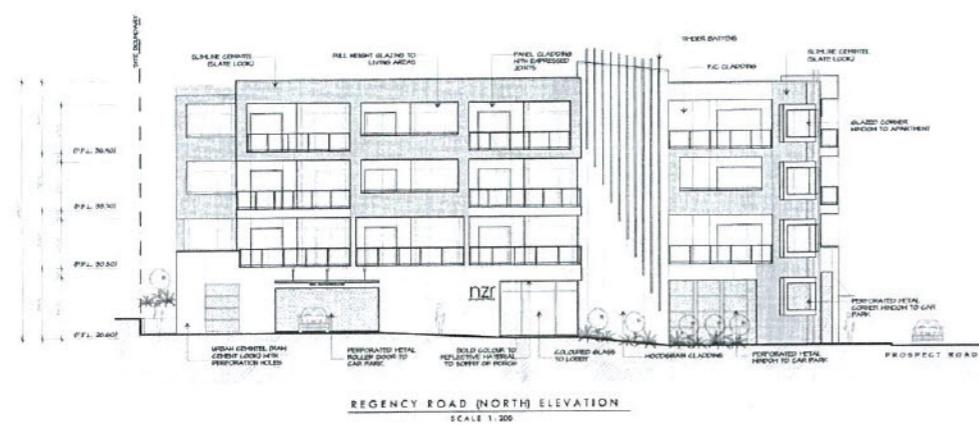


AREA SUMMARY	
GROUND FLOOR	82.46 SQ.M
LOBBIES	45.80 SQ.M
STAIRCASE	25.87 SQ.M
PORCH	10.79 SQ.M
<b>FIRST FLOOR RESIDENTIAL</b>	<b>1,022.91 SQ.M</b>
LIVING AREA	688.36 SQ.M
CIRCULATION SPACE & COURTYARD	201.18 SQ.M
BALCONIES	133.37 SQ.M
<b>SECOND FLOOR RESIDENTIAL</b>	<b>968.47 SQ.M</b>
LIVING AREA	688.36 SQ.M
CIRCULATION SPACE	146.74 SQ.M
BALCONIES	133.37 SQ.M
<b>THIRD FLOOR RESIDENTIAL</b>	<b>968.47 SQ.M</b>
LIVING AREA	688.36 SQ.M
CIRCULATION SPACE	146.74 SQ.M
BALCONIES	133.37 SQ.M
<b>TOTAL BUILDING AREA</b>	<b>3,042.31 SQ.M</b>
CARPARK AREA	1,039.88 SQ.M
LANDSCAPE AREA	106.24 SQ.M
EXTERNAL FIRE ESCAPE	38.36 SQ.M
<b>TOTAL SITE AREA</b>	<b>1,266.94 SQ.M</b>
SUMMARY	
<b>TOTAL ON-SITE CARPARKS</b>	<b>37</b>
TOTAL 2 BEDROOM 2 BATH APARTMENTS	15
TOTAL 2 BEDROOM 1 BATH APARTMENTS	15
TOTAL 2 VISITORS CAR PARK	7
BICYCLE RACKS	10
DISABLED CARPARKS NEEDED	2



SUPERSEDED

Issue	Date	Revision
VARTZOKAS ARCHITECTS PTY LTD		
Level 1/216 Glen Osmond Road Fullerton SA 5063 Phone: 08 8379 9833 Fax: 08 8379 9811 e-mail: osman@vartzokasarchitects.com.au		
Project PROPOSED APARTMENT DEVELOPMENT		
Address Cnr PROSPECT & REGENCY ROAD, PROSPECT		
Client ANAND INVESTMENTS PTY LTD.		
Title FLOOR PLANS		
Drawn: PY	Checked: TV	Scale: 1:200
Date: SEPT'14	Plotted: 03-09-14	Sheet No: 1 of 2
Job No: A213-464	Drg No:	PDA 1/1
Verify all dimensions and set-outs prior to commencing work. Figure dimensions take preference. Do not scale drawings.		
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**SUPERSEDED**

Issue	Date	Revision

**VARTZOKAS ARCHITECTS PTY LTD**  
 Level 1/216 Glen Osmond Road  
 Fullerton SA 5065  
 Phone: 08 8379 9903  
 Fax: 08 8379 9911  
 e-mail: admin@vartzokasarchitects.com.au

Project: PROPOSED APARTMENT DEVELOPMENT

Address: Cnr PROSPECT & REGENCY ROAD, PROSPECT

Client: ANAND INVESTMENTS PTY LTD.

Title: ELEVATIONS

Drawn: PY	Checked: TV	Scale: 1:200
Date: SEPT'14	Plotted: 03-09-14	Sheet No: 2 of 2

Job No: N214-473 | Dra No: PDA 1/2

Verify all dimensions and set-outs prior to commencing work.  
 Figure dimensions take preference. Do not scale drawings.

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# STORMWATER DRAWINGS

## FOR

# 227 PROSPECT ROAD

# PROSPECT SA 5082

**SYMBOLS**

FFL	FINISHED FLOOR LEVEL
F.G.L.	FINISHED GARAGE LEVEL
T.K.	TOP OF KERB
* 11.0	FINISHED LEVEL
SL	PIT SURFACE LEVEL
IL	INVERT LEVEL
20 R	ROOF CATCHMENT AREA ( $m^2$ )
20 I	IMPERVIOUS CATCHMENT AREA ( $m^2$ )
20 L	LANDSCAPED CATCHMENT AREA ( $m^2$ )
-----	STORMWATER DRAINAGE PIPE
— RWT —	DOWNPipe TO RAINWATER TANK
-----	OVERFLOW PIPE FROM RAINWATER TANK
— 450 PUMP LINE	450 PUMP LINE
-----	100 SUBSOIL PIPE
----- ES -----	EMERGENCY SPITTER PIPE
• DP	DOWN PIPE
• SP	SPREADER
• IO	INSPECTION OPENING
• VD	VERTICAL DROP
• VR	VERTICAL RISER
----- MW -----	MASONRY RETAINING WALL
TW 81.20	TOP OF WALL LEVEL
□	CONCRETE COVER JUNCTION PIT
OFW	FLOOR WASTE 150mm
■■■■■	GRATED INLET PIT
■■■■■	INDE GRATED DRAIN
←	OVERLAND FLOW PATH
ES	EMERGENCY SPITTER
RWH	RAIN WATER HEAD
• CO	CLEAN OUT
• OF	EMERGENCY OVERFLOW FLOOR WASTE 100mm
• RWD	RAINWATER OUTLET 300mm

**NOTE RE: SERVICES**  
APPROXIMATE LOCATIONS OF  
EXISTING SERVICES SHOWN  
ON LONGITUDINAL SECTION.  
EXACT LOCATIONS & DEPTHS  
TO BE ACCURATELY LOCATED BY  
BUILDER CONTRACTOR BY CONTACTING  
THE RELEVANT AUTHORITIES BEFORE  
COMMENCEMENT OF ANY WORKS



- NOTES**
- ALL LINES ARE TO BE MIN. 100mm UPVC @ MIN 1.0% GRADE UNLESS NOTED OTHERWISE.
  - IT IS THE CONTRACTORS RESPONSIBILITY TO LOCATE & LEVEL ALL EXISTING SERVICES PRIOR TO THE COMMENCEMENT OF ANY EARTHWORKS. ALL DESIGN LEVELS SHOWN ON PLANS WILL BE VERIFIED ON SITE PRIOR TO THE COMMENCEMENT OF ANY WORK.
  - ALL PIPES TO HAVE MIN 200mm COVER IF LOCATED WITHIN PROPERTY.
  - ALL PITS IN DRIVEWAYS BE HEAVY DUTY GRATES. DIRECTION OF SURFACE FLOW TO ALL GRATED SURFACE INLET PITS.
  - ALL WORK TO BE DONE IN ACCORDANCE WITH AS/NZS 3500.3.2:1998 AND COUNCIL SPECIFICATIONS.
  - LOCATION OF DOWNPipes & FLOOR WASTES ARE INDICATIVE ONLY. DOWNPipe & FLOOR WASTE SIZE, LOCATION & QUANTITY TO BE DETERMINED BY BUILDER & IN ACCORDANCE WITH RELEVANT AUSTRALIAN STANDARDS.
  - THIS PLAN IS TO BE READ IN CONJUNCTION WITH THE ARCHITECTURAL, LANDSCAPE AND STRUCTURAL PLANS.
  - ANY DISCREPANCIES OR OMISSIONS SHALL BE REFERRED TO THE DESIGN ENGINEER FOR RESOLUTION.
  - ALL PITS OR GRATES IN TRAFFICABLE AREAS TO BE HEAVY DUTY.
  - ALL CUTTERS WILL BE FITTED WITH LEAF GUARDS AND SHOULD BE INSPECTED AND CLEANED TO ENSURE LEAF LITTER CANNOT ENTER THE DOWNPipes.
  - PROVIDE EMERGENCY OVERFLOW TO ALL PLANTER BOX AND BALCONIES.
  - ALL PITS WITH DEPTH MORE THAN 1M MUST HAVE IRON STEPS.
  - PROVIDE STORMWATER GRATE 200mmx200mm AT THE BASE OF ALL MECHANICAL SHAFTS AND UNCOVERED STAIRS OR OPENINGS.
  - ENSURE ALL DRAINAGE WORKS ARE AWAY FROM TREE ROOTS

**LEGEND**

DP : 100mm DOWN PIPE  
 ----- : STORMWATER PIPE  
 0% M. U.N. : 0% M. U.N.  
 REFER TO AS 3500 PART 1 & E.7.2  
 P1 : 100mm UPVC PIPE AT 1.0% MIN. GRADE  
 P2 : 150mm UPVC PIPE AT 1.0% MIN. GRADE  
 P3 : 225mm UPVC PIPE AT 0.5% MIN. GRADE  
 P4 : 300mm UPVC PIPE AT 0.4% MIN. GRADE  
 P5 : 375mm UPVC PIPE AT 0.4% MIN. GRADE  
 P6 : 450mm CP PVC AT 0.4% MIN. GRADE

DEPTH OF INVERT OF OUTLET	MINIMUM INTERNAL DIMENSIONS (mm)	
	RECTANGULAR	CIRCULAR
≤600	450	450
600 ≤ 900	600	600
900 ≤ 1200	600	900
≥1200	900	1000

\*RL 22.20 NEW LEVEL

EXISTING LEVEL

**NOTE: RETAINING WALLS & 'AGG' LINES**  
ALL RETAINING WALLS ARE TO BE WATERPROOFED AND  
CONSTRUCTED WITH ≥100mm AGRICULTURAL LINES AT  
THE BASE AND CONNECTED TO THE NEAREST PIT IN THE COURTYARD.

**DRAWING SCHEDULE**

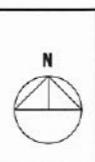
DRAWING No.	DRAWING TITLE
D00	COVER SHEET, LEGEND & DRAWING SCHEDULE
D01	EROSION AND SEDIMENT CONTROL PLAN & DETAILS
D02	GROUND FLOOR STORMWATER DRAINAGE PLAN
D03	STORMWATER DRAINAGE DETAILS & SECTIONS

A1	0	1	2	3	4	5	6	7	8	9	10
FOR D.A. APPROVAL	ASH.	E.H.	26-08-14								
AMENDMENT	ENG.	DRAFT	DATE	No.	AMENDMENT	ENG.	DRAFT	DATE	No.	AMENDMENT	ENG.

ARCHITECT  
VARTZOKAS ARCHITECTS PTY LTD  
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Concord, NSW 2137  
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Email: nazero@optus.com.au

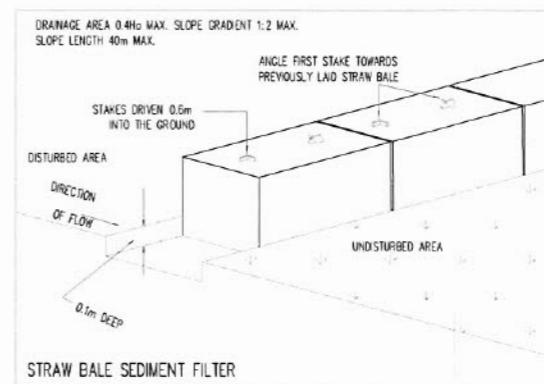
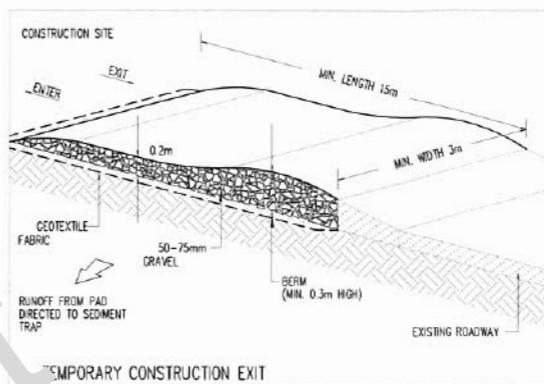
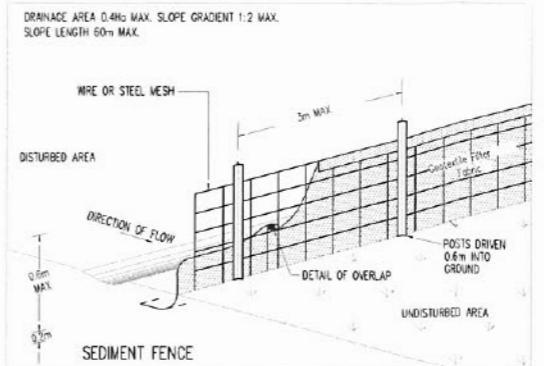
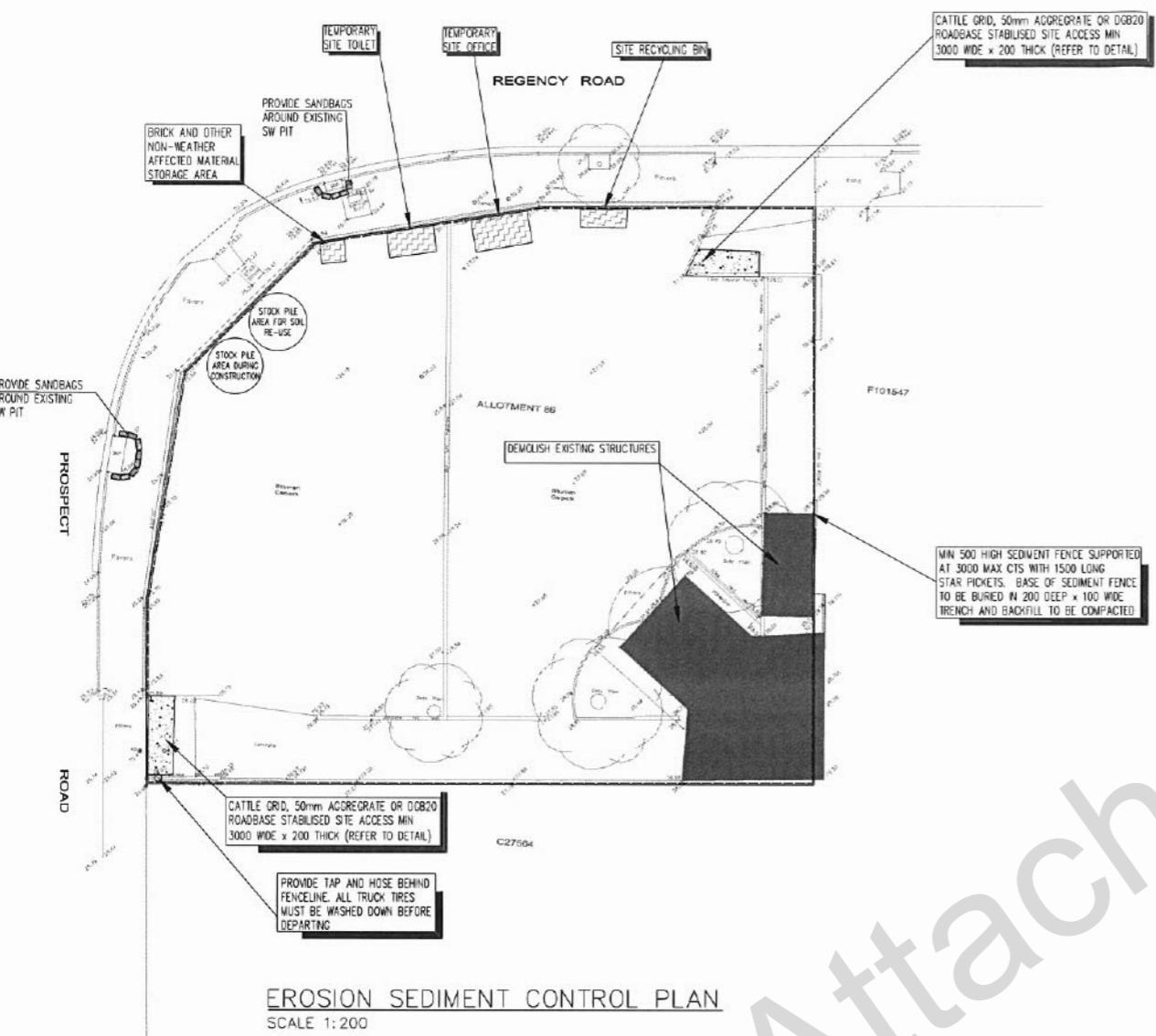
**AUSTRALIAN CONSULTING ENGINEERS.**  
PTY LTD ACN 081 659 941  
Shop 2-4-1 Concord Rd North Strathfield NSW 2137  
Ph (02) 9765 1500 Fx (02) 9765 1515  
Email: ace@acech.com.au

PROJECT 227 PROSPECT ROAD, PROSPECT, SA 5082  
RESIDENTIAL DEVELOPMENT  
227 PROSPECT ROAD  
PROSPECT SA 5082



SHEET SUBJECT  
COVER SHEET, LEGEND &  
DRAWING SCHEDULE

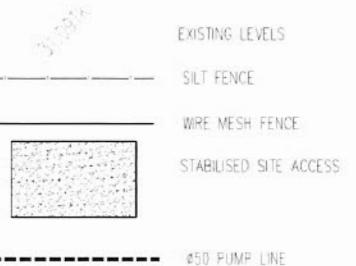
PROJECT 227 PROSPECT ROAD, PROSPECT, SA 5082  
DATE AUG 14 DRAWN E.H. DESIGNED A.S.H. CHECKED A.H.  
SCALE 1:200 AT NTS JOB No. 140666  
AUTORISED Dr. Anthony S. Hashem B.E. Ph.D. Date No. D00 Rev. A



## EROSION CONTROL NOTES

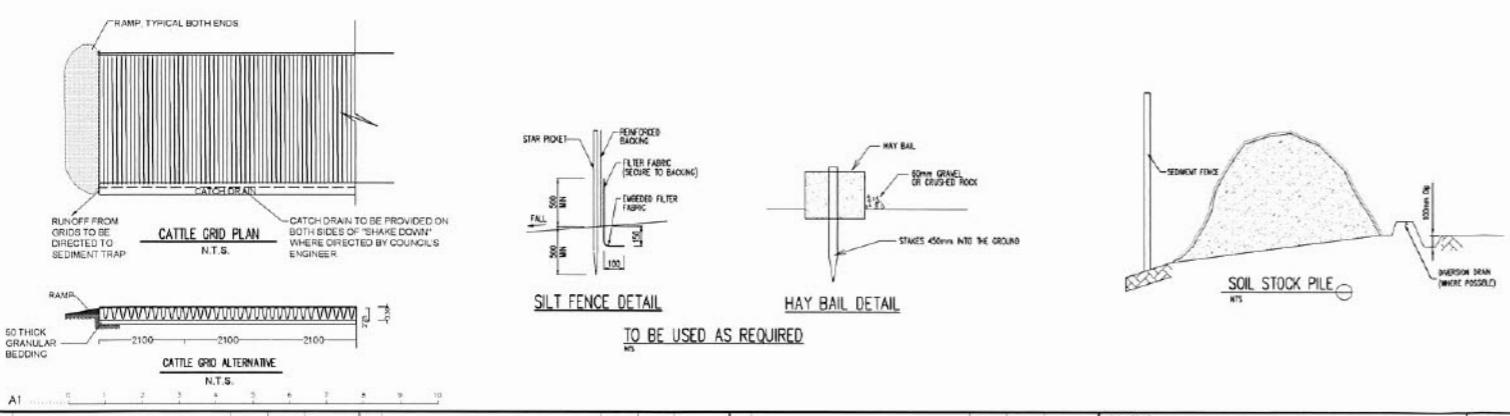
- ALL EROSION & SEDIMENT CONTROL MEASURES ARE TO BE INSTALLED AND MAINTAINED IN ACCORDANCE WITH 'MANAGING URBAN STORMWATER, 3rd EDITION' PRODUCED BY THE NSW DEPARTMENT OF HOUSING.
- ALL EROSION AND SILTATION CONTROL DEVICES ARE TO BE PLACED PRIOR TO THE COMMENCEMENT OF ANY CONSTRUCTION WORKS, AND ALL SILT TRAPS ARE TO HAVE DEPOSITED SILT REMOVED REGULARLY DURING CONSTRUCTION.
- ALL TREES ARE TO BE PRESERVED UNLESS INDICATED OTHERWISE ON THE ARCHITECT'S OR LANDSCAPE ARCHITECT'S DRAWINGS. EXISTING GRASS COVER SHALL BE MAINTAINED EXCEPT IN AREAS CLEARED FOR BUILDINGS, PAVEMENTS ETC.
- INSTALL TEMPORARY SEDIMENT BARRIERS TO ALL INLET PITS LIKELY TO COLLECT SILT LADDEN WATER.
- NOTWITHSTANDING DETAILS SHOWN IT IS THE CONTRACTORS SOLE RESPONSIBILITY TO ENSURE THAT ALL SITE ACTIVITIES COMPLY WITH THE REQUIREMENTS OF THE CLEAN WATERS ACT.

## SYMBOLS



## NOTES: SOIL & WATER MANAGEMENT

- ALL EROSION AND SEDIMENT CONTROL MEASURES TO BE INSPECTED AND MAINTAINED DAILY BY SITE MANAGER.
- MINIMISE DISTURBED AREAS.
- ALL STOCKPILES TO BE CLEAR FROM DRAINS, GUTTERS AND FOOTPATHS.
- DRAINAGE IS TO BE CONNECTED TO STORMWATER SYSTEM AS SOON AS POSSIBLE.
- ROADS AND FOOTPATH TO BE SWEEP DAILY.
- NO MATERIAL TO BE STORED ON FOOTPATH.



A FOR D.A. APPROVAL	A.S.H.	R.A.	26-08-14												
No	AMENDMENT	ENG	DRAFT	DATE	No	AMENDMENT	ENG	DRAFT	DATE	No	AMENDMENT	ENG	DRAFT	DATE	

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PROJECT  
RESIDENTIAL DEVELOPMENT  
227 PROSPECT ROAD  
PROSPECT SA 5082

N  
SHEET SUBJECT  
EROSION & SEDIMENT CONTROL PLAN

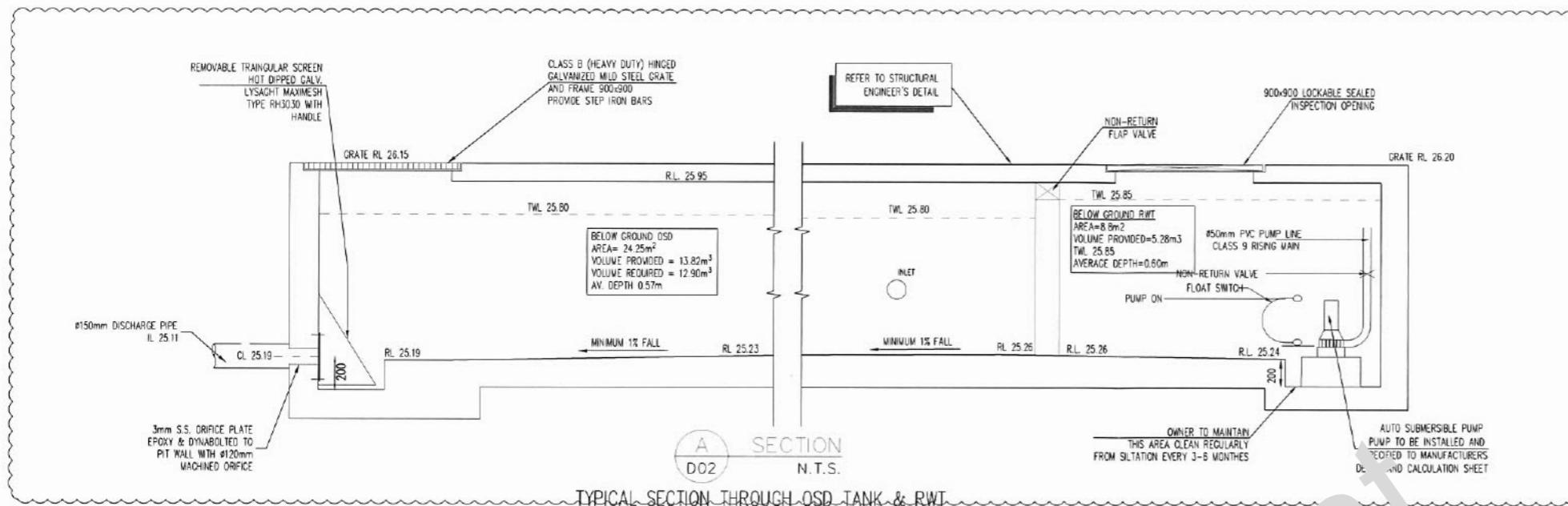
PROJECT 227 PROSPECT ROAD, PROSPECT SA 5082  
DATE AUGUST 14 DR 8.0% DESIGNED A.S.H. CHECKED A.S.H.  
SCALE 1:50 AS SHOWN JOB No 140666  
AUTHORIZED Dr. Anthony S. Hasham BE, Ph.D D01 REV A

## NOTES THIS DRAWING

- ALL DOCUMENTS WILL BE SUBMITTED TO COUNCIL FOR APPROVAL.
- ALL SEDIMENT CONTROL MEASURES ARE TO BE IN PLACE.
- INSTALLATION OF SILT FENCING, SEDIMENTATION BARRIERS AROUND DRAINS. FENCING IS TO BE 1.8m(min) HEIGHT.
- THE HARDSTAND AREAS OR CATTLE GRIDS WILL BE PLACED AT THE SITE ENTRANCE AND EXIT TO REMOVE THE BULK OF DIRT AND MUD THAT MAY ACCUMULATE ON TRUCK TYRES.
- CONTRACTOR WILL CONDUCT REGULAR STREET SWEEPS ALONG THE ACCESS ROUTE TO ENSURE THE ROADS ADJACENT TO THE SITE ENTRANCES ARE KEPT CLEAN OF ANY DIRT AND DEBRIS.
- REGULAR ENVIRONMENTAL INSPECTIONS WILL BE CARRIED OUT BY CONTRACTOR'S PERSONNEL TO ENSURE COMPLIANCE WITH THIS PLAN.



CONTRACTOR IS TO PROVIDE OVERFLOW OUTLETS  
TO ALL BALCONIES AND OTHER ENTRAPPED AREAS

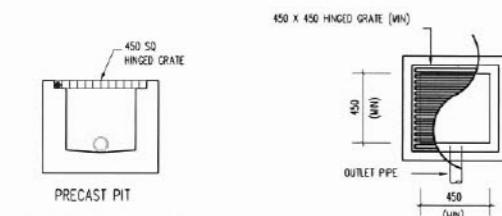
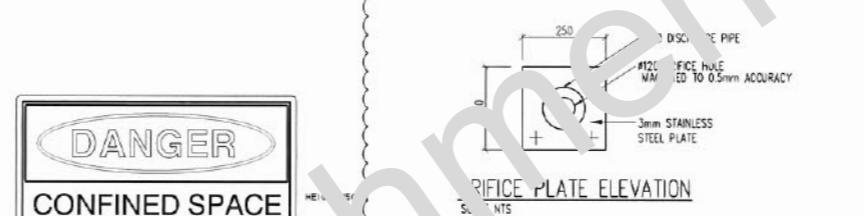
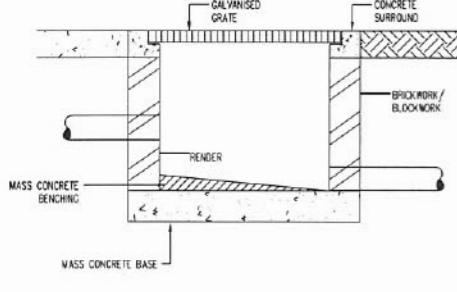
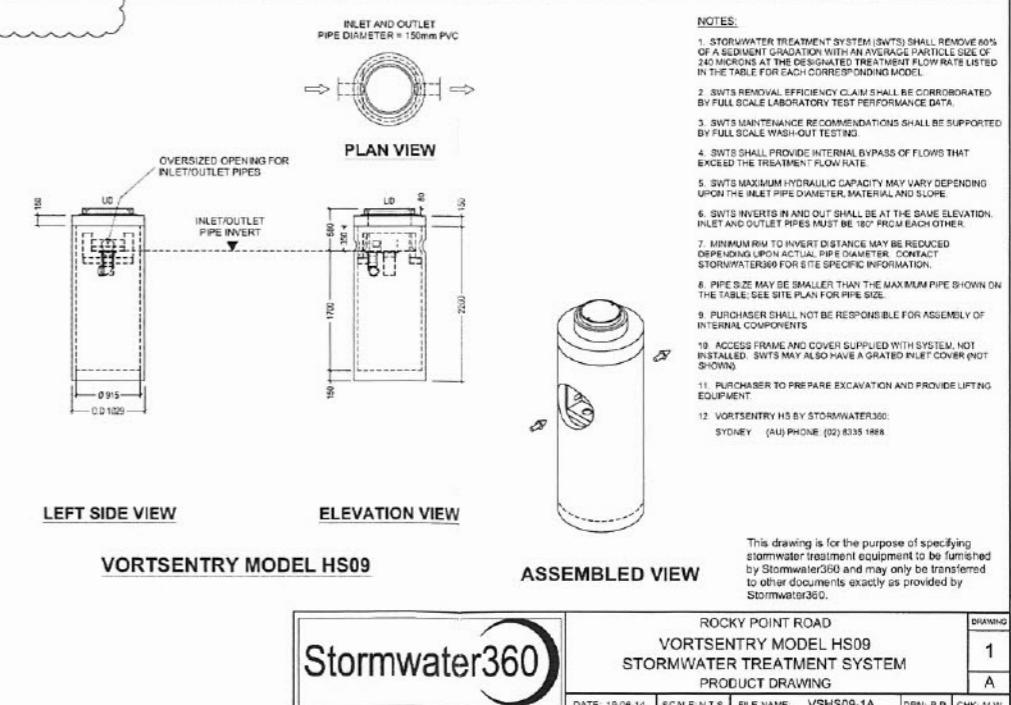
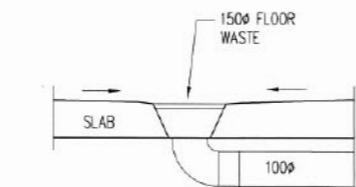


## DRAINS RESULTS SUMMARY

STORM EVENT	PRE-DEVELOPED SITE DISCHARGE (L/S)	POST-DEVELOPED SITE DISCHARGE (L/S)
5 YEAR	19	12
10 YEAR	23	14
20 YEAR	29	16
50 YEAR	38	18
100 YEAR	47	19



NOTES: RAINWATER TANKS  
1. RAINWATER TANK CAPACITY OF 5,280 LITRES.  
2. NO DIRECT CROSS-CONNECTION WITH THE SYDNEY WATER POTABLE SUPPLY  
AND AN AIR GAP MAINTAINED ABOVE THE OVERFLOW IN THE TANK.  
3. A SIGN TO BE INSTALLED STATING "NOT FOR HUMAN CONSUMPTION."

TYPICAL PIT SECTION  
N.T.S.TYPICAL PIT DETAIL  
N.T.S.TYPICAL FLOOR WASTE 'FW' DETAIL  
SCALE N.T.S.

Stormwater360

ROCKY POINT ROAD  
VORTSEENTRY MODEL HS09  
STORMWATER TREATMENT SYSTEM  
PRODUCT DRAWING  
1  
A  
DATE: 19-06-14 SCALE: N.T.S. FILE NAME: VSHS09-1A DRN: R.P. CHK: N.W.

A1 0 1 2 3 4 5 6 7 8 9 10

B ISSUED FOR D.A. APPROVAL	KE	E.H.	29.05.15
A FOR D.A. APPROVAL	E.H.	A.S.H.	26-06-14
No	AMENDMENT	ENG DRAFT DATE	No
	AMENDMENT	ENG DRAFT DATE	No
	AMENDMENT	ENG DRAFT DATE	No

ARCHITECT:  
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AUSTRALIAN CONSULTING ENGINEERS.  
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Shop 2/211 CONCORD RD NORTH STRATHFIELD NSW 2137  
PH: (02) 9763 1500 FX: (02) 9765 1515  
EMAIL: ace@aceau.com.au

PROJECT:  
RESIDENTIAL DEVELOPMENT  
227 PROSPECT ROAD  
PROSPECT SA 5082

SHEET SUBJECT:  
N  
STORMWATER DRAINAGE  
DETAILS & SECTIONS

PROJECT: 227 PROSPECT ROAD, PROSPECT SA 5082  
DATE: AUG 14 DRAWN: E.H. DESIGNED: K.E. CHECKED: A.S.H.  
SCALE: @ A1 JOB NO: 140666  
1:100 U.N.O.  
APPROVED: Dr. Anthony S. Haslam B.E. Ph.D. DWG. No: D03 REV: B

**AGENDA ITEM:** 5.3

**To:** Development Assessment Panel (DAP) on 15 June 2015

**From:** Susan Giles, Development Officer, Planning

**Proposal:** Variation to Development Application 050/467/2013 – Four Storey Residential Flat Building Comprising 24 dwellings (DA 050/116/2015)

**Address:** 100 Churchill Road, Prospect (CT 5271/55)

---

### **SUMMARY:**

Applicant: Qattro on behalf of 100 Churchill Pty Ltd

Planning Authority: Council

Referrals (Schedule 8): Nil

Public Notification: Category 1

Representations: Not applicable

Respondent: Not applicable

Development Plan Version: Consolidated 12<sup>th</sup> February 2015

Zone and Policy Area: Urban Corridor Zone (Boulevard Policy Area)

Issues: Appearance, Setbacks, Car Parking, Waste Management

**Recommendation:** **Approval, subject to conditions**

---

### **1. EXECUTIVE SUMMARY**

- 1.1 The proposal is for the variation to a previously approved four storey residential flat building, comprising 21 dwellings. The applicant proposes to increase the number of dwellings to 24. Changes are proposed to the external façade to accommodate the additional dwellings. No changes are proposed to the car parking layout, vehicular access, front and rear setbacks, or the location of the private open space or waste collection area.
- 1.2 The variation application was a Category 1 form of development therefore no public notification was undertaken. The variations proposed did not warrant any further comment from previous referral agencies. The key considerations were the impact of the proposed revised setback of the building to the side boundaries, the proposed built form and use of materials and finishes, the provision of adequate car parking, and the impact to the waste management facilities.

- 1.3 The proposal achieves the minimum residential density desired within the policy area, and within a building that proposes to use materials and finishes which are considered to provide visual interest and an appropriate built form that would not unreasonably impact adjoining allotments. Therefore it is recommended that the variation is supported.

## **2. BACKGROUND**

- 2.1 The Development Assessment Panel (DAP) at its meeting of 14 April 2014, considered a proposal for the construction of Five Storey Residential Flat Building Comprising 24 dwellings (DA 050/467/2013). The application was granted consent.
- 2.2 At its meeting on 11 August 2014, the DAP considered a variation to the development application, for a Four Storey Residential Flat Building comprising 21 dwellings (DA 050/214/2014). This application was also granted consent.
- 2.3 After the subject site changed ownership an application was considered by the DAP at its meeting of 8 December 2014, where the applicant sought to amend the external finishes on the façade (DA 050/429/2014). The application was granted consent.
- 2.4 All reports from the previous DAP meetings are available for viewing on the City of Prospect website ([www.prospect.sa.gov.au](http://www.prospect.sa.gov.au)). The plans approved by DAP at its 8 December 2014 meeting (DA 050/429/2014) are also attached for the DAP's reference (refer **Attachments 12-18**).

## **3. PROPOSAL**

- 3.1 The applicant has lodged a development application to vary the development plan consent for the four storey residential flat building. The proposed variation seeks to increase the number of apartments to 24, re-locate the lift and stairwells and modify the external finishes of the building to reflect the internal changes. Other variations would also include reconfiguration of the floor plans and balconies and an increase in the waste storage and bicycle parking area. No other amendments are proposed.
- 3.2 The proposed plans are attached (refer **Attachments 1-4**), along with an artist impression of the proposed building (refer **Attachment 5**), and correspondence from the applicant in support of the amendments (refer **Attachments 6-11**).

## **4. REFERRALS**

### **4.1 Internal (Advisory) Referrals**

- 4.1.1 With the exception of the most recent variation application, the previous development applications were referred informally to the Office of Design and Architecture SA (ODASA). The proposed changes would not vary any aspect that hasn't been previously considered by ODASA. Therefore no further consultation was undertaken.

### **4.2 External (Legislated) Referrals**

- 4.2.1 Similarly, the proposal does not vary any element of the proposal previously considered by the Department of Planning, Transport and Infrastructure, therefore no further consultation with DPTI was required.

## **5. PUBLIC NOTIFICATION**

- 5.1 The application is a Category 1 form of development pursuant to Section 38 of the *Development Act 1993*, Schedule 9 of the *Development Regulations 2008* and Urban Corridor Zone Principle of Development Control 22.

## **6. PLANNING COMMENTARY**

- 6.1 The application proposes a variation to an application previously granted development plan consent. Section 39(4) of the *Development Act 1993*, allows a relevant authority to permit an applicant to vary an application or vary any plans, drawings, specifications or other documents, provided that the essential nature of the proposed development is not changed.
- 6.2 The variation proposes to increase the number of apartments by three and modifications to the external materials and finishes of the building. Accordingly, the essential nature of the proposal as a multi-storey residential development remains unchanged.
- 6.3 The proposal is neither a complying nor a non-complying development with reference to Principle of Development Control 21 and 22 of the Urban Corridor Zone and is therefore to be considered on its merits against the relevant provisions of Council's Development Plan.
- 6.4 Pursuant to Section 35(2) of the *Development Act 1993*, a development that is assessed by the Council as being seriously at variance with the Development Plan must not be granted consent. To this end, the Panel must determine whether the proposal is seriously at variance with the Development Plan prior to making a decision on the application.

## **7. PLANNING ASSESSMENT**

### **7.1 Design and Appearance**

- 7.1.1 The proposal would vary the previously approved development to increase the number of apartments from 21 to 24. Each level is now proposed to contain 8 apartments, rather than 7.
- 7.1.2 The north-south orientation of the apartments remains unchanged, however the internal floor layout, including the size of balconies, have been modified. The lift, stairwells and common walkway have been re-located to be closer to the southern boundary. The waste and bicycle storage area has increased in area as a result of moving the southern stairwell.
- 7.1.3 The location of the air conditioning plant would remain unchanged, with the air conditioning plant for the first floor apartments located on the western end of Level 1. The remainder of air conditioning plant would be located centrally on the roof.
- 7.1.4 The external form of the building is proposed to change, reflecting the proposed modifications to the floor plans. The proposed materials and finishes to the building would comprise feature coloured Hebel power panel cladding, precast concrete panel with paint finish, lightweight feature cladding, a steel column with paint finish, and a mix of stainless steel framed glass balustrading and solid balustrading comprising Hebel power panel cladding. All elements of brick have been removed.

- 7.1.5 The external walls would comprise a render colour pallet consisting of light grey, dark grey/charcoal and red. The colours would not incorporate highly reflective materials that would result in glare to neighbouring properties, drivers or cyclists (Council-wide PDC 133).
- 7.1.6 At ground level, the location of the vehicle access point would remain unchanged. The location of the pedestrian entry point would be located next to the garage door, and would have a feature canopy that would provide a distinctive and direct access from the street (Council-wide PDC 143).
- 7.1.7 It is anticipated that development within the Urban Corridor Zone would achieve a high standard of architectural design through careful building articulation and fenestration to all visible sides. The design of building facades should contribute positively to the street by articulating the built form and accentuating the building's functions. The proposed use of materials and finishes, together with the balconies that wrap around the building, is considered to provide a high level of visual interest and results in an appropriate built form.
- 7.1.8 During the assessment of the previously approved applications for the subject site, ODASA was supportive of the design layout, including the height, scale, built form and floor plans. While the variations would alter some aspects of the previous plans, overall, the variations would not affect the advice previously provided from ODASA.

## **7.2 Private open space provision**

- 7.2.1 Private open space areas located above ground level should have a minimum width of 2 metres and be directly accessible from a habitable room (Council-wide PDC 153). Two bedroom dwellings should have a minimum of 11m<sup>2</sup> (Council-wide PDC 152).
- 7.2.2 The dwellings would be provided with an area of between 10.2m<sup>2</sup> and 14.19m<sup>2</sup> for the dwellings facing Churchill Road. The balconies for the dwellings at either end of the building would have an area of 7.6m<sup>2</sup> and 9.7m<sup>2</sup>, when excluding those areas that do not achieve the desired minimum dimension of 2m.
- 7.2.3 While the balconies would not quite achieve the minimum area of 11m<sup>2</sup>, it is acknowledged that the areas could still achieve the domestic functions expected for an occupant, such as being directly accessible from the internal living areas of the dwelling; having a northerly aspect to provide for comfortable year round use; and having sufficient area and shape to be functional, including the provision for external clothes drying areas (Council-wide PDC 148). Therefore the amount of private open space for the dwellings is considered appropriate.

## **7.3 Setbacks**

- 7.3.1 The amendments have not altered the front or rear setback of the building, thus the proposed building would satisfy the minimum front and rear setbacks desired within the Boulevard Policy Area (UCZ PDC 16). However the side boundary setbacks would change.
- 7.3.2 Within the Urban Corridor Zone on allotments with a frontage width of 20 metres or less, there is no minimum setback for the first 2 levels of a building from a side boundary when adjoining another allotment, and a minimum 2m setback is required for all levels above this height (UCZ PDC 18).

- 7.3.3 As a result of increasing the number of dwellings per floor, the internal layout of the building has accordingly been modified to accommodate the reconfiguration. The siting of the ground floor level would remain unchanged. However, the lift and stairwells have been moved. The stairwell at the front of the building has been extended out to adjoin the southern boundary for a length of 4.8m.
- 7.3.4 The common walkway located on the southern side of the building would be setback 1.4m from the southern boundary. The walkway would be 1.5m wide. The balconies for the dwellings located at each end of the building would be 1m from the northern boundary. The bulk of the southern side of the building would be setback 3m from the side boundary, with the majority of the northern side of the building setback 2m.
- 7.3.5 The setback of the stairwell, the walkway and the balconies on Level 2 and 3 would be less than the desired minimum of a 2m setback to the side boundaries. The balustrade for the walkway is proposed to be predominantly glass, with the inclusion of Hebel power panel cladding to provide visual interest. The balustrade for the balconies would comprise all glass. When considering the combination of materials and the location of the balconies and walkway, the lesser setback is not considered to have an unreasonable impact on adjoining properties.
- 7.3.6 The subject site is located on Concept Plan Fig UrC/6 where there is no minimum front setback. Unlike the subject site, the minimum front setback desired for the adjoining allotment at 98 Churchill Road would be 3m from Churchill Road. Currently, the adjoining site is occupied by commercial enterprise where the area to the front of the site, adjoining 100 Churchill Road is used as a staff car park. The wall is proposed to be a dark grey/charcoal feature wall, therefore together with its location the boundary wall is unlikely to have an unreasonable impact to the current and future occupiers of the adjoining southern allotment.
- 7.3.7 The siting of the building on the land is therefore considered appropriate.

#### **7.4 Car parking**

- 7.4.1 The proposal would vary the number of dwellings provided on site. However, the proposed car parking configuration and layout would remain unchanged. Within the Urban Corridor Zone, it is anticipated that the provision of car and bicycle parking would be in accordance with Tables Pr/5 and Pr/6 of Council's Development Plan.
- 7.4.2 Accordingly, it is anticipated that one bicycle park for every four dwellings, and one bicycle park per visitor for every ten dwellings (Table Pr/6). Therefore 8.5 bicycle parks should be provided. The proposal remains unchanged with ten bicycle parks, thus comfortably satisfying the Development Plan provision.
- 7.4.3 With regard to the provision of car parking, one car parking space is desired for each 1-2 bedroom dwelling. An additional 0.25 space is desired per dwelling for visitor parking. Consequently, the anticipated car parking rate for 24 dwellings would be 30 car parking spaces, comprised of 24 for occupants and 6 for visitor parking.
- 7.4.4 The original application for the subject site was for 24 dwellings, within a five storey building. At that time, it was suggested that the number of car parking spaces would be sufficient for the 24 dwellings given the proximity of the site to public transport and the availability of nearby on-street parking. Accordingly, the position of Council's planning staff remains unchanged.

## 7.5 Waste Management

- 7.5.1 New developments should provide a dedicated area for the on-site storage, collection and sorting of recyclable materials and waste that is consistent with the following:
- (a) easily and safely accessible to the collection point
  - (b) easily and safely accessible to residents and collection service providers
  - (c) well screened to prevent vandalism and theft
  - (d) designed to reduce odour and discourage vermin (Council-wide PDC 170).
- 7.5.2 The communal waste system area would remain at the rear of the car park and be larger in area than previously considered. With the increase of dwellings, the applicant has adjusted the waste arrangement in accordance with the '*SA Best Practise Guide: Waste Management in Residential or Mixed Use Development*', published by the State Government.
- 7.5.3 The guidelines suggest that there would be 30 litres of waste per room per week, 25 litres of recycling waste per room per week and 10 litres of organic waste per room per week. Accordingly, for 48 bedrooms the weekly total anticipated would be 1440 litres of waste, 1200 litres of recycling waste and 480 litres of green organics collected each week. The applicant advises that four 660 litre bins for general and organic waste and two 660 litre bins for recycling waste will be provided (refer **Attachment 10**). Accordingly, the development can accommodate for the volumes of waste anticipated within the building.

## 8. CONCLUSION

- 8.1 The proposed amendments are essentially to increase the number of dwellings within the four-storey building. While the most notable amendments would be to the external appearance of the building, the essential nature of the development remains unchanged. The proposal still achieves the minimum residential density desired within the policy area through the construction of a four-storey residential flat building comprising 24 dwellings.
- 8.2 For the most part, the configuration of each level remains largely unchanged, with each apartment facing north-south, balconies facing north, the common walkway along the southern side of the building, lift and stairwells to the front and rear of the site, and car parking provided at ground level. While the changes have resulted in the building falling short of achieving the minimum 2m setback desired to the side boundaries, the overall impact to the adjoining properties is considered to be minor.
- 8.3 The materials and finishes to the external built form are considered to provide a high level of visual interest. The front and rear setbacks, car parking, vehicular access, private open space, storage facilities, waste collection and thermal comforts remain consistent with the previously approved proposal.
- 8.4 The application is therefore considered to be consistent with the relevant provisions of the Prospect (City) Development Plan and warrants the granting of development plan consent, subject to appropriate conditions as per the original consent (refer **Attachments 18-21**).

## 9. RECOMMENDATION

It is recommended:

That with reference to the relevant provisions of the Prospect (City) Development Plan, the zoning of the land within which the proposed development is situated and the locality within which the land is situated, the Panel resolves that development application 050/116/2015 is not seriously at variance with the Development Plan and as such a decision shall be made on the merits of the application; and

That pursuant to the *Development Act 1993*, as amended, Development Plan Consent be approved to DA 050/116/2015 from Qattro on behalf of 100 Churchill Pty Ltd for a variation to Development Application 050/467/2013 - Four Storey Residential Flat Building Comprising 24 dwellings at 100 Churchill Road, Prospect (CT 5271/55), subject to the following conditions and notes:

1. The development shall take place in accordance with plans and details stamped by Council relating to Development Application Number 050/116/2015, except as modified by any conditions detailed herein. All works detailed in the approved plans and required by conditions are to be completed prior to the occupation of the approved development.
2. The conditions, where pertinent, of the original development plan consent numbered 050/467/2013 (for a *Five Storey Residential Flat Building Comprising 24 dwellings*) shall be complied with to the reasonable satisfaction of Council at all times particularly conditions 2, 3, 4, 5, 6, 7, 8, 9, 10, 11, 12, 13, 14, 15, 16, 17, 18, 19, 20, 21, 22, 23, 24.
3. The external building materials and finishes shall be constructed as per Drawing Numbers 03 of 04 and 04 of 04, Revision B, dated 13/05/15.

### **Advisory Notes:**

- (1) Pursuant to Section 86(1)(a) of the Development Act, 1993, you have the right of appeal to the Environment, Resources and Development Court against either 1) a refusal of consent or 2) any condition(s) which have been imposed on a consent. Any such appeal must be lodged with the Court within two (2) months from the day on which you receive this notification or such longer period as may be allowed by the Court.

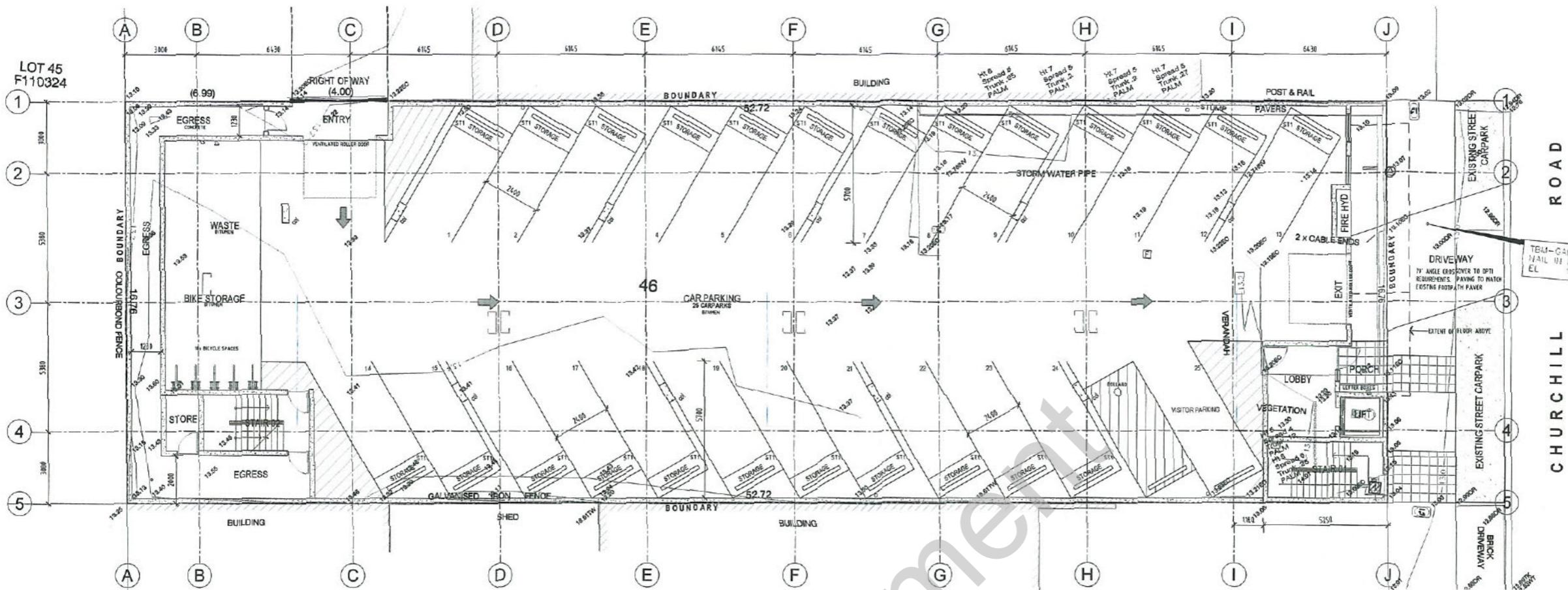
The Environment, Resources and Development Court is located in the Sir Samuel Way Building, Victoria Square, Adelaide SA 5000 (Postal Address: GPO Box 2465, Adelaide SA 5001).

- (2) The development plan consent granted herein is effective for a period of twelve (12) months from the date of the decision. Unless Council extends this period, building rules consent is required within this time or the consent will lapse.

Any request for an extension of the operative period of the consent must be submitted to Council in writing, accompanied by the applicable fee.

- (3) The consent of the Commissioner of Highways is required under the Metropolitan Adelaide Road Widening Plan Act to all building work on or within 6.0 metres of the possible requirement. The applicant is to complete the 'Metropolitan Adelaide Road Widening Plan Act, 1972', and submit to the Department of Transport, Energy and Infrastructure, together with three copies of the approved plans.

**Susan Giles**  
Development Officer, Planning



LEGEND	
SF	SELECTED PROPRIETARY OVER BONNET STORAGE REFER DETAIL
P	TOILET PAN
V8	VANITY BASIN WITH ASSOCIATED JEWELLERY UNIT
SH	SHOWER
PR	REFRIGERATOR
DW	DISHWASHER
SS	STAINLESS STEEL SINK
H&L	HOT PLATE & UNDER BENCH
UD	UTENSIL DRAWER
PTY	PANTRY
RH	RANGE HOOD VENTED TO ATMOSPHERE
WM	WASHING MACHINE
TR	INSET TRIM
<b>TOTAL APARTMENTS:</b> 10	
<b>TOTAL CARPARKS:</b> 25	
<b>TOTAL VISITORS:</b> 25 - 70 + 10 VISITORS	
<b>BICYCLE STORAGE:</b> 10	

AREAS	
UNIT 1,9,17	62.47m <sup>2</sup> 14.08m <sup>2</sup>
TOTAL	72.46m <sup>2</sup>
UNIT 2,10,18	62.47m <sup>2</sup> 9.15m <sup>2</sup>
TOTAL	71.62m <sup>2</sup>
UNIT 3,11,19	62.47m <sup>2</sup> 9.15m <sup>2</sup>
TOTAL	71.62m <sup>2</sup>
UNIT 4,12,20	62.47m <sup>2</sup> 9.15m <sup>2</sup>
TOTAL	71.62m <sup>2</sup>
UNIT 5,13,21	62.47m <sup>2</sup> 9.15m <sup>2</sup>
TOTAL	71.62m <sup>2</sup>
UNIT 6,14,22	62.47m <sup>2</sup> 9.15m <sup>2</sup>
TOTAL	71.62m <sup>2</sup>
UNIT 7,15,23	62.47m <sup>2</sup> 9.15m <sup>2</sup>
TOTAL	71.62m <sup>2</sup>
UNIT 8,16,24	62.47m <sup>2</sup> 14.08m <sup>2</sup>
TOTAL	76.55m <sup>2</sup>



AMENDMENTS		
REV DATE	DESCRIPTION	WHO
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C		
B 10/05/15	REVISED FOR COUNCIL	G5
A 18/05/15	FOR DA APPROVAL	G5



Unit 1, 10, 18, 26, 34, 42, 50, 58, 66, 74, 82, 90, 98, 106, 114, 122, 130, 138, 146, 154, 162, 170, 178, 186, 194, 202, 210, 218, 226, 234, 242, 250, 258, 266, 274, 282, 290, 298, 306, 314, 322, 330, 338, 346, 354, 362, 370, 378, 386, 394, 402, 410, 418, 426, 434, 442, 450, 458, 466, 474, 482, 490, 498, 506, 514, 522, 530, 538, 546, 554, 562, 570, 578, 586, 594, 602, 610, 618, 626, 634, 642, 650, 658, 666, 674, 682, 690, 698, 706, 714, 722, 730, 738, 746, 754, 762, 770, 778, 786, 794, 802, 810, 818, 826, 834, 842, 850, 858, 866, 874, 882, 890, 898, 906, 914, 922, 930, 938, 946, 954, 962, 970, 978, 986, 994, 1002, 1010, 1018, 1026, 1034, 1042, 1050, 1058, 1066, 1074, 1082, 1090, 1098, 1106, 1114, 1122, 1130, 1138, 1146, 1154, 1162, 1170, 1178, 1186, 1194, 1202, 1210, 1218, 1226, 1234, 1242, 1250, 1258, 1266, 1274, 1282, 1290, 1298, 1306, 1314, 1322, 1330, 1338, 1346, 1354, 1362, 1370, 1378, 1386, 1394, 1402, 1410, 1418, 1426, 1434, 1442, 1450, 1458, 1466, 1474, 1482, 1490, 1498, 1506, 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**LEGEND**

P	TILE PAN
VB	VANITY BASIN WITH ASSOCIATED JEWELLERY UNIT
SH	SINK
FR	REFRIGERATOR
DW	DISHWASHER
SS	STAINLESS STEEL SINK
HPL	HOT PLATE & UNDER BENCH
OVN	OVEN
PTY	RANGE HOOD VENTED TO ATMOSPHERE
PH	PANTRY
WN	WASHING MACHINE
TR	INLET THROUGH

# AMENDED PLAN



FOR PLANNING APPROVAL

AMENDMENTS			
REV	DATE	DESCRIPTION	RENO
G			
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E			
D			
C			
B	13/05/15	REVISED FOR COUNCIL	GS
A	18/05/15	FOR DA APPROVAL	GS



Liaison Officer: Mr. Rajesh Patel  
Quarry St, St. Helier  
Guernsey GY1 1AB  
Email: rajesh.patel@qattro.com

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Address: 100 CHURCHILL RD,  
PROSPECT

Title: PROPOSED FLOOR PLANS

Drawings Number: 02 of 04

Drawings Number: A1

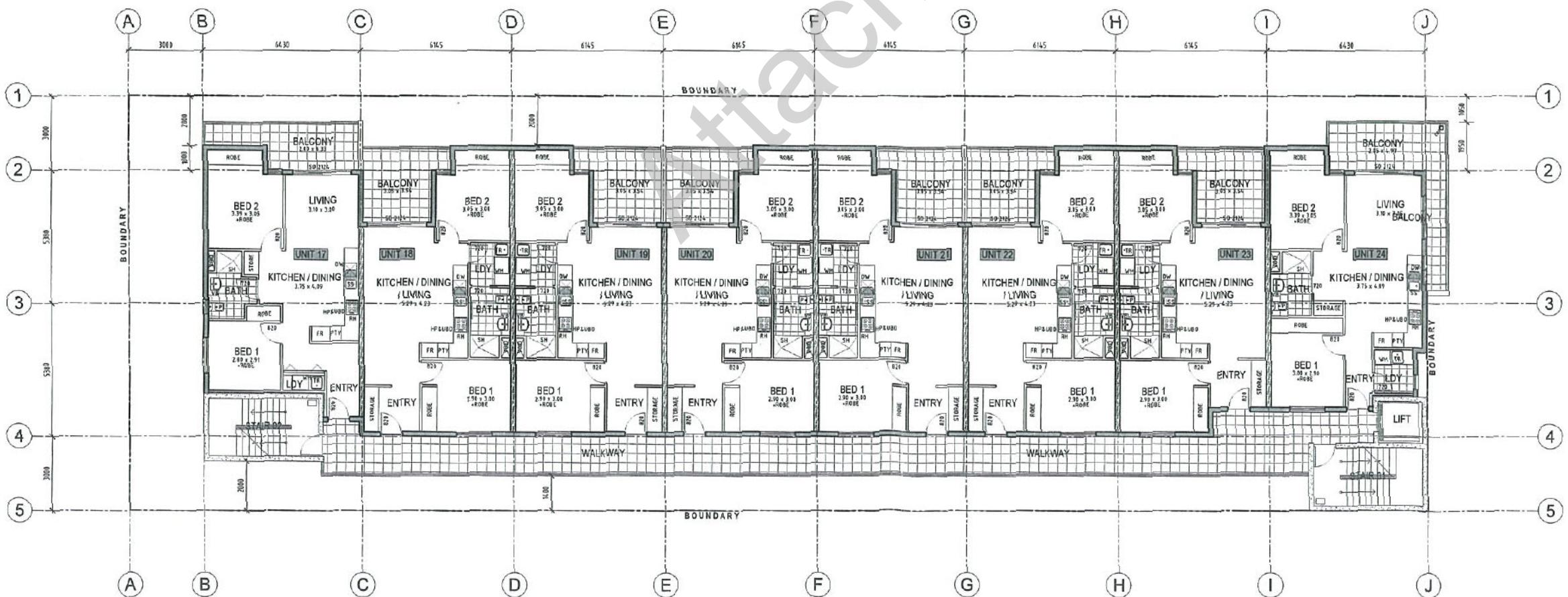
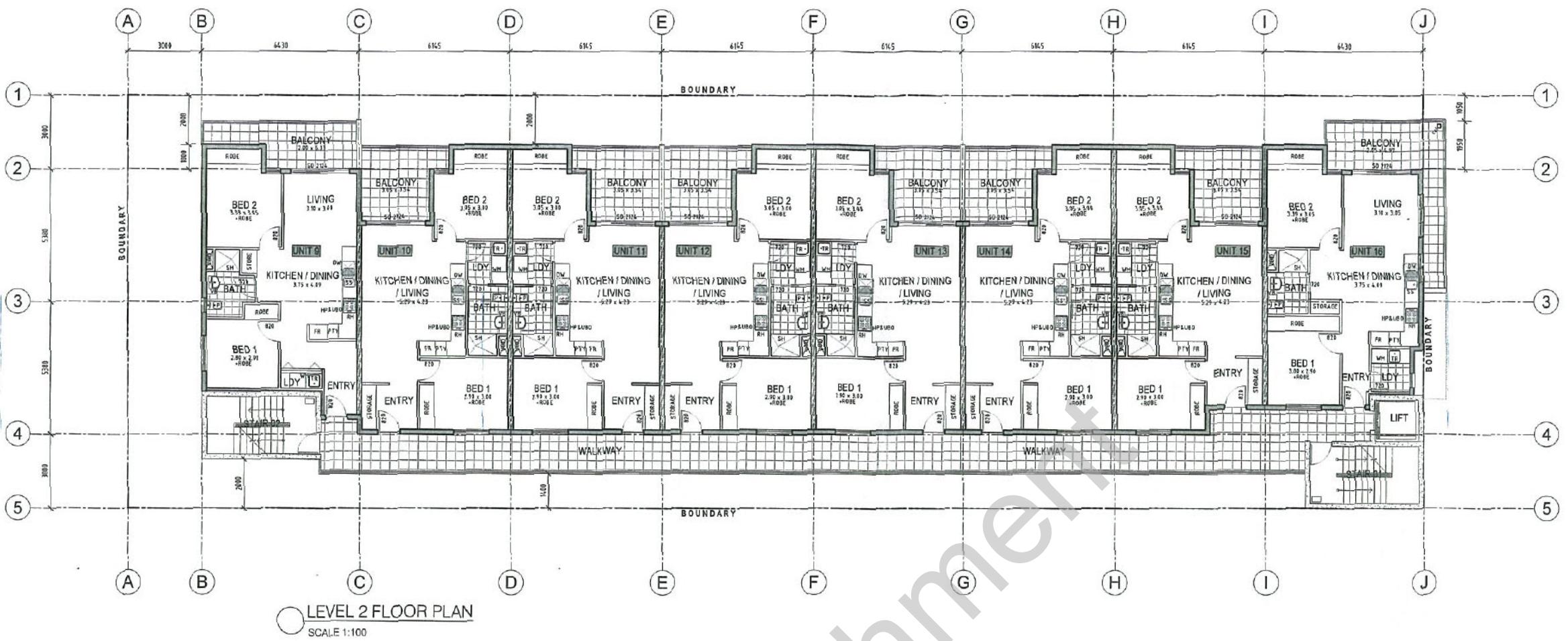
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Drawings Number: 11380

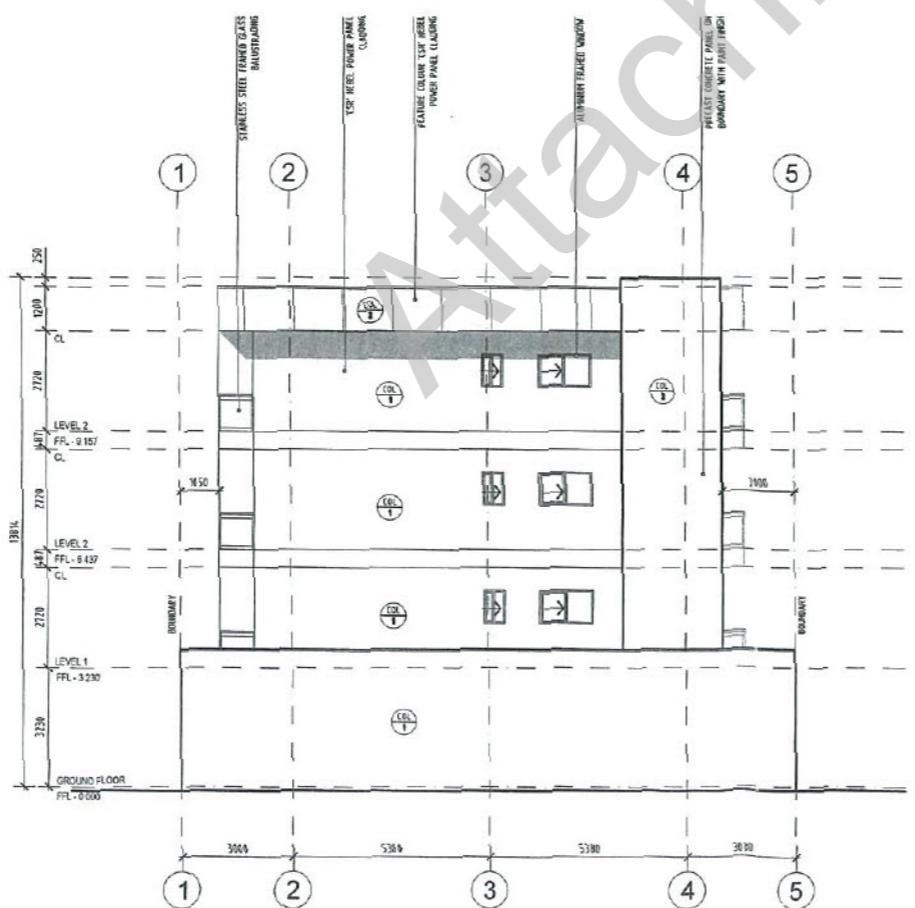
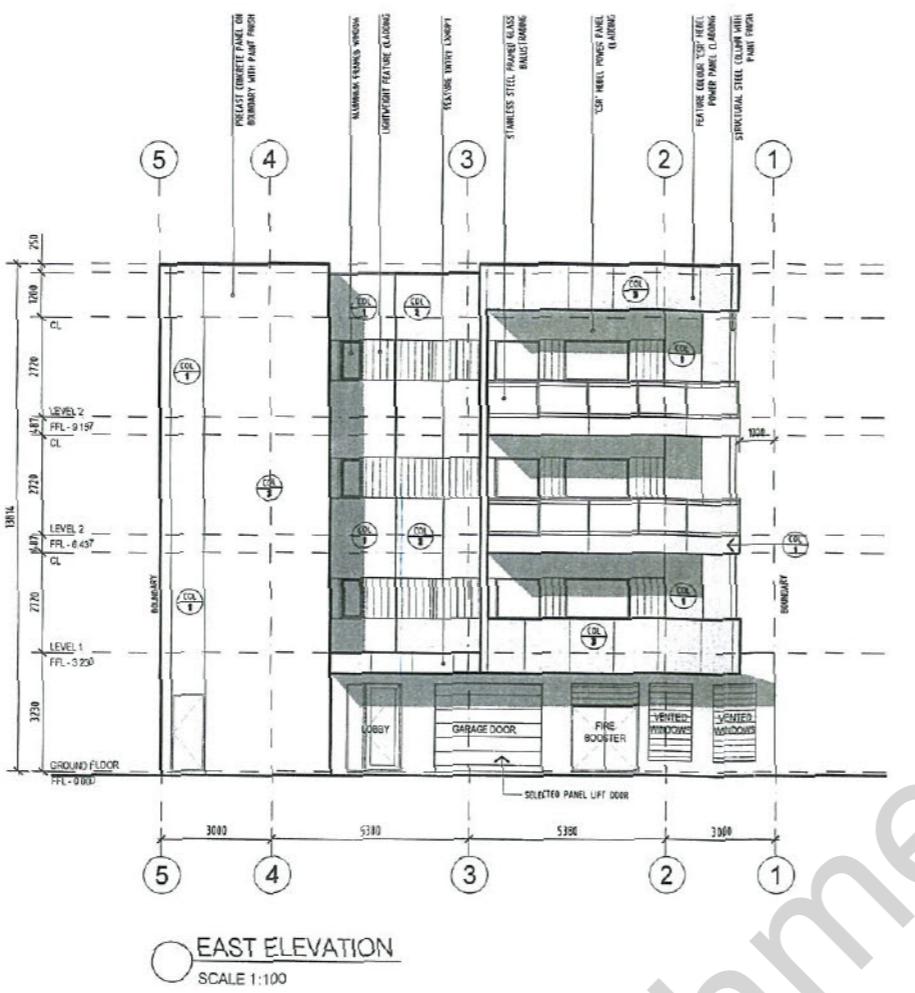
Drawings Scale: AS SPECIFIED

Revision: B

Client Initials:



# AMENDED PLAN



**WALL COLOUR LEGEND**

- COL 1: LIGHT GREY - HAIN RENDER
- COL 2: DARK GREY / CHARCOAL - FEATURE WALL
- COL 3: RED - FEATURE COLOUR

NOTE: ALL SOFFIT LININGS INCLUDING EAVES, PORCH & CANOPIES TO BE SOLVER - SIMPLY WHITE 5229 UNLESS OTHERWISE NOTED

NOTE: REFER TO IMAGE FOR FURTHER DETAIL

FOR PLANNING APPROVAL  
18/05/15

AMENDMENTS			
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E			
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C			
B	13/05/15	REVISED FOR COUNCIL	GS
A	18/05/15	FOR DA APPROVAL	GS



Level 1, 100 Churchill Rd,  
Dartford, Kent DA1 4JL  
01322 380500  
[www.qattrouk.co.uk](http://www.qattrouk.co.uk)

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ADDRESS:  
100, CHURCHILL RD,  
PROSPECT

FILE:  
ELEVATIONS / TYPE PLAN

DRAWN NUMBER: 03 of 04

DRAWN SCALE: A1

JOH NUMBER: 11380

ENHANCED SCALE: AS SPECIFIED

REVISION: B

SCALE: 1:100 @ A1  
1:200 @ A3

CUSTOM INITIALS:

## AMENDED PLAN

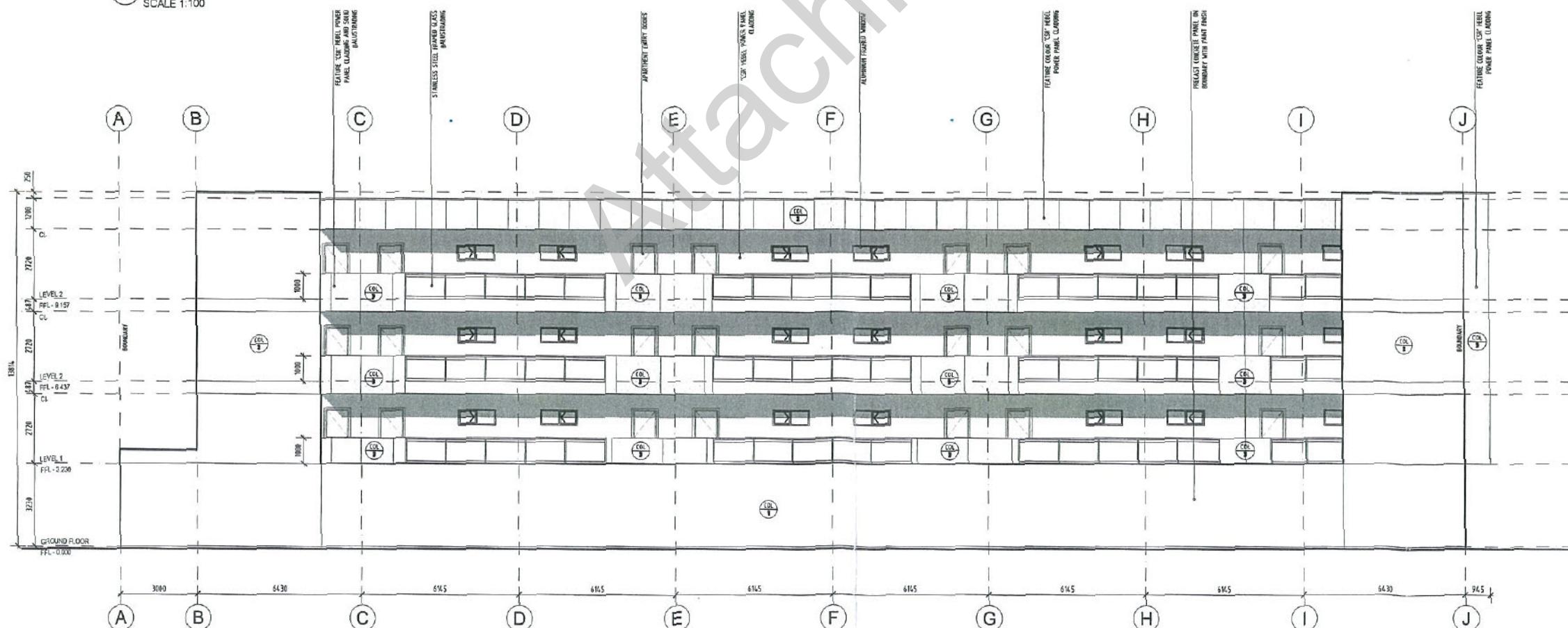
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TE: ALL SOFFIT LININGS INCLUDING  
VES, PORCH & CANOPIES TO BE  
LVER - SIMPLY WHITE 6329 UNLESS  
HERWISE NOTED)

TE: REFER 3D IMAGE FOR FURTHER  
TAIL



 NORTH ELEVATION  
SCALE 1:100



 SOUTH ELEVATION  
SCALE 1:100

FOR PLANNING  
APPROVAL  
180515

AMENDMENTS			
REV	DATE	DESCRIPTION	WHO
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F			
I			
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C			
B	13/09/15	REvised FOR COUNCIL	GS
A	18/09/15	FOR DA APPROVAL	GS






  
 HIA NSW [www.hia.org.au/nsw](http://www.hia.org.au/nsw) 08 8350 5600
   
 South West Building Association [www.southwest.com.au](http://www.southwest.com.au) 08 8350 0265
   
 Mortgage Australia [www.mortgageaustralia.com.au](http://www.mortgageaustralia.com.au) 08 8350 0265

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SPECT

**NOTATIONS - SHEET 2**

DRAWING SCALE:  
AS SPECIFIED

INITIALS \_\_\_\_\_





11380

27 May 2015

Susan Giles  
Development Officer, Planning  
City of Prospect  
128 Prospect Road  
PROSPECT SA 5082

Dear Ms Giles

**Development Application – Variation to previous DA050/429/2014 to vary the approval to a four-storey residential flat building containing 24 two-bedroom dwellings at 100 Churchill Road, Prospect.**

Qattro acts for 100 Churchill Road Pty Ltd in relation to the abovementioned development application.

This submission is made in support the development application, which seeks development plan consent to vary the approval to a four-storey residential flat building containing 24 dwellings at 100 Churchill Road, Prospect.

**1. Application Documents**

This letter supports the following plans and other documents that have previously been lodged with Council constituting the development application:

- development application form;
- site, landscape, land division and architectural plans (in triplicate);
- copy of CT Volume 5271 Folio 55;
- power line clearance declaration form; and
- 3D Perspective

**2. Background**

The land is located at 100 Churchill Road, Prospect, being situated on the western side of Churchill Road and to the south of Charles Cane Reserve.

The land is currently is currently vacant following the demolition of the existing office building.

### 3. Definitions

The *Development Regulations 2008* defines a “residential flat building” to mean:

*...a single building in which there are 2 or more dwellings, but does not include a semi-detached dwelling, a row dwelling or a group dwelling*

### 4. The proposed development

The subject site enjoys a granted development plan consent for a residential flat building containing twenty-one (21) dwellings. Qattro seek development plan consent on behalf of the client to use the subject land for a four-storey residential flat building containing 24 two-bedroom dwellings.

More specifically, this variation application seeks development plan consent for:

- Change from 21 to 24 apartments
- Lobby and lift and stair mirrored
- Stair 1 and Lift is on the boundary
- Additional Store Room on ground floor behind Stair 2
- Stair 2 has been rotated and moved rearwards to have a 2m setback in lieu of 1620mm as approved
- Elevations are similar but all screens and brickwork has been deleted
- Refer to 3D image for elevation
- Refer to plans for further detail

### 5. Development Plan

The relevant consolidation of Council's Development Plan is dated 12 February 2015.

The land is located in the Urban Corridor Zone and within the Boulevard Policy Area.

### 6. Relevant principles

An application seeking development plan consent involves an assessment of the development proposal against the provisions of the Development Plan.

The relevant Urban Corridor Zone and Boulevard Policy Area Objectives and Principles of Development Control from Council's Development Plan as consolidated 12 February 2015 are:

**Urban Corridor Zone**

Objectives: 1, 2, 3, 4, 5, 6, 7 and 8

Principles of Development Control: 1, 3, 5, 6, 7, 8, 9, 10, 11, 12, 13, 14, 15, 16, 17, 18 and 22

**Boulevard Policy Area**

Objectives: 1, 2, 3 and 4

Principles of Development Control: 1, 2, 4, 5, 6 and 7.

I have considered the relevant Objectives and Principles of Development Control and assessed them against the proposed variation and I consider that the proposal warrants the granting of development plan consent.

In a more qualitative assessment the proposed variation complies with the intent of the Urban Corridor Zone and Boulevard Policy Area and is not at variance with Council's Development Plan. The proposed variation and development in its entirety responds well to its location within the immediate locality and is representative of the quality of built form Council is advocating along the Churchill Road precinct.

**7. The planning merits**

*Land Use*

When considering the suitability of a proposed land use, it is necessary to consider not just the relevant provisions of the Development Plan, but also the intent and factual context applicable to the land.

The land is within the Urban Corridor Zone and Boulevard Policy Area. The Zone provision contemplate that generally the area will continue as a residential area, albeit at an increased density.

*Intensity of Use*

The proposed use and subsequent density is envisaged within the Zone and Policy Area. It will, in my contention, sit comfortably and compatibly within the immediate and wider

locality. The Development Plan anticipates minimum net residential site density of 100 dwellings per hectare. The proposal adequate achieves this minimum density target.

#### *Car Parking*

The variation proposes an additional three (3) dwellings to the current planning approved development. The proposed car parking layout provides one (1) car parking space for each dwelling and one (1) visitor car parking space.

The proposed car parking layout provides car parking that takes advantage of the site's right of way and offers a layout and dimensions which achieves concurrence with the Australian Standards and the intent of the Development Plan.

Table Pr/5 suggests that development of this nature should theoretically provide 30 car parking spaces, whereas 25 are proposed (meaning a shortfall of 5 car parking spaces). Table Pr/5 also contemplates a lesser parking supply depending on local circumstances. In this case, the proximity to Churchill Road and the 'Go-Zone' for public transport in the form of buses, the proximity to Dudley Park Railway Station (approximately 500m) and the availability of on-street parking for visitors along Churchill Road, Elizabeth Street and Devonport Terrace between Charles Cane Reserve and the railway line, provides much flexibility when applying the car parking rate.

In our experience in designing, constructing, selling and managing these type of developments within the City of Prospect along the Churchill Road precinct, Qattro have observed that car ownership of residents of 2-bedroom dwellings tends to average at just under one (1) car per apartment. Accordingly the level of supply in this instance will be acceptable.

Overall, the traffic and parking arrangements proposed as part of this variation are considered to be acceptable and not at variance with Council's Development Plan.

#### *Outdoor Storage and Service Areas*

The proposal is considered to comply with Council Wide Principles 147 and 170, which provide that outdoor storage and service areas should be screened from view and located to be convenient for users whilst achieving adequate separation from sensitive land uses.

An area for waste storage is provided near the western boundary, enclosed on the ground floor. This area is to be screened from both public and private view.

The waste area is located in order to take advantage of the right-of-way avoiding bin collection from Churchill Road. The waste area is located within close proximity of the rear stair to enable easy access for apartment residents.

Adjacent the waste storage area is the location of the bicycle storage area.

As per Council Wide PDC 171, a communal collection of waste is proposed as the proposed development is over 2000m<sup>2</sup>. Accordingly, a private waste collection is proposed rather than a Council collection.

The '*SA Best Practice Guide: Waste Management in Residential or Mixed Use Development*', published by the State Government in 2014, provides the following waste generation rate:

**Table C.2: Waste Resource Generation Rates (WRGRs) by land use type**

Land Use Type	Waste Resource Generation Rate				
	General Waste	Recycling	Organics	Metric	Other
High Density Residential Dwelling	30	25	10	L/bedroom/wk	Hard and Electronic Waste 0.77m <sup>3</sup> / household/year

As the variation proposes three additional dwellings to the 21 previously approved, the development will consist of 48 bedrooms. The following levels of waste are generated per week according to the guide above:

Type of Waste	Generation / week (L)
General	1,440
Recycling	1,200
Organic	480

Based on the figures presented in the above table, the development requires four 660L bins for general and organic waste and two 660L bins for recycling, collected weekly by a private contractor.

Given the above, it is our view that the proposed variation of development provides for the adequate provision of waste storage and collection.

#### Conclusion

For all of the reasons set out above, it is tendered that the application submitted to Council vary to previous DA050/429/2014 to vary the approval to a four-storey residential flat building containing 24 two-bedroom dwellings at 100 Churchill Road, Prospect warrants the granting of development plan consent for the following, but not limited to, reasons:

- residential flat buildings of higher density forms of residential development are much sought after developments within the Urban Corridor Zone and Boulevard Policy

Area. In this sense, the proposal meets the intent of the Development Plan from a land use and built form development perspective;

- the proposed density is considered acceptable as higher-than-existing density residential uses are contemplated in the Boulevard Policy Area. The proposal also satisfies the majority of setback, height and car parking standards as prescribed in Council's Development Plan;
- the building will address the street in a positive manner. In particular, the use of glazing and various colours and materials will present a visually interesting appearance to the public realm. The inclusion of numerous balconies will assist by creating an active and engaging relationship with the Churchill Road streetscape;
- the design gives adequate regard to the interface with the adjoining Residential Zone with the use of generous setbacks to the western boundary, which in turn minimises the impacts of overshadowing, bulk and massing. Similarly, the orientation of the balconies and glazing largely prevent any overlooking; and
- it is contended by this report that the proposal will generate an appropriate amount of car parking and waste, that has been mitigated by inclusive design, generated by residential development, and in turn, the proposal makes adequate provision for car parking and waste storage.

The proposed development satisfies the most relevant provisions and intent of the City of Prospect Development Plan. For this reason the variation proposal is not seriously at variance with Council's Development Plan and therefore warrants the granting a development plan consent.

Yours sincerely

**Dylan Grieve**  
**Qattro**  
Planning Manager

0458 501 124  
[dgrieve@qattro.com.au](mailto:dgrieve@qattro.com.au)

## SITE NOTES

THIS DRAWING IS TO BE READ IN CONJUNCTION WITH ALL OTHER DRAWINGS, DOCUMENTATION, SCHEDULES AND SPECIFICATIONS.

BUILDER TO CHECK AND CONFIRM ALL SITE AND SET OUT DIMENSIONS PRIOR TO COMMENCEMENT OF CONSTRUCTION.

WRITTEN DIMENSION TO BE TAKEN IN PREFERENCE TO SCALE.

ALL CONSTRUCTION TO COMPLY WITH RELEVANT AUSTRALIAN BUILDING CODES AND STANDARDS.

FOUNDATIONS, EXCAVATIONS, REINFORCEMENT PLACEMENT, ETC TO BE INSPECTED AND APPROVED BY ENGINEER OR BUILDING SURVEYOR PRIOR TO ANY CONCRETE PLACEMENT.

GROUND LEVELS AND FINISHED FLOOR LEVELS INDICATED ARE APPROXIMATE ONLY AND ARE TO BE CONFIRMED ON SITE.

NO VARIATION MAY BE MADE TO THIS DRAWING WITHOUT PRIOR APPROVAL OF THE PROPRIETOR OR DESIGNERS.

REFER TO ENGINEERS DESIGN, DOCUMENTATION, CALCULATION AND SPECIFICATION FOR STRUCTURAL, ELECTRICAL, HYDRAULIC AND CIVIL DETAILS IF APPLICABLE.

STORMWATER TO BE DISCHARGED TO STREET WATER TABLE OR RAINWATER TANK IN ACCORDANCE WITH COUNCIL REQUIREMENTS AND/OR DIRECTIVE.

SURFACE WATER RUN-OFF FROM NOT LESS THAN 50% OF ROOF CATCHMENT AREA TO BE STORED IN ROOF RAIN WATER TANK AND, PLUMBED TO EITHER A TOILET, WATER HEATER OR LAUNDRY. RAIN WATER AND INTERNAL OVERFLOW FROM RAIN WATER TANK MUST BE FITTED WITH INSECT PROOF AND NON DEGRADABLE SCREENS.

ANY DISPARANCES IN DOCUMENTS AND/OR ON SITE TO BE REPORTED TO THE DESIGNER BEFORE ANY WORK IS COMMENCED.

GROUND FLOOR AREAS	
PORCH:	4.5m <sup>2</sup>
Lobby:	11.8m <sup>2</sup>
CAR PARKING:	74.3m <sup>2</sup>
SERVICES:	42.3m <sup>2</sup>
TOTAL:	100.8m <sup>2</sup>

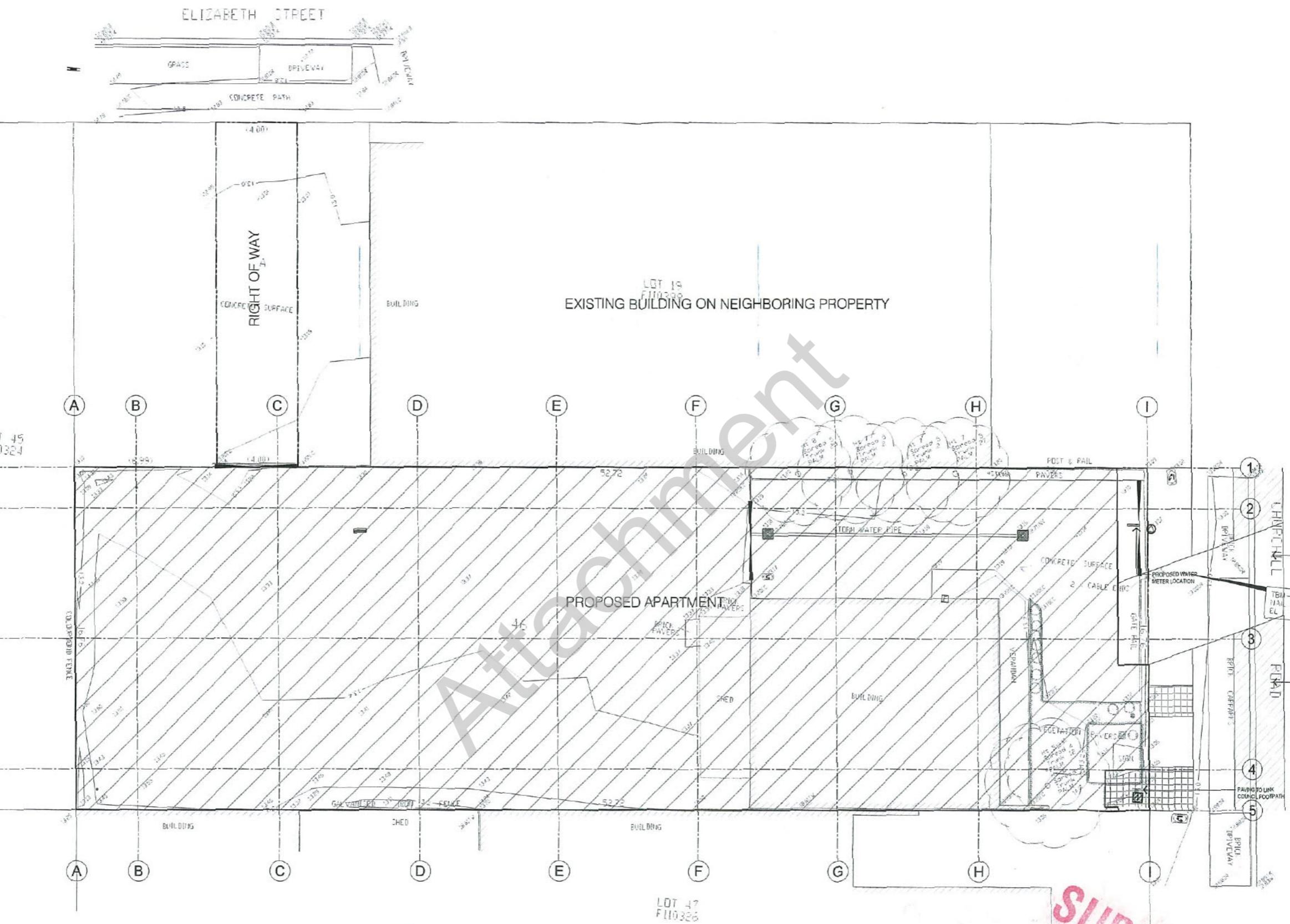
FIRST FLOOR AREAS	
UNIT 1 APARTMENTS:	65.8m <sup>2</sup>
BALCONY:	11.8m <sup>2</sup>
UNIT 2-6 APARTMENTS:	53.8m <sup>2</sup>
BALCONY:	9.8m <sup>2</sup>
TERACE:	8.8m <sup>2</sup>
UNIT 7 APARTMENTS:	55.8m <sup>2</sup>
BALCONY:	31.8m <sup>2</sup>

TOTAL FIRST FLOOR AREAS	
TOTAL COMMON:	156.8m <sup>2</sup>
TOTAL APARTMENTS:	405.8m <sup>2</sup>
TOTAL BALCONY:	139.8m <sup>2</sup>
TOTAL TERRACE:	48.8m <sup>2</sup>
TOTAL FLOOR:	600.4m <sup>2</sup>

SECOND / THIRD FLOOR AREAS	
UNIT 1-5 APARTMENTS:	10.8m <sup>2</sup>
BALCONY:	15.8m <sup>2</sup>
UNIT 6-10, N-20 APARTMENTS:	66.8m <sup>2</sup>
BALCONY:	10.8m <sup>2</sup>
UNIT 11-21 APARTMENTS:	50.8m <sup>2</sup>
BALCONY:	21.8m <sup>2</sup>

SECOND / THIRD FLOOR AREAS	
TOTAL COMMON:	104.8m <sup>2</sup>
TOTAL APARTMENTS:	452.8m <sup>2</sup>
TOTAL BALCONY:	82.8m <sup>2</sup>
TOTAL FLOOR:	650.4m <sup>2</sup>

TOTAL BUILDING AREAS	
GROUND FLOOR:	60.8m <sup>2</sup>
FIRST FLOOR:	424.8m <sup>2</sup>
SECOND FLOOR:	151.2m <sup>2</sup>
THIRD FLOOR:	151.2m <sup>2</sup>
TOTAL BUILDING:	707.2m <sup>2</sup>



SITE PLAN  
SCALE 1:100



**SUPERSEDED**  
**AMENDED PLAN**

SITEWORKS, DRAINAGE AND LEVELS TO BE AS PER ENGINEERS DESIGN AND DETAIL

BUILDER TO CHECK AND CONFIRM ALL DIMENSIONS PRIOR TO COMMENCEMENT OF CONSTRUCTION

## NOTES

ALL STRUCTURAL WALLS INTERNAL & EXTERNAL TO ACHIEVE MINIMUM 90mm TFR FROM BOTH SIDES

ALL PENETRATIONS THROUGH FIRE RATED WALLS MUST BE TREATED IN ACCORDANCE WITH BCA CP2 & CP4

PROVIDE FIRE COLLARS TO FLOORING / CEILING PENETRATIONS (EXCLUDING WIRING)

UNDER FLOOR PLUMBING TO BE LAGGED TO MAINTAIN SOUND SEPARATION

EACH APARTMENT ENTRY DOOR TO BE -143/19 FIL SELV CLOSING FIRE DOOR IN ACCORDANCE WITH AS 1905.1 & BCA D2.2

EXIT DOORS TO BE OPERABLE IN ACCORDANCE WITH BCA D2.2 (INCLUDES EXTERNAL GATES)

HWS STORAGE TO BE MINIMUM 5 STAR RATING FITTED WITH THERMOSTAT TIMER

FIRE RATED PLASTERBOARD TO ALL EXTERNAL WINDOWS / DOOR REVEALS (S6 SET NO TIMBER REVEALS)

VISUAL INDICATORS ON GLAZING IN ACCORDANCE WITH AS 1926.1-2009

ALL BEDROOM AWNING WINDOWS ON THE FIRST & SECOND FLOOR TO HAVE RESTRICTED WINDERS TO A MAX OF 125mm OPENING

ALL ARTIFICIAL LIGHTING TO MEET REQUIREMENTS OF AS 1929

EXT SONE TO BE LOCATED IN COMMON STAIRWELL ON EACH LANDING IN ACCORDANCE WITH AS 2931

ALL REQUIRED SELF CONTAINED SMOKE ALARMS TO BE STRICTLY IN ACCORDANCE WITH AS 3760

STAIRS TO STRICTLY COMPLY WITH BCA REQUIREMENTS  
MINIMUM CLEARANCE 2000mm  
MAXIMUM RISER - 190mm  
MAXIMUM GONG - 240mm  
MAXIMUM GONG - 355mm  
MANDATORY TO BE 1000mm ABOVE FIL TO HAVE A MAX OF NO GREATER THAN 120mm BEING INSTALLED IN ACCORDANCE WITH PART D2-CLAUSE D2.16 OF THE BCA

PIPE HYDRANT SYSTEM TO BE INSTALLED IN ACCORDANCE WITH PART E1 - CLAUSE E1.3 OF THE BCA AND THE REQUIREMENTS OF AS 1901



AMENDMENTS	REV	DATE	DESCRIPTION	WHICH
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A	20/10/14	PRELIMINARY		TL



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REF ID: 00000000000000000000000000000000

ADDRESS: 100 CHURCHILL RD, PROSPECT 5082

FILE: SITE PLAN

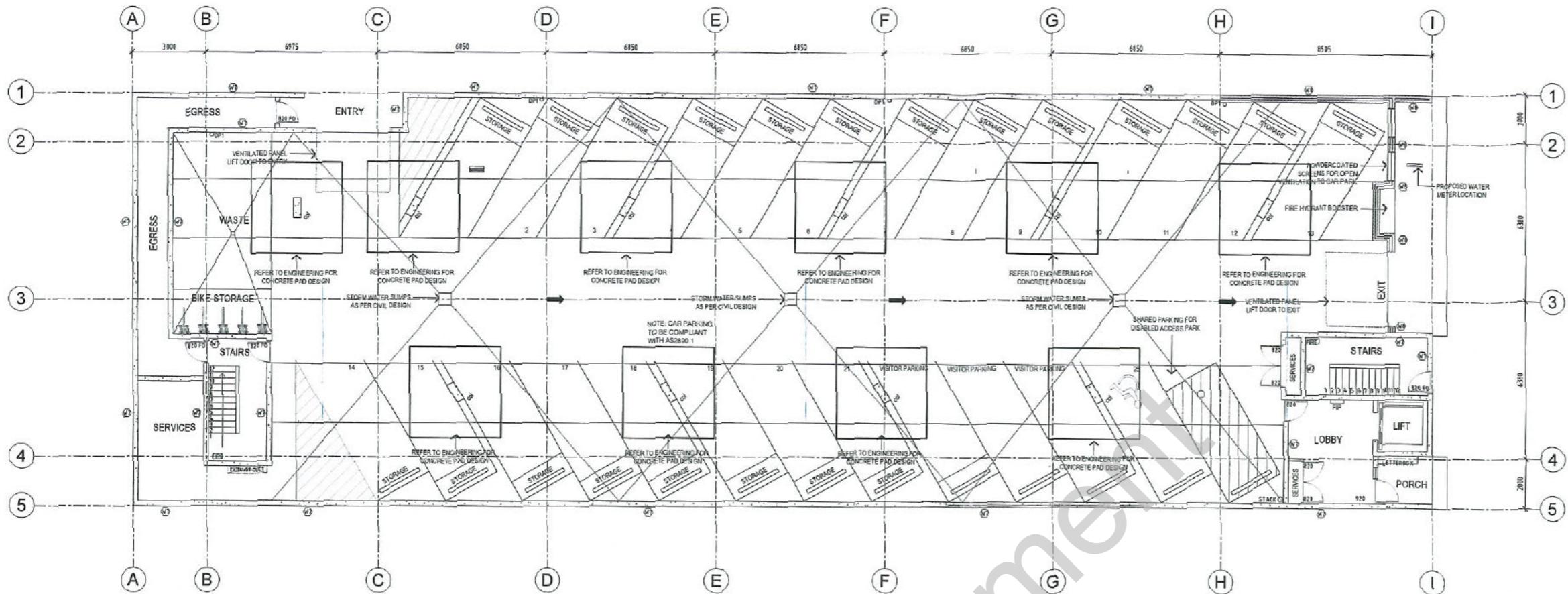
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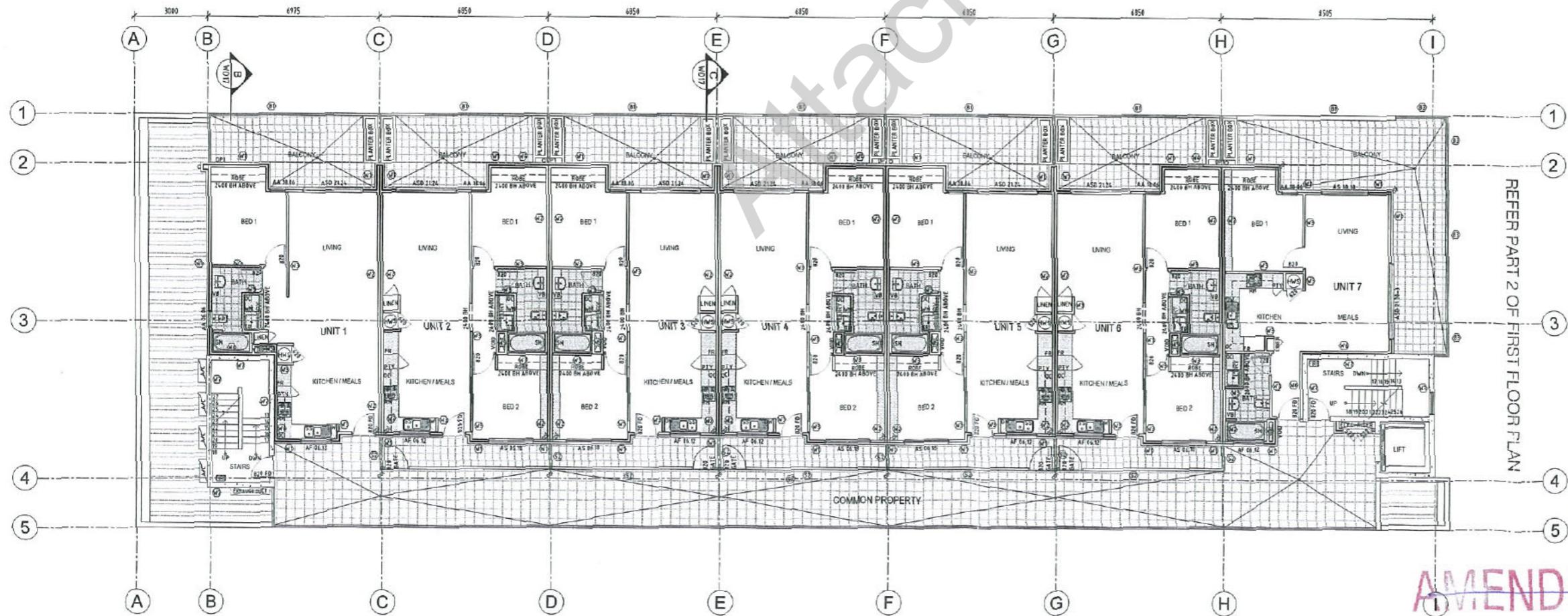
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GROUND FLOOR PLAN

SCALE 1:100



FIRST FLOOR PLAN

SCALE 1:100

## WALL LEGEND

## LEGEND

- WALL TYPE 1 - CSR HEBEL EXTERNAL WALL  
FIRE RATED WALL - 90min FRC  
REFER TO ENGINEER FRAMING DOCUMENTATION  
75mm CSR HEBEL POWERFRAME SCREW FIRED TO 25MM BATTENS  
INTERNAL SIDE TO BE 90mm TIMBER STUD FRAME  
LINED WITH 2 LAYERS OF 13mm FIRESTOP PLASTERBOARD WITH 20MM GAP  
APPLIED TO THE LAYERS OF 13mm FIRESTOP PLASTERBOARD  
PAINT FINISH
- WALL TYPE 2 - PARTY WALL SYSTEM  
FIRE RATED WALL - BORAL TT2429 - 90min FRC  
REFER TO ENGINEER FRAMING DOCUMENTATION  
2 ROWS 90mm TIMBER STUDS 649 MAX CENTRES WITH 20mm GAP  
LINED WITH 2 LAYERS OF 13mm FIRESTOP PLASTERBOARD  
PAINT FINISH
- WALL TYPE 3 - 90mm TIMBER STUD INTERNAL  
REFER TO ENGINEER FRAMING DOCUMENTATION  
LINED BOTH SIDES WITH 13mm PLASTERBOARD  
PAINT FINISH
- WALL TYPE 4 - LIGHTWEIGHT EXTERNAL WALL  
FIRE RATED WALL - BORAL TT2429 - 90min FRC  
REFER TO ENGINEER FRAMING DOCUMENTATION  
90mm TIMBER STUD FRAME LINED INTERNALLY WITH 2 LAYERS OF  
13mm FIRESTOP & EXTERIOR WITH 1 LAYER OF 13mm FIRESTOP PLASTERBOARD WITH APPROVED  
BARKING WITH 90mm JAMES HARDIE 'SCYON AXXON' SMOOTH  
TEXTURE 133mm WIRE GROOVE SPACING  
PAINT FINISH
- WALL TYPE 5 - 90mm TIMBER STUD INTERNAL  
REFER TO ENGINEER FRAMING DOCUMENTATION  
LINED WITH 2 LAYERS OF 13mm FIRESTOP PLASTERBOARD  
PAINT FINISH
- WALL TYPE 6 - STAR - 90mm TIMBER STUD INTERNAL  
REFER TO ENGINEER FRAMING DOCUMENTATION  
LINED WITH 2 LAYERS OF 13mm FIRESTOP PLASTERBOARD  
PAINT FINISH
- WALL TYPE 7 - PRECAST CONCRETE PANEL  
SEE ENGINEERS DOCUMENTS FOR DESIGN AND DETAIL
- WALL TYPE 8 - 90mm FACE BRICK WITH 90mm CAVITY WITH 90mm  
BLOCK WORK AS PER ENGINEERS DESIGN AND DETAIL INTERNAL  
FACE TO BE TEXTURE COAT FINISH
- BALUSTRADE 1 - 100mm HIGH CSR HEBEL WALL  
POWERFRAME SCREW FIRED TO 25MM BATTENS  
INTERNAL & SIDES TO BE 90mm TIMBER STUD FRAME  
LINED WITH 90mm JAMES HARDIE 'SCYON AXXON' SMOOTH TEXTURE  
133mm WIRE GROOVE SPACING
- BALUSTRADE 2 - 100mm HIGH SELECTED STEEL BALUSTRADE  
WITH FIXINGS AND INSTALLATION AS PER MANUFACTURERS SPECIFICATIONS
- BALUSTRADE 3 - 100mm HIGH SELECTED POWDER COATED  
BALUSTRADE WITH FIXINGS AND INSTALLATION AS PER MANUFACTURERS SPECIFICATIONS
- SCREEN 1 - FULL LENGTH LOUVRED SCREEN  
WITH FIXINGS AND INSTALLATION AS PER MANUFACTURERS SPECIFICATIONS
- SCREEN 2 - 1400mm POWDER COATED SLAT SCREEN  
WITH FIXINGS AND INSTALLATION AS PER MANUFACTURERS SPECIFICATIONS

## NOTES

ALL STRUCTURAL WALLS INTERNAL &  
EXTERNAL TO ACHIEVE MINIMUM  
90min FRC FROM BOTH SIDES

ALL PENETRATIONS THROUGH FIRE  
RATED WALLS MUST BE TREATED IN  
ACCORDANCE WITH BCA CP2 & CP8

PROVIDE FIRE COLLARS TO FLUORING /  
CEILING PENETRATIONS (EXCLUDING  
WIRING)

UNDER FLOOR PLUMBING TO BE LAGGED  
TO MAINTAIN SOUND SEPARATION

EACH APARTMENT ENTRY DOOR TO BE  
-1800x900mm FREE CLOSING FIRE DOOR  
IN ACCORDANCE WITH AS 1905.1 & BCA  
D2.23

EXIT DOORS TO BE OPERABLE IN  
ACCORDANCE WITH BCA CP2 (INCLUDES  
EXTERNAL GATES)

HWS STORAGE TO BE MINIMUM 5 STAR  
RATING FITTED WITH THERMOSTAT  
THER

FIRE RATED PLASTERBOARD TO ALL  
EXTERNAL WINDOWS / DOOR REVEALS  
(50mm SET BACK THERMISTOR REVEALS)

VISUAL INDICATORS ON GLAZING IN  
ACCORDANCE WITH AS1428.1-2009

ALL BEDROOM ANGLING WINDERS ON  
THE GROUND & SECOND FLOOR TO HAVE  
RESTRICTED WINDERS TO A MAX OF  
125mm OPENING

ALL ARTIFICIAL LIGHTING TO MEET  
REQUIREMENTS AS 1600

EXIT SIGNS TO BE LOCATED IN COMMON  
STAIRWELL ON EACH LANDING IN  
ACCORDANCE WITH AS 2293.1

ALL REQUIRED SELF CONTAINED SHOKE  
ALARMS TO BE STRICTLY IN  
ACCORDANCE WITH AS 3786

STAIRS TO STRICTLY COMPLY WITH  
BCA REQUIREMENTS

MINIMUM RISER - 150mm

MAXIMUM RISER - 150mm

MINIMUM GOING - 240mm

MAXIMUM GOING - 240mm

HANDRAILS TO BE 900mm ABOVE FFL TO  
HAVE A GAP OF NO GREATER THAN  
125mm AND BE INSTALLED IN  
ACCORDANCE WITH PART D2-(CLAUSE  
D2.16 OF THE BCA) THE SYSTEM TO BE INSTALLED  
IN ACCORDANCE WITH PART D1- CLAUSE  
D1.16 OF THE BCA AND THE REQUIREMENTS  
OF AS 2141.1

THE STAIR SYSTEM TO BE INSTALLED  
IN ACCORDANCE WITH PART D1- CLAUSE  
D1.16 OF THE BCA AND THE REQUIREMENTS  
OF AS 2141.1

DEVELOPMENT ACT 1993  
CITY OF PROSPECT  
CITY OF PROSPECT  
CONSENT  
APPROVED DOCUMENTS  
Subject to Conditions  
- 8 DEC 2014

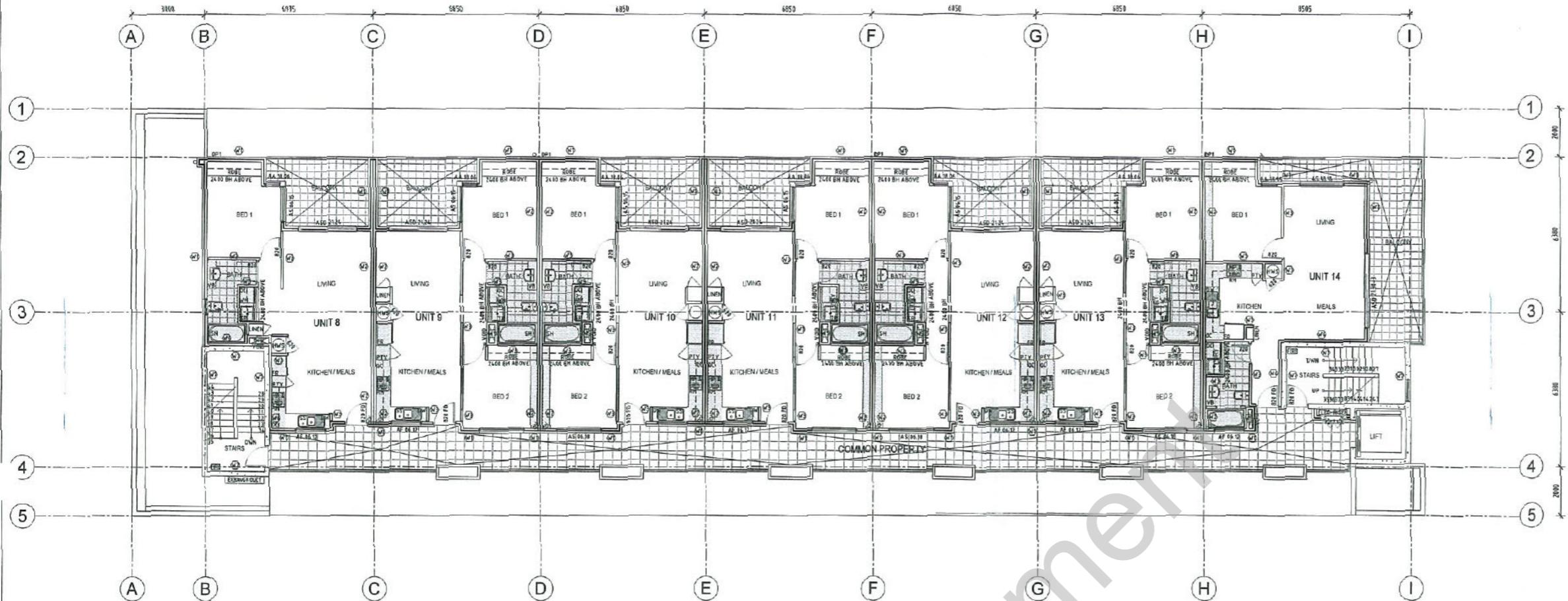
PRELIMINARY  
20/10/14

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AMENDMENTS		
REV	DATE	DESCRIPTION
G		
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A	20/10/14	PRELIMINARY
TL		
SUPERSEDED		
AMENDED PLAN		
GROUND / FIRST FLOOR PLAN		
DRAWING NUMBER 02 of 06		
DRAWING SCALE A1		
JOB NUMBER 11362		
DRAWING SCALE AS SPECIFIED		
CLIENT INITIALS: A		

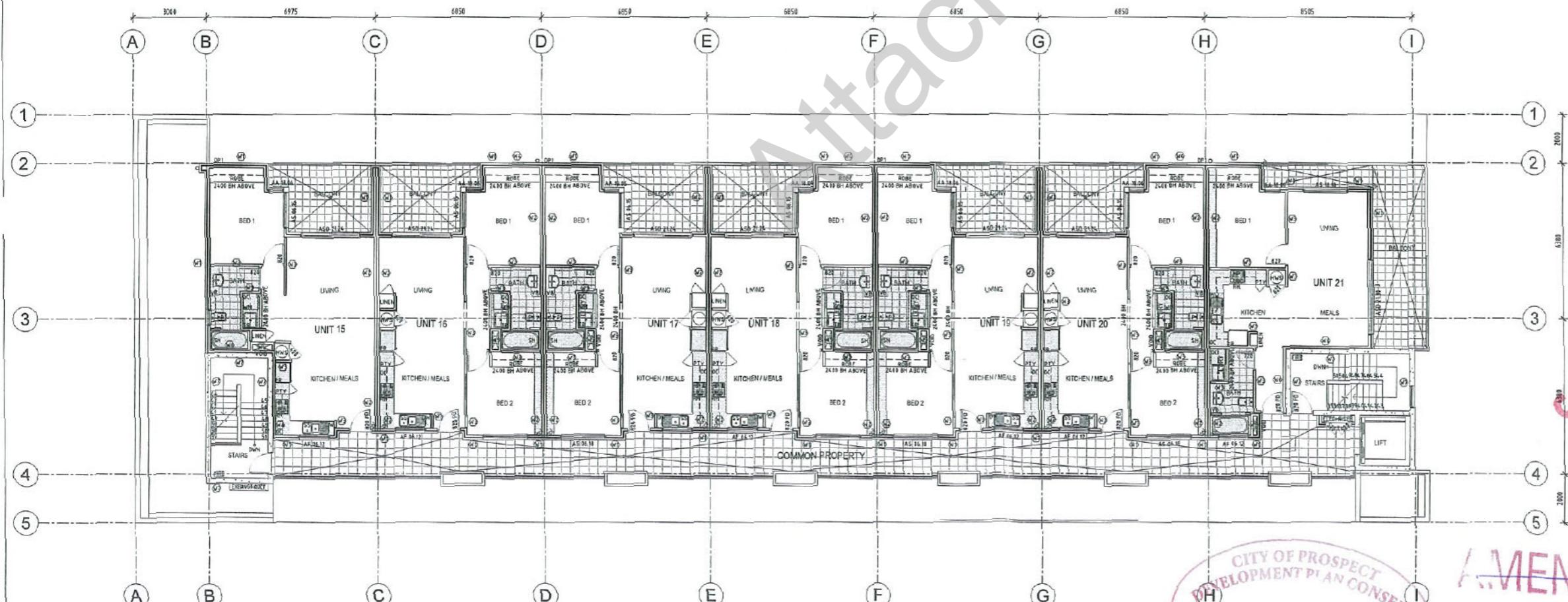
### WALL LEGEND

EXTERNAL WALL	P	TOILET PAN
IS	VB	VANITY BASIN WITH ASSOCIATED JEWELLERY UNIT
UMENTATION	SH	SHOWER
EL SCREW FIxed TO 25MM BATTENS	PR	REFRIGERATOR
CENTRES	DW	DISHWASHER
TIMBER STUD FRAME	SS	STAINLESS STEEL SINK
nn FIRESTOP PLASTERBOARD WITHIN	HP &	HOT PLATE & UNDER BENCH
L SYSTEM	UBO	QUES.
1126SF - 99/98/99	PTV	PANTRY
UMENTATION	RN	RANGE HOOD VENTED TO ATMOSPHERE
650 MAX CENTRES WITH 26mm GAP	DE	OVER HEAD CUPBOARDS
nn FIRESTOP PLASTERBOARD	WH	WASHING MACHINE
R STUD INTERNAL	TR	INSET TUMBLE
UMENTATION	BH	DRAIKER
IN PLASTERBOARD	DF1	150mm DIA. PAINTED PVC DOWNPipe, WITH LEAF GUARD, SUPPORTED BY STAND OFF BRACKETS
IT EXTERNAL WALL	DP2	90mm DIA. PAINTED PVC DWARPIPE, WITH LEAF GUARD, SUPPORTED BY STAND OFF BRACKETS
IS	FD	FIRE DOOR
UMENTATION	FPE	FIRE HYDRANT - REFER DETAILS
ED INSTRUMENTALLY WITH 2 LAYERS OF	LDY	LAUNDRY
P PLASTERBOARD WITH APPROVED	HR	CONTINUOUS HAND RAIL 1000mm AFTL
HARDIE SCYON AXON SMOOTH	RH	600 x 600 ROOF ACCESS HATCH TO MANUFACTURERS DETAILS
VE SPACING		EXTENT OF 2400 CEILING LEVEL
nn TIMBER STUD INTERNAL		 BOX GUTTER SUMP
IS	DP1	150mm DIA. PAINTED PVC DWARPIPE, WITH LEAF GUARD, SUPPORTED BY STAND OFF BRACKETS
UMENTATION	DP2	90mm DIA. PAINTED PVC DWARPIPE, WITH LEAF GUARD, SUPPORTED BY STAND OFF BRACKETS
13mm OF FIRESTOP PLASTERBOARD	RWH	SELECTED COLORBOND GUTTERHEAD WITH OVERFLOW OUTLET
CONCRETE PANEL	BG1	450mm x 1000 GL BOX GUTTER SUPPORTED ON 20mm HARDWOOD GUTTER BOARD & FLANGE TO EXTEND 200mm UP RAKE OF ROOF
FOR DESIGN AND DETAIL	BG2	300W x 1000 GL BOX GUTTER SUPPORTED ON 20mm HARDWOOD GUTTER BOARD & FLANGE TO EXTEND 200mm UP RAKE OF ROOF
CH CSM CEBEL WALL	EJ	EXPANSION JOINT
EL SCREEN FIxed TO 25MM BATTENS	OF	OVERFLOW
CENTRES		 DIRECTION OF ROOF FALL & PITCH
nn TIMBER STUD FRAME	RH	ROOF ACCESS WITH LADDER INSTALLED TO MANUFACTURERS SPECIFICATION
SCYON AXON SMOOTH TEXTURE	MWS	HOT WATER SYSTEM
NG	FD	FIRE DOOR
CH SELECTED STEEL BALUSTRADE	DC	OVER HEAD CUPBOARDS
ATION AS PER MANUFACTURERS	WH	WASHING MACHINE
CH SELECTED POWDER COATED	TR	INSET TUMBLE
AND INSTALLATION AS PER	LOV	LAUNDRY
ATIONS		 EXTENT OF 2400 CEILING LEVEL
UNIVERSAL SCREEN		
ATION AS PER MANUFACTURERS		
COATED SLAT SCREEN		
ATION AS PER MANUFACTURERS		



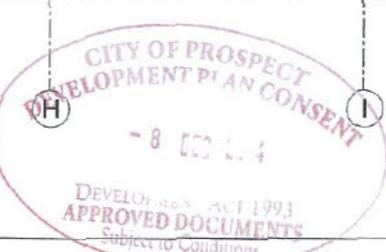
## SECOND FLOOR PLAN

SCALE 1:100



### THIRD FLOOR PLAN

SCALE 1:100



DEVELOPMENT ACT 1993  
APPROVED DOCUMENTS  
Subject to Conditions

~~AMENDED PLAN~~

AMENDMENTS		DESCRIPTION	WHO
REV	DATE		
G			
F			
E			
D			
C			
B			

A 2010/14 PRELIMINARY

Level 1, 661 McBee Hwy  
Glenrothes, SK S0A 5L0  
www.qldsoft.com.au

ADDRESS  
100 CHURCHILL RD.

PROSPECT 5082

NAME \_\_\_\_\_

**SECOND / THIRD FLOOR PL**

03 of 16 A1

JOB NUMBER DRAWING SCALE  
11362 AS SPECIFIED

**CLIENT INITIALS:**

—

**SITE NOTES**

THIS DRAWING IS TO BE READ IN CONJUNCTION WITH ALL OTHER DRAWINGS, DOCUMENTATION, SCHEDULES AND SPECIFICATIONS.

BUILDER TO CHECK AND CONFIRM ALL SITE AND SET OUT DIMENSIONS PRIOR TO COMMENCEMENT OF CONSTRUCTION.

WRITTEN DIMENSION TO BE TAKEN IN PREFERENCE TO SCALE.

ALL CONSTRUCTION TO COMPLY WITH RELEVANT AUSTRALIAN BUILDING CODES AND STANDARDS.

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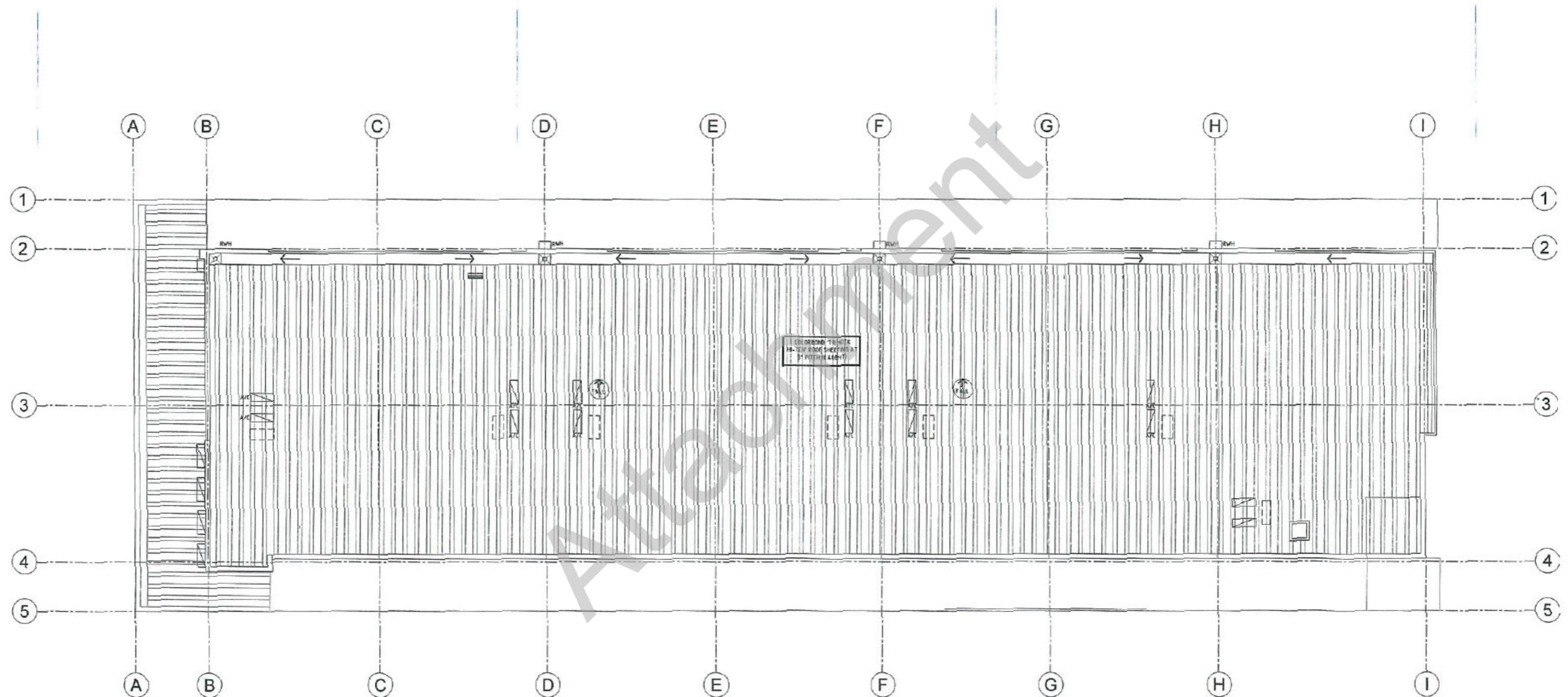
NO VARIATION MAY BE MADE TO THIS DRAWING WITHOUT PRIOR APPROVAL OF THE PROPRIETOR OR DESIGNERS.

REFER TO ENGINEERS DESIGN, DOCUMENTATION, CALCULATION AND SPECIFICATION FOR STRUCTURAL, ELECTRICAL, HYDRAULIC AND CIVIL DETAILS IF APPLICABLE.

STORMWATER TO BE DISCHARGED TO STREET WATERTABLE OR RAINWATER TANK IN ACCORDANCE WITH COUNCIL REQUIREMENTS AND/OR DIRECTION.

SURFACE WATER RUN-OFF FROM NOT LESS THAN 500sqm OF ROOF CATCHMENT AREA TO BE STORED IN 1000L RAIN WATER TANK AND PLUMBED TO EITHER A TOILET, WATER HEATER OR LAUNDRY COLD WATER AND, INLET/OVERFLOW ON RAIN WATER TANK MUST BE FITTED WITH INSPECT PROOF AND NON DEGRADABLE SCREENS.

ANY DISCREPANCIES IN DOCUMENTS AND/OR ON SITE TO BE REPORTED TO THE DESIGNER BEFORE ANY WORK IS COMMENCED.



ROOF PLAN  
SCALE 1:100

AMENDMENTS			
REV. DATE	DESCRIPTION	WHO	
G			
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E			
D			
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A	2011/07/14 PRELIMINARY		TL

**PRELIMINARY**  
2011/07/14

**CITY OF PROSPECT**  
**DEVELOPMENT PLAN CONSENT**  
~ 8 DEC 2014

**DEVELOPED ON 11/07/2014**  
**APPROVED DOCUMENTS**  
Subject to Conditions

**QATTRO**

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**ADDRESS**  
100 CHURCHILL RD,  
PROSPECT 3008

**FILE NUMBER**  
04 of 06 A1

**ISSUE NUMBER**  
11362 AS SPECIFIED A

**CURRENT INITIALS**

**SUPERSEDED PLAN**  
**AMENDED PLAN**

## NOTES

ALL STRUCTURAL WALLS INTERNAL & EXTERNAL TO ACHIEVE MINIMUM 50mm FRL FROM BOTH SIDES

ALL PENETRATING THROUGH FIRE RATED WALLS MUST BE TREATED IN ACCORDANCE WITH BCA (P2 & CP8)

PROVIDE FIRE COLLARS TO FLOORING / CEILING PENETRATIONS (EXCLUDING MIRNING)

UNDER FLOOR PLUMBING TO BE LAGGED TO MAINTAIN SOUND SEPARATION

EACH APARTMENT ENTRY DOOR TO BE 1.18m x 1.9m FRL SELF CLOSING FIRE DOOR IN ACCORDANCE WITH AS 1905.1 & BCA D2.23

EXIT DOORS TO BE OPERABLE IN ACCORDANCE WITH BCA D2.21 INCLUDES EXTERNAL GATES

HWS SURFACE TO BE MINIMUM 5 STAR RATING FITTED WITH THERMOSTAT TIMER

FIRE RATED PLASTERBOARD TO ALL EXTERNAL WINDOWS / DOOR REVEALS (SOFT SET TIMBER REVEALS)

VISUAL INDICATORS ON GLAZING IN ACCORDANCE WITH AS/NZS 2615-2009

ALL EGRESS ANNING WINDOWS ON THE FIRST & SECOND FLOOR TO HAVE RESTRICTED WINDERS TO A MAX OF 125mm OPENING

ALL ARTIFICIAL LIGHTING TO MEET REQUIREMENTS OF AS 1488

EXIT SIGNS TO BE LOCATED IN COMMON STAIRWELL EACH LANDING IN ACCORDANCE WITH AS 2293.1

ALL REQUIRED SELF CONTAINED SMOKE ALARMS TO BE STRICTLY IN ACCORDANCE WITH AS 3706

STAIRS TO STRICTLY COMPLY WITH BCA REQUIREMENTS

MINIMUM RISER - 150mm

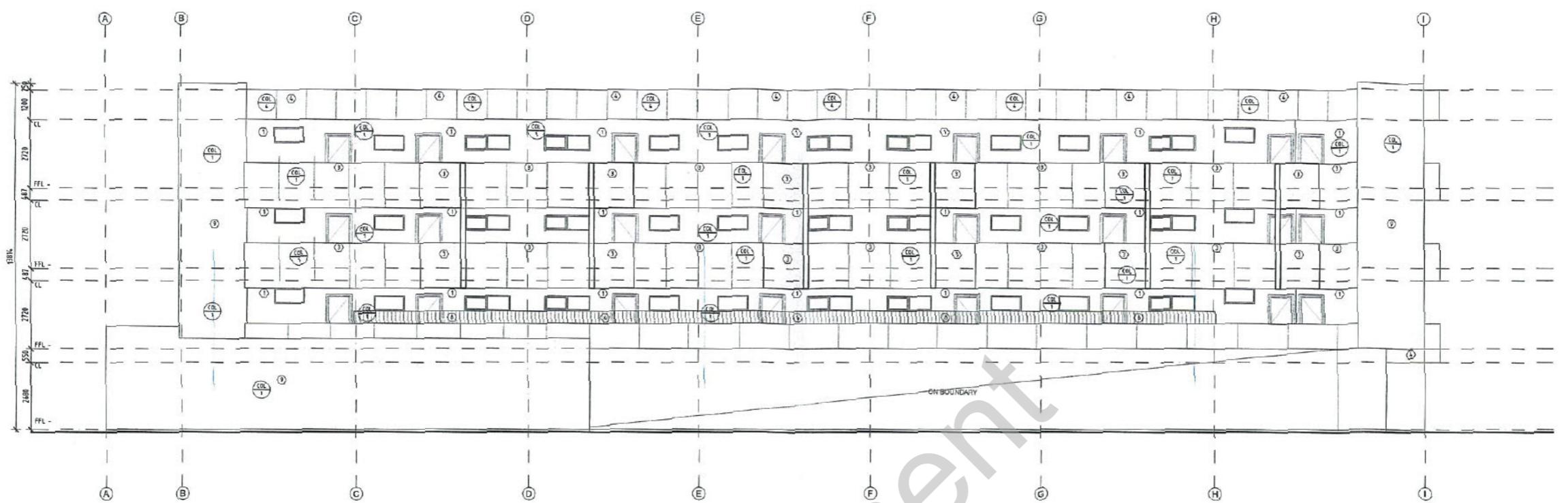
MAXIMUM RISER - 190mm

MINIMUM DEPTH - 240mm

MAXIMUM DEPTH - 350mm

HANDRAIL TO BE 900mm ABOVE FFL TO HAVE A GAP OF NO GREATER THAN 12mm AND BE INSTALLED IN ACCORDANCE WITH PART D2-CLAUSE 2.7.1.1

SHOWER SYSTEM TO BE INSTALLED ACCORDING WITH PART E5 - CLAUSE 1.1.1 OF THE BCA AND THE REQUIREMENTS OF AS 2615



SOUTH ELEVATION

SCALE 1:100

## WALL LEGEND

① REBEL POWERANEL CLADDING WITH APPROVED ACRYLIC TEXTURE COATING SYSTEM OR SIMILAR PAINT FINISH

② EXTERNALLY LINING WITH 50mm JAMES HARVEY SOFT SET ANGIN SHROPSHIRE TEXTURE 13mm WIDE GROOVE SPACING WITH APPROVED PAINT FINISH WITH 2x LAYERS OF 15mm PYRCHEK & APPROVED LINING IN EXTERIOR ZONE - INTERNALLY LINING WITH 2x LAYERS OF 15mm PYRCHEK.

③ REBEL POWERANEL CLADDING WITH APPROVED ACRYLIC TEXTURE COATING SYSTEM OR SIMILAR PAINT FINISH

④ ALUMINUM COMPOSITE PANEL FOR EXTERIOR CLADDING, COMPLETE WITH ALL ANCHORS, HARDWARE & FITTINGS TO PROVIDE A TOTAL INSTALLATION TO CANOPY & SOFFIT INSTALLED TO MANUFACTURERS SPECIFICATION

⑤ EXTERIOR CLADDING WITH 15mm PYRCHEK & APPROVED FLASHINGS, FLASHINGS ETC BY APPROVED CLADDING SPECIALIST SUBCONTRACTOR IN ACCORDANCE WITH MANUFACTURERS RECOMMENDATIONS

⑥ POWDER COATED BALUSTRADE AND SCREENS TO BALCONIES AS PER APPROVED PATTERN

⑦ METAL LOUVED SCREEN WITH POWDERCOATED FINISH INSTALLED TO MANUFACTURERS SPECIFICATION

⑧ RENDERED FOAM PANEL WITH APPROVED TEXTURE FINISH INSTALLED TO MANUFACTURERS SPECIFICATION

⑨ STANDARD FACE BRICK AS PER SELECTIONS WITH STANDARD GRAY MORTAR.

⑩ CONCRETE PRECAST PANEL WITH APPROVED ACRYLIC TEXTURE COATING SYSTEM OR SIMILAR PAINT FINISH

NOTE: ALL WET AREA WINDOWS TO HAVE OBFUSCATE GLAZING

## WALL COLOUR LEGEND

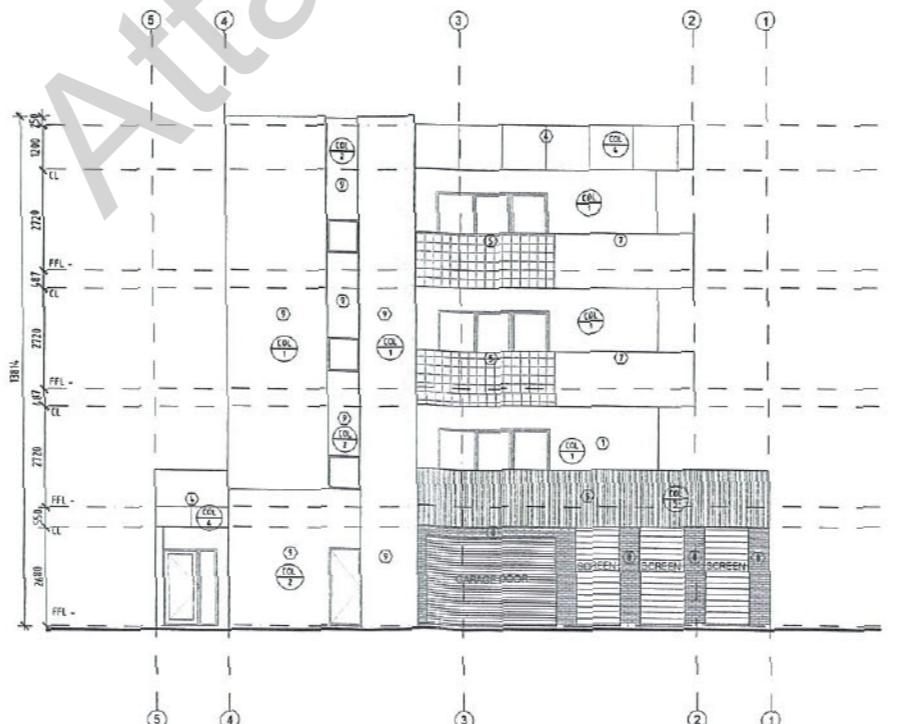
⑪ DULUX - GREY PEBBLE HALF PM/BN MATT RENDER

⑫ DULUX - RAKU PEZET FEATURE WALL

⑬ DULUX - BURNT ORANGE

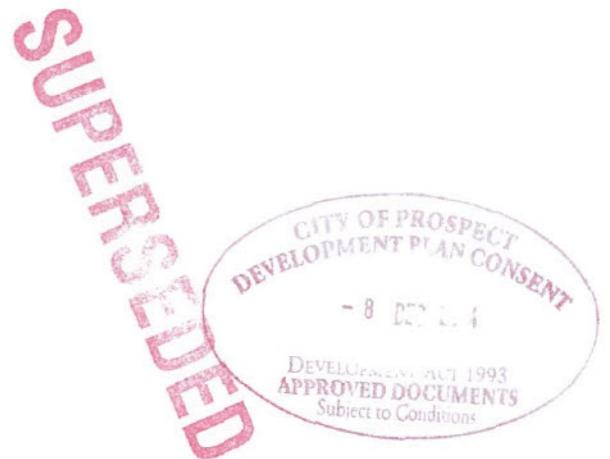
⑭ DULUX - ORANGE

NOTE: ALL SOFFIT LININGS INCLUDING EAVES, GUTTERS & CAMPIONS TO BE PAINTED - ENAMEL WHITE 6329 UNLESS OTHERWISE NOTED



EAST ELEVATION

SCALE 1:100



AMENDED PLAN

PRELIMINARY  
26/10/14

AMENDMENTS	REV	BACS	DESCRIPTION	WHO
G				
F				
E				
D				
C				
B				
A	20/10/14	PRELIMINARY		TL



Level 1, 47 Anzac Hwy  
Glenelg, SA 5045  
Phone: 08 8200 0000  
Fax: 08 8200 0000  
Email: info@qattro.com.au

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ADDRESS:  
100 CHURCHILL RD,  
PROSPECT 5082

TITLE:  
ELEVATIONS  
DRAWN BY: A1  
DRAFTED BY: A1  
JOB NUMBER: 11362  
DRAWING SCALE: AS SPECIFIED  
REVISION: A

CUSTOM INITIALS:

## NOTES

ALL STRUCTURAL WALLS INTERNAL & EXTERNAL TO ACHIEVE MINIMUM 92/95/97 FRL FROM BOTH SIDES

ALL PENETRATIONS THROUGH FIRE RATED WALLS MUST BE TREATED IN ACCORDANCE WITH BCA (P2 & P8)

PROVIDE FIRE COLLARS TO FLOORING / CEILINGS PENETRATIONS (EXCLUDING MINERAL)

UNDER FLOOR PLUMBING TO BE LAGGED TO HANTAN SOUND SEPARATION

EACH APARTMENT ENTRY DOOR TO BE 1A/2/39 FRL SELF CLOSING FIRE DOOR IN ACCORDANCE WITH AS 1951 & BCA (P2)

EXIT DOORS TO BE OPERABLE IN ACCORDANCE WITH BCA (P2) INCLUDES EXTERNAL GATES

HWS STORAGE TO BE MINIMUM 5 STAR RATING FITTED WITH THERMOSTAT TIMER

FIRE RATED PLASTERBOARD TO ALL EXTERNAL WINDOWS / DOOR REVEALS (50 SET NO HUMMER REVEALS)

VISUAL INDICATORS ON GLAZING IN ACCORDANCE WITH AS 1281-2009

ALL REFRIGERATOR WINDOWS ON THE FIRST & SECOND FLOOR TO HAVE RESTRICTED WINDERS TO A MAX OF 125mm OPENING

ALL ARTIFICIAL LIGHTING TO MEET REQUIREMENTS OF AS/NZS

EXIT SIGNS TO BE LOCATED IN COMMON STAIRWELL ON EACH LANDING IN ACCORDANCE WITH AS 2293.1

ALL REQUIRED SELF CONTAINED SMOKE ALARM TO BE STRICTLY IN ACCORDANCE WITH AS/NZS

STAIRS TO STRICTLY COMPLY WITH BCA REQUIREMENTS

MINIMUM RISER = 15mm

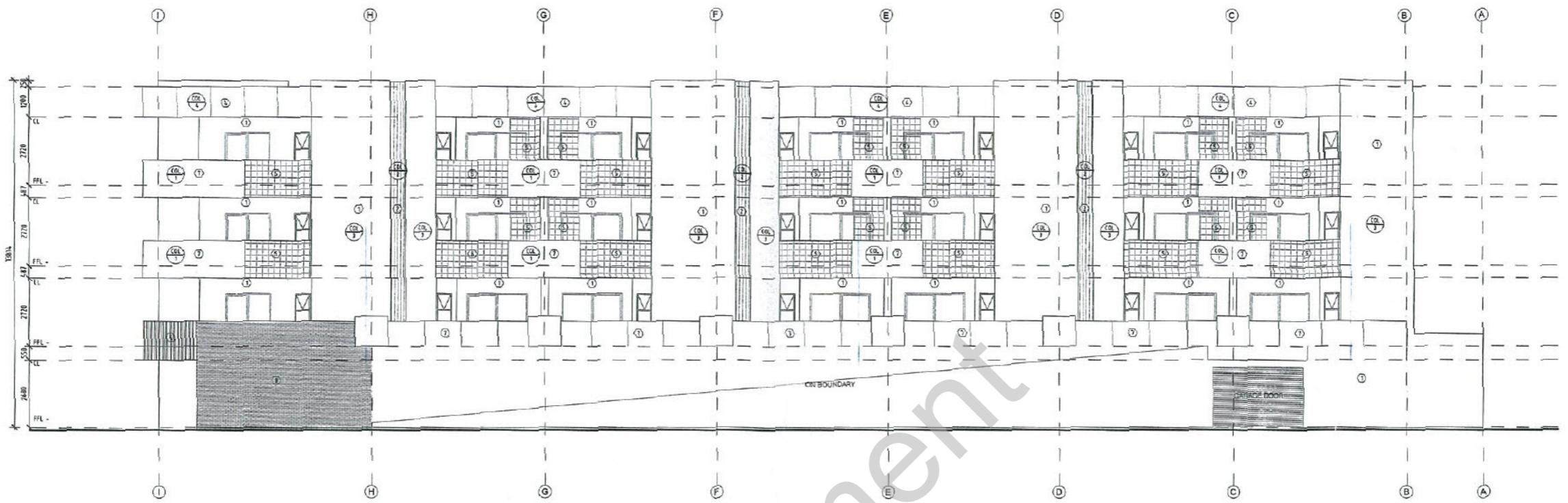
MAXIMUM RISER = 19mm

MINIMUM GOING = 25mm

MAXIMUM GOING = 35mm

HANDRAIL TO BE 105mm ABOVE FFL TO HAVE A GAP OF NO GREATER THAN 120mm AND BE INSTALLED IN ACCORDANCE WITH PART P2 - CLAUSE 12.1

WIRING SYSTEM TO BE INSTALLED ACCORDING TO PART E1 - CLAUSE 12.3 OF THE BCA AND THE REQUIREMENTS OF S24.1



NORTH ELEVATION

SCALE 1:100

## WALL LEGEND

(1) DULUX - POWER PANEL CLADDING WITH APPROVED ACRYLIC TEXTURE COATING SYSTEM OR SIMILAR PAINT FINISH

(2) EXTERIALLY LINER WITH 10mm JAMES HARDE SCYCLON AS/NC SMOOTH TEXTURE 150mm WIDE GROOVE SPACING WITH APPROVED PAINT FINISH WITH 2x LAYERS OF 10mm FVR/NC & INTERNALLY LINER WITH 2x LAYERS OF 10mm FVR/NC

(3) DULUX - POWER PANEL CLADDING WITH APPROVED ACRYLIC TEXTURE COATING SYSTEM OR SIMILAR PAINT FINISH

(4) ALUMINIUM COMPOSITE PANEL (OR similair cladding), COMPLETE WITH ALL NECESSARY SUB STRUCTURE, ANCHORS, HARDWARE & FITTINGS TO PROVIDE A TOTAL INSTALLATION TO CANOPY & SOFFIT INSTALLED TO EXISTING CONCRETE WALLS. CLADDING SYSTEM TO BE INSTALLED COMPLETE WITH MATCHING COPINGS, FLASHINGS ETC BY APPROVED CLADDING SPECIALIST SUBCONTRACTOR IN ACCORDANCE WITH MANUFACTURER'S RECOMMENDATIONS

(5) POWDER COATED BALUSTRADE AND SCREENS TO BALCONIES AS PER APPROVED DESIGN

(6) METAL LOUVED SCREEN WITH APPROVED PAINT FINISH INSTALLED TO MANUFACTURER'S SPECIFICATION

(7) RENDERED FOAM PANEL WITH APPROVED TEXTURE FINISH INSTALLED TO MANUFACTURER'S SPECIFICATION

(8) STANDARD FAKE BRICK JS PER SELECTIONS WITH STANDARD GRAY MORTAR

(9) CONCRETE PRECAST PANELS WITH APPROVED ACRYLIC TEXTURE COATING SYSTEM OR SIMILAR PAINT FINISH.

NOTE: ALL WET AREA WINDOWS TO HAVE OBFSCURE GLAZING

## WALL COLOUR LEGEND

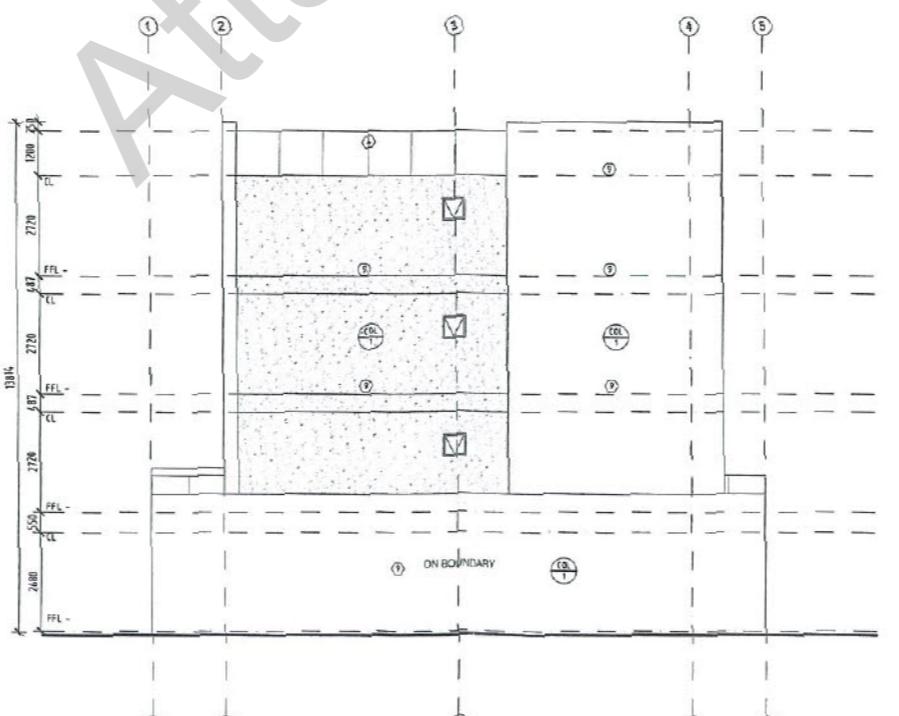
(10) DULUX - GREY PEBBLE HALF PEBBLE MAIN RENDER

(11) DULUX - RAKU PG27 FEATURE WALL

(12) DULUX - BURNT ORANGE

(13) DULUX - ORANGE

NOTE: ALL SOFFIT LINERS INCLUDING EAVES, PORCH & CANOPIES TO BE SILVER - SIMPLY WHITE SIZE 0000 UNLESS OTHERWISE NOTED



WEST ELEVATION

SCALE 1:100



PRELIMINARY  
20/10/14

AMENDMENTS			
REV	DATE	DESCRIPTION	WHO
G			
F			
E			
D			
C			
B			
A	20/10/14	PRELIMINARY	TL



ELEVATIONS		
DRAWING NUMBER	TRIMMED	REVISION
05 of 06	A1	
11362	AS SPECIFIED	A
		CLIENT INITIALS:

**CITY OF PROSPECT**

128 Prospect Road, Prospect SA 5082

Telephone (08) 8269 5355 Facsimile (08) 8269 5834

To: Hames Sharley (SA) Pty Ltd  
 GPO Box 1752  
 Adelaide SA 5001

**COPY****DECISION NOTIFICATION**Development Application: **050/467/2013**Dated: 15/11/2013  
 Registered: 18/11/2013

Location: 100 Churchill Road PROSPECT

Description: Five storey residential flat building comprising 24 dwellings

The following decisions have been made in respect of the development application:

NATURE OF DECISION	DETERMINATION	NO. OF CONDITIONS	DATE OF DECISION
Development Plan Consent	Granted	24	14/04/2014
Building Rules Consent	To be determined	-	-
Development Approval	To be determined	-	-

- Any conditions that have been imposed against the authorisation(s) granted herein and advisory notes that may be relevant to the development are detailed on the following page(s).
- The application was determined to be a **Category 1** application for the purpose of public notification. Nil representation(s) were received from third parties.
- The building classification assigned to the development under the Building Code is:
- The development authorisation (consent or approval) granted herein remains operative for a period of 12 months from the date of the decision.

Signed: ..... *[Signature]* ..... Chief Executive Officer DelegateDate: ..... *17/4/2014* .....

## Conditions and notes that apply to this authorisation

Where relevant to the ongoing maintenance or operation of the development to which this authorisation applies, the condition(s) identified herein will continue to apply unless or until varied or revoked by the relevant authority.

Any conditions detailed herein are binding on and enforceable against:

- the person by whom the development is undertaken;
- any person who acquires the benefit of the decision or the development; and
- the owners and occupiers of the land on which the development is undertaken.

### Conditions of Development Plan Consent

The following conditions apply to the Development Plan Consent. These conditions have been imposed in accordance with the *Development Act 1993* to ensure the development complies with the provisions of Council's Development Plan and relevant legislation:

1. The development shall take place in accordance with plans and details stamped by Council relating to Development Application Number 050467/2014, except as modified by any conditions detailed herein. All works detailed in the approved plans and required by conditions are to be completed prior to the occupation of the approved development.
2. All driveways, parking and manoeuvring areas must be formed, surfaced with concrete, bitumen or paving, and be properly drained. The surfacing of the driveway and drainage shall be maintained to the reasonable satisfaction of Council thereafter.
3. The drainage system shall be designed, installed and maintained at all times thereafter to ensure that water from the site does not:
  - a) Flow or discharge onto adjoining properties;
  - b) Flow across the surface of footpaths or public ways;
  - c) Affect the stability of any building; or
  - d) Create unhealthy or dangerous conditions on the site or within any building.
4. Air-conditioning units and solar hot water heaters shall be provided with screening devices designed to complement the colours, materials and finishes of the building approved herein, and shall be sited to adequately screen the units from view to the reasonable satisfaction of Council.
5. Driveways, car parking spaces, manoeuvring areas and landscaping areas shall not be used for the storage or display of materials or goods including waste products and refuse.
6. Permanently fixed clotheslines shall not be permitted within 1.5m of the edge of each balcony.
7. To maximise the efficiency of waste recycling:
  - a) Provision shall be made for the separation of recyclable materials for collection and recycling, including paper, cardboard, glass and plastic containers, tins, and any other plastic that 'holds its shape';
  - b) Separate provision shall be made for the collection of food waste (food organics) and food-contaminated cardboard, paper or paper products, which are to be collected for composting; and
  - c) Paper attached to plastic, wax paper or chemically-treated/gloss cardboard will not be included with the materials collected for composting.

8. Service vehicle movements shall occur outside of the peak periods associated with the use approved herein and in any event shall not occur before 6am or after 9pm on any day.
9. The building must be maintained, kept tidy, free of graffiti and in good repair and condition at all times.
10. All car parking spaces must be line-marked in accordance with the approved plans and to comply with the Australian/New Zealand Standard for Parking Facilities (Part 1: Off-street Car Parking (AS/NZS 2890.1:2004) prior to occupation.
11. The surfacing of the car park, line marking and directional arrows shall be maintained to the reasonable satisfaction of Council at all times.
12. All disabled parking spaces shall be provided with line marking and signage that accords with Australian/New Zealand Standard for Parking Facilities (Part 1: Off-street Car Parking (AS/NZS 2890.1:2004) and the car parking area shall be signposted as an area subject to the Private Parking Areas Act 1975.
13. Lighting to driveways, parking and manoeuvring areas shall be lit in accordance with the Australian Standard for Lighting for Roads and Public Spaces (AS1158.1 and AS1158.3) during the hours of darkness that they are in use and accessible by the general public.
14. Wheel-stops or similar non-mountable devices shall be provided to each car park to prevent damage to the building or vehicle movements thereon.
15. All landscaping areas shall be maintained at all times to the reasonable satisfaction of Council. The applicant or the persons for the time being making use of the subject land shall cultivate, tend and nurture the landscaping, and shall replace any landscaping that becomes diseased or dies. An automated drip irrigation or similar watering system shall be established and maintained to ensure that sufficient water is available to satisfy the needs of the landscaping species selected.
16. During construction of the development approved herein, measures will be implemented to ensure that the construction works do not result in an unreasonable impact on occupiers of adjacent properties or pollution of existing infrastructure through drag-out or stormwater runoff. Measures shall include as necessary:
  - a) A hard surface and controlled washing zone at the entry/exit points to the site, designed to reduce the potential for mud and material dragged out by construction vehicles; and
  - b) Containment of stormwater run-off within the site, which if being discharged into the stormwater system will be filtered to the satisfaction of Council; and
  - c) Reduction of the potential for dust and other airborne particles by the use of water sprinklers and/or other means of containment; and
  - d) The establishment of an appropriate storage compound for waste materials and litter. No building waste material shall be stored outside of the storage compound or similar industrial bin; and
  - e) All mechanical equipment shall be used in a manner to minimise the potential for noise pollution and ensure compliance with the requirements of the Environment Protection (Noise) Policy.
17. Footpaths adjacent to the site are to be kept in a safe condition for pedestrians at all times during construction works. All driveways and footpaths traversed by vehicles using the site are to be maintained in a reasonable condition for the duration of the works, and are to be reinstated to the satisfaction of Council on completion of the works.

No obstruction of the footpath or roadway may occur without the prior permission of Council. For further advice, please contact Council's Infrastructure and Environment Department on 8269 5355.

18. To ensure compliance with applicable standards as described in the Environment Protection (Noise) Policy established under the Environment Protection Act, construction activities shall only take place between the hours of 7:00am and 7:00pm, Monday to Saturday inclusive, and not on Sundays or public holidays.

***The following conditions have been imposed by the Department of Planning, Transport and Infrastructure in accordance with Section 37(7) of the Development Act 1993:***

19. The Churchill Road access point shall be a maximum of 4.0 metres in width at the property boundary.
20. The proposed crossover to Churchill Road shall be angled between 70-90 degrees to the road edge to reinforce the left-out only movements desired at this location.
21. The obsolete crossover and/or any portion of crossover that is not required for the subject development shall be reinstated to Council standard kerb and gutter at the applicant's cost prior to occupation of the completed development.
22. All vehicles shall enter and exit the site in a forward direction.
23. The access points shall be clearly signposted and line marked to assist in promoting the desired flow of traffic through the site.
24. No stormwater from this development shall be permitted to discharge on-surface to Churchill Road. In addition, any existing drainage of the roads is to be accommodated in the development and any alterations to road drainage infrastructure as a result of this development are to be at the expense of the applicant.

### **Advisory Notes**

The following advisory notes are provided for your information:

- (1) Pursuant to Section 86(1)(a) of the Development Act, 1993, you have the right of appeal to the Environment, Resources and Development Court against either 1) a refusal of consent or 2) any condition(s) which have been imposed on a consent. Any such appeal must be lodged with the Court within two (2) months from the day on which you receive this notification or such longer period as may be allowed by the Court.

The Environment, Resources and Development Court is located in the Sir Samuel Way Building, Victoria Square, Adelaide SA 5000 (Postal Address: GPO Box 2465, Adelaide SA 5001).

- (2) The development plan consent granted herein is effective for a period of twelve (12) months from the date of the decision. Unless Council extends this period, building rules consent is required within this time or the consent will lapse.

Any request for an extension of the operative period of the consent must be submitted to Council in writing, accompanied by the applicable fee.

- (3) The consent of the Commissioner of Highways is required under the Metropolitan Adelaide Road Widening Plan Act to all building work on or within 6.0 metres of the possible requirement. The applicant is to complete the 'Metropolitan Adelaide Road Widening Plan Act, 1972', and submit to the Department of Transport, Energy and Infrastructure, together with three copies of the approved plans.

**ITEM NO.:** 6.1

**TO:** Development Assessment Panel (DAP) on 15 June 2015

**FROM:** Director Community, Planning and Communications

**SUBJECT:** Summary of Development Assessment Commission (DAC) Decisions and Proposals Greater than \$3 Million called in by the Coordinator-General

The summary of matters before and decisions by DAC together with proposals called in by the Coordinator-General is provided to the DAP for information purposes.

For the purpose of this report, the table/s below also include matters before, considered or determined by the Inner Metropolitan Development Assessment Commission.

## 1. MATTERS BEFORE DAC

<b>Development Application / Address</b>	<b>Nature of development</b>	<b>Process update</b>
DA 050/052/15 20A Main North Road and 45 Carter Street, Thorngate	Integrated Service Station Complex (comprising demolition of existing petrol filling station and dwelling, and construction of a two storey petrol filling station, shop and restaurant building with associated underground fuel tanks, canopies, freestanding pylon sign, boundary wall and landscaping)	<p>The DAC considered the application at its meeting on 28 May 2015.</p> <p>The proposal as presented included several minor variations (refer <b>Attachments 1-3</b>) including:</p> <ul style="list-style-type: none"> <li>- the introduction of timber panelling to the northern façade of the building,</li> <li>- a redesign of the acoustic wall where the upper 1m of the 4m high wall would now be 'cranked' inwards at a 45 degree angle, and,</li> <li>- reconfiguration of some parking spaces and delivery vehicle movements though the site.</li> </ul> <p>The DAC granted Development Plan Consent to the proposal subject to 23 conditions.</p>

## 2. RELEVANT DECISIONS BY DAC

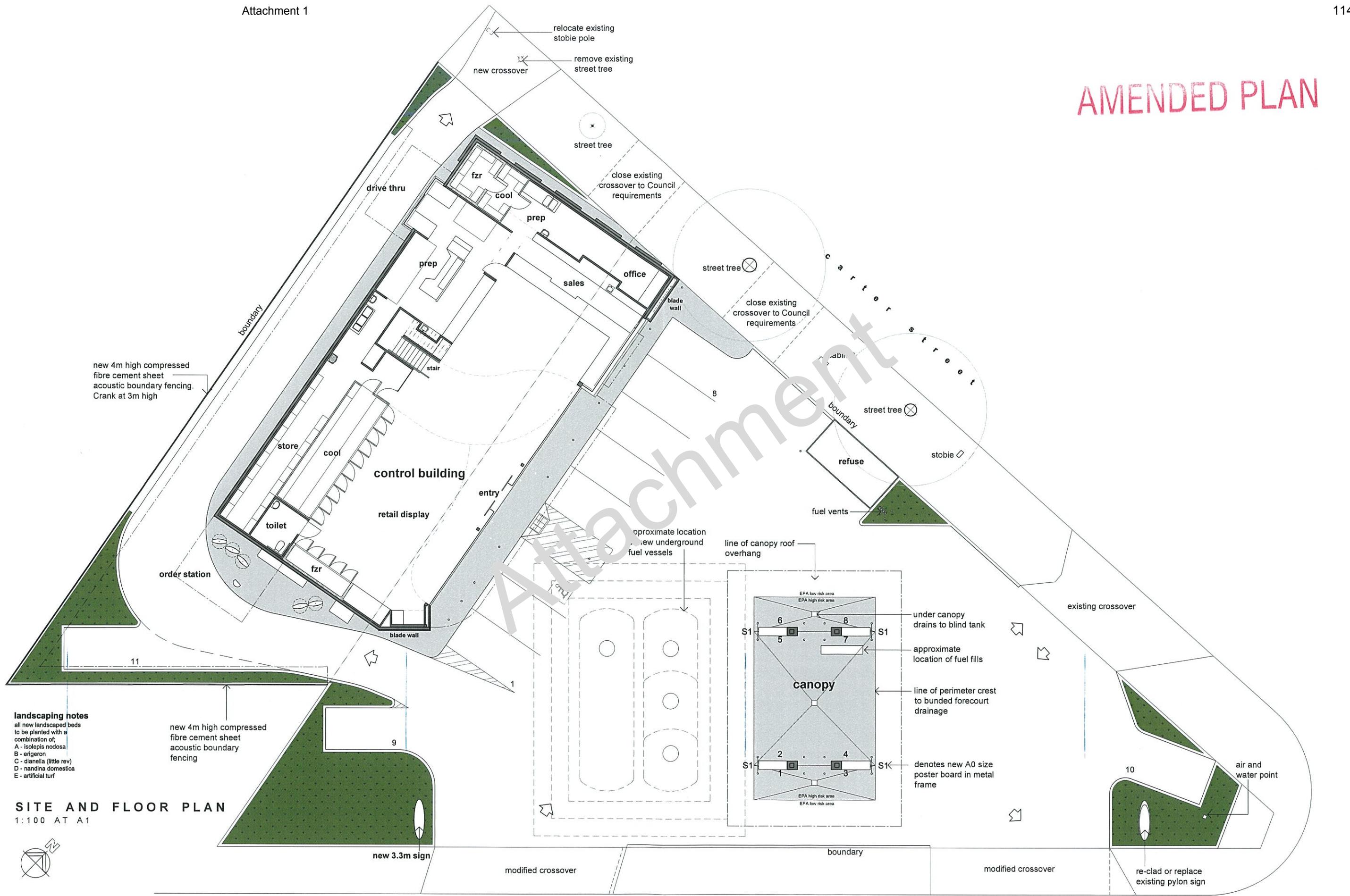
No new proposals have been determined by the DAC.

## 3. MATTERS CALLED IN BY THE CO-ORDINATOR GENERAL

No new proposals have been called in by the Co-ordinator General.

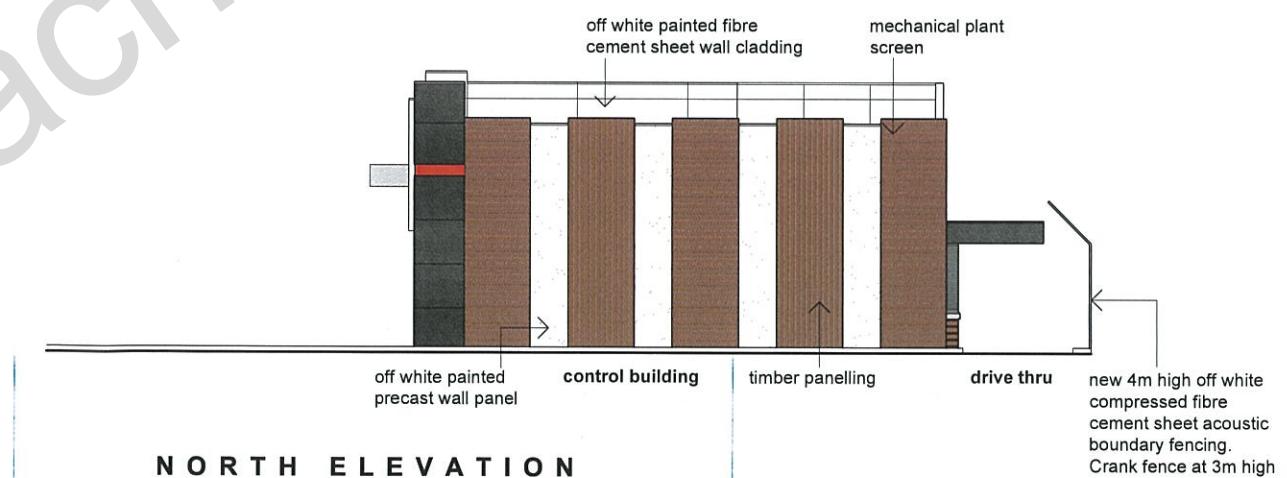
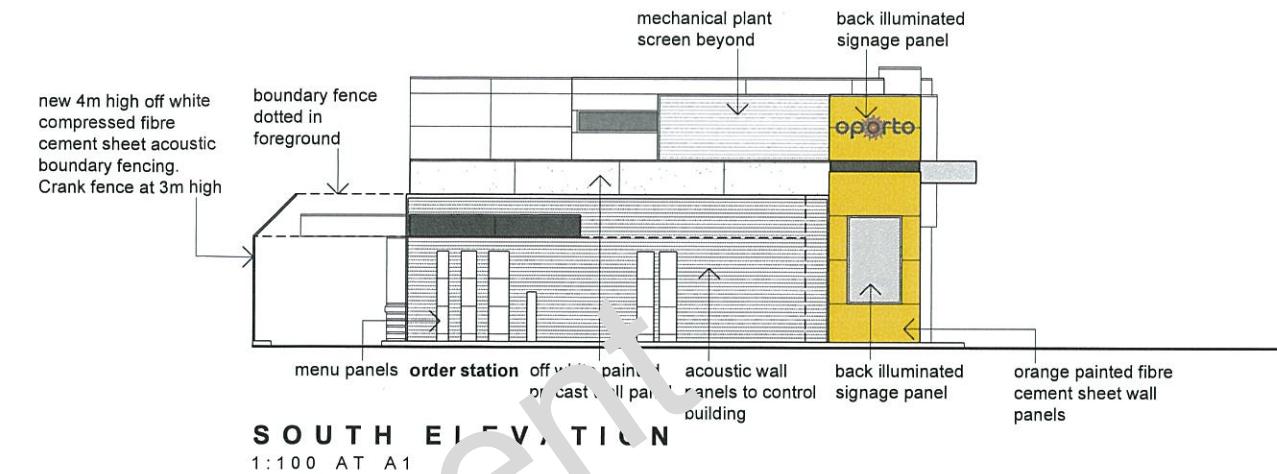
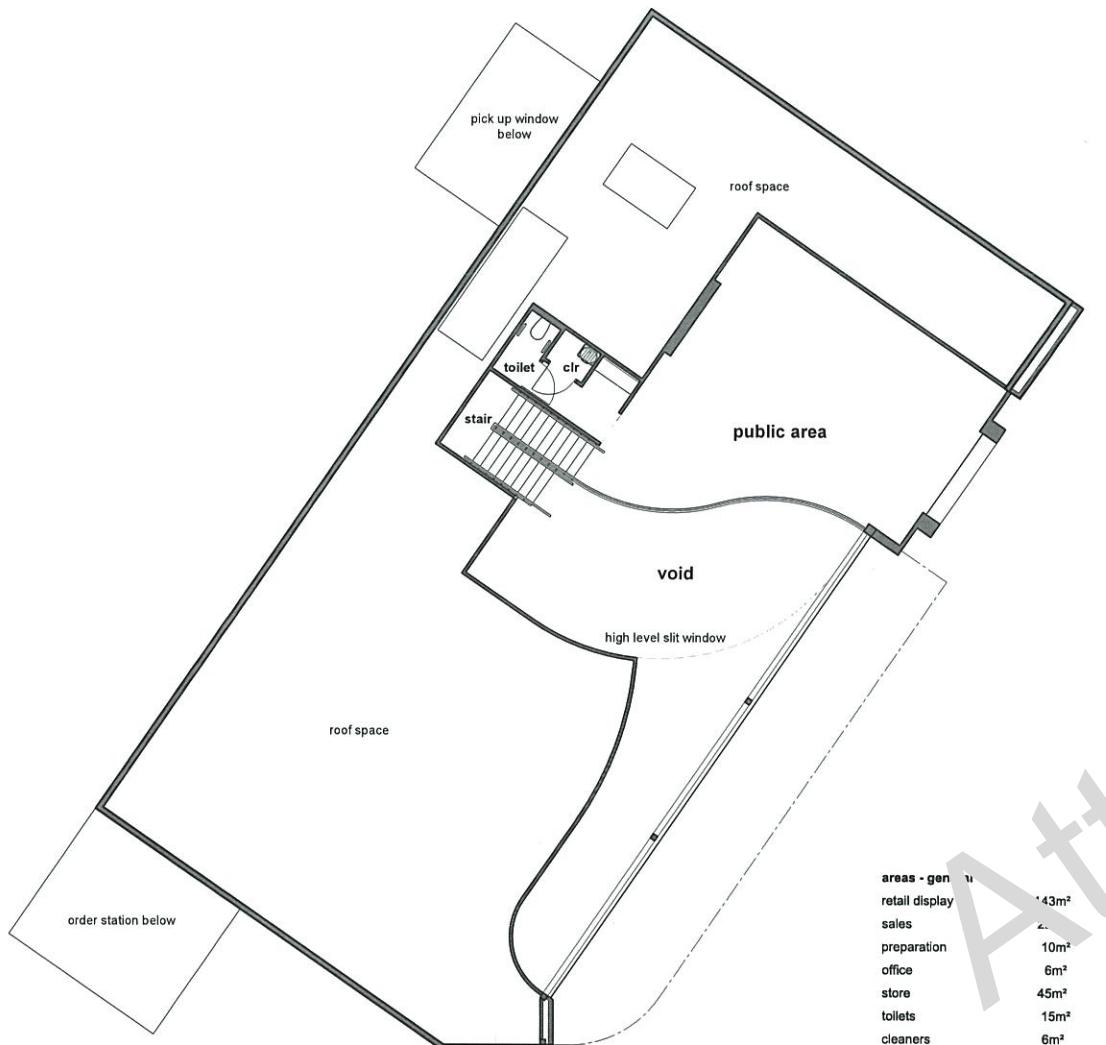
**Nathan Cunningham**  
Director Community, Planning and Communications

## AMENDED PLAN

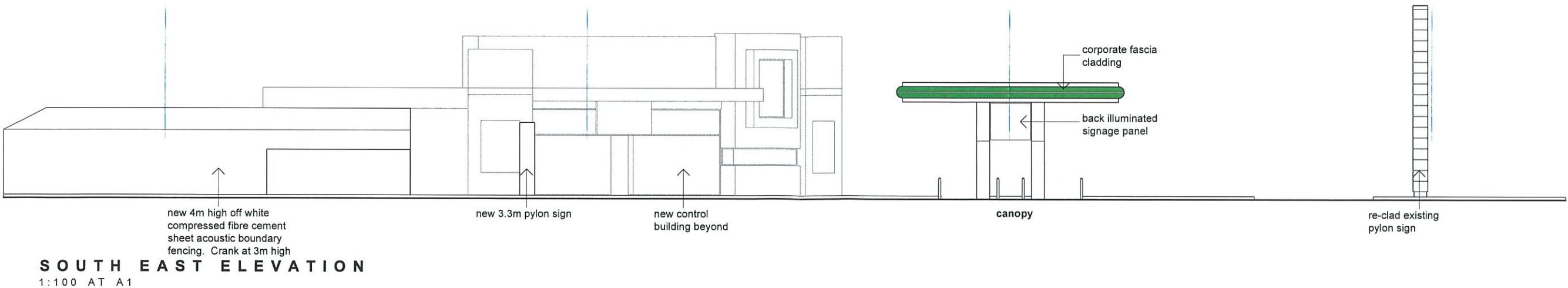
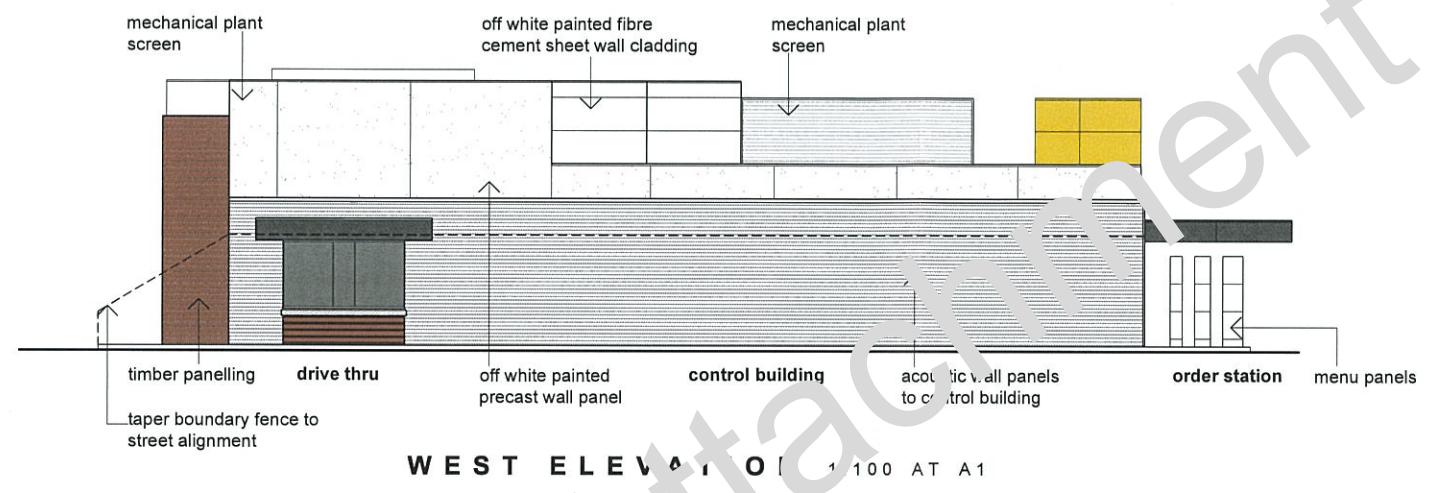
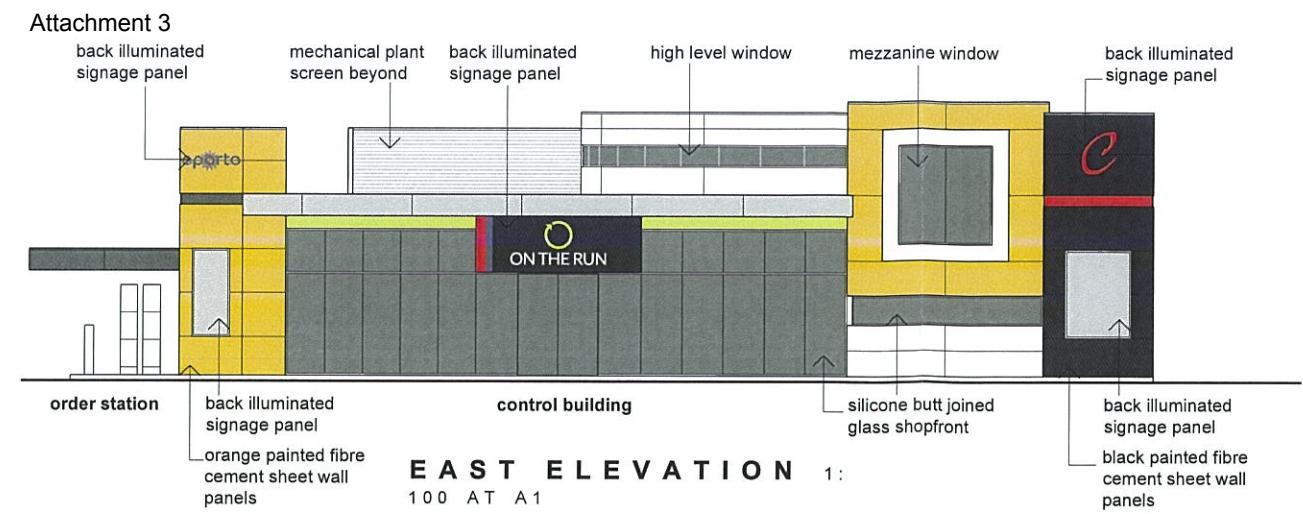
**BP FITZROY**

20a Main North Road Thorngate SA

# AMENDED PLAN



# AMENDED PLAN



**BP FITZROY**  
20a Main North Road Thorngate SA

**ITEM NO.:** 7.1

**TO:** Development Assessment Panel (DAP) on 15 June 2015  
**FROM:** Director Community, Planning and Communications  
**SUBJECT:** Summary of Court Appeals

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The status of appeals is provided to the DAP for information purposes. Questions or further clarification may be sought from staff during the meeting.

## APPEALS

<b>Development Application / Subject Site</b>	<b>Nature of Development</b>	<b>Decision authority and date</b>	<b>Current status</b>
DA 050/277/2014  252-264 Main North Road, Prospect	Extension to Pylon Sign	9 February 2015  By the DAP	Appeal lodged by applicant.  <i>Appealing against Refusal.</i>  The applicant has advised of their intention to provide amended plans for consideration. It is anticipated that this will occur at the July DAP meeting.
DA 050/323/2014  32 Hampstead Road, Broadview	Two, Two Storey Residential Flat Buildings comprising Seven Dwellings with associated Carports, Driveway and Landscaping	12 January 2015  By the DAP	Appeal lodged by land owner (on behalf of applicant).  <i>Appealing against Refusal.</i>  Following the applicant's non-attendance at court on both 8 April 2015 and 13 May 2015, the Court initiated a telephone conference between the parties.  The applicant has advised of their intention to lodge a new application for a reconfigured proposal. The Court has scheduled a conference for 9 July 2015 to ensure this application is lodged in a timely fashion.
DA 050/205/2014  19 North East Road, Collinswood	Decking and Privacy Screens	8 December 2014  By the DAP	Appeal lodged by neighbour.  <i>Appealing against Approval.</i>  The appeal was heard by the Court on 14 May 2015, during which an independent planning expert provided evidence in support of the DAP's decision.  The Commissioner has reserved judgement at this stage. It is unknown when the judgement will be handed down.

DA 050/290/2014  125 Devonport Terrace, Prospect	Single Storey Detached Dwelling	22 October 2014  By Staff Under Delegation	<i>Appeal withdrawn.</i>  An amended engineering plan has been lodged and approved. It is noted that the amended plan did not alter the previously approved floor levels.
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**Nathan Cunningham**  
Director Community, Planning and Communications